



Rugby Borough Council

Matter 4

Issue 4a: Overall Soundness of the MRS
Allocations

Issues 4b: Site Specific Issues for the MRS and
Coton House Allocations

Issue 4a: Overall Soundness of the MRS Allocations

- 1. Are the proposed residential allocations at the Main Rural Settlements (MRS) identified in Policy DS3 positively prepared, justified, effective and consistent with national policy? In particular:**
 - a. Having regard to the benefits which may arise and the harm which may be caused, do 'exceptional circumstances' exist to justify the alteration of Green Belt boundaries to allow residential development at the Main Rural Settlements?**

Introduction and Background to the Approach in the Development Plan

- 1.1 As set out in the Introduction to the Council's Hearing Statement to Matter 3 – Development Strategy, Chapter 2 of the Publication Local Plan (LP01) provides the context for planning the growth for the borough over the plan period. The settlement hierarchy for the Borough is established and straightforward and dominated by Rugby Town. It is by far the largest settlement in the borough, where two thirds of the Borough population reside. Beyond the town, the remaining population reside in much smaller settlements, where populations are no greater than 3000 people.
- 1.2 60% of the Borough is Green Belt, where all but two of the Main Rural Settlements, (the second most sustainable tier in Policy GP2) are located. Historically very little growth has come forward in these locations, with the exception of a small number of previously Green Belt safeguarded sites. As stated in the Housing Background Paper (LP11), the two other Main Rural Settlements (Dunchurch and Clifton-on-Dunsmore) are located in close proximity to Rugby town.
- 1.3 Those remaining settlements within countryside locations are much smaller, and as detailed in the Rural Sustainability Study (LP28) have much more limited services and are not considered sustainable for further growth. It should be noted that the Rural Sustainability Study is not the first iteration of such work and it updates a Settlement Hierarchy Background Paper produced in 2008 as part of preparation of the Core Strategy. The unifying objective behind this work, and the spatial strategy as a whole, is to ensure that the latest information is taken into account in ensuring that the projected requirements for growth are met in the most sustainable locations over the plan period.

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1.4 This is further explained by the supporting text of the existing Core Strategy in Paragraph 2.2 that explains:

“This focus on the town centre for services and facilities and on the remaining town for wider development needs is further supported by the role of the Main Rural Settlements and Local Needs Settlements across the rural areas of the Borough. This approach continues to ensure that development is co-located with supporting infrastructure and is informed by the Settlement Hierarchy Background Paper and experience of the Rugby Borough Local Plan 2006.”

1.5 Policy CS1 of the existing Core Strategy took forward support for development within existing village boundaries for the Main Rural Settlements, consistent with their nature as inset from the Green Belt. The approach to providing for development boundaries at Main Rural Settlements which are inset from the Green Belt is consistent with Paragraph 86 of the NPPF. This approach is preferred where it has been established that the prevention of development would not be in the interests of maintaining the open character of the Green Belt.

1.6 The approach followed by the 2006 Local Plan also included provision for ‘Reserve Housing Sites’ and ‘Safeguarded Housing Land’ in some Main Rural Settlements. The Previous recent development at Wolston and Long Lawford provides good examples of delivery within these housing markets under this approach.

1.7 The Council’s overall approach to the development strategy has been discussed in detail in its Statement for Matter 3: Issue 3a (in-particular questions 1a and 1c). These responses cover the overall options for the distribution of development and the considerations (including the need to release land from the Green Belt) to support sustainable patterns of development across the settlement hierarchy. The benefits of maintaining and enhancing the role of Main Rural Settlements as part of the settlement hierarchy are evident in the preparation of the development strategy. This includes both the identification of land for a new Main Rural Settlement at Lodge Farm (outside of the Green Belt) and supporting existing centres. Discussion of Policy GP2 in the Stage 1 Hearings considered the need for a Modification to the ‘Main Rural Settlements’ component of the table, confirming support for sites allocated for development and the allocation of Lodge Farm at this tier.

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- 1.8 Whilst retaining a significant focus on for development at Rugby town and the urban edge, the overall direction of the development strategy and justification of sites within Policies DS3 and DS4 also considers the constraints of the Borough. This is compliant with paragraph 17 of the NPPF, to allocate land of lesser environmental value and actively manage patterns of growth to make the fullest use of public transport, walking and cycling, and focus significant development in locations which are, or can be made sustainable.
- 1.9 The Council's Housing Background Paper [LP11] and Sustainability Appraisal [LP03] respectively set out the options for development and significant effects with respect to Main Rural Settlements over the plan period. These assessments also consider the overall development strategy and consistency with the existing Core Strategy approach.
- 1.10 The Borough Council's decision to take forward a combination of 'Options 3 and 5' [see LP11 Figure 9 and LP03 Para 5.74] was reached following an assessment of each of the Distribution Strategy Options, including whether there were sufficient sites to deliver a continuous five-year housing supply throughout the Local Plan period. This supports the contribution to sustainable development arising from both some boundary alterations to existing Main Rural Settlements and the allocation at Lodge Farm. The Borough Council's view is that the combination selected also takes account of policies and principles of the NPPF taken as a whole.

The Identification of Exceptional Circumstances

- 1.11 The approach taken by Rugby Borough Council in preparing this Local Plan is entirely consistent with national policy and the evidence base illustrates the basis for exceptional circumstances to provide for the proposed alteration of Green Belt Boundaries. In-particular, Paragraph 83 of the NPPF clarifies that the Green Belt boundaries established by Local Plans should set the framework for Green Belt *and settlement policy*. The preparation or review of a Local Plan represents the appropriate points to consider whether exceptional circumstances exist to amend these boundaries. Paragraph 84 of the NPPF continues that taking account of the consequences for sustainable development, including at towns and villages inset within the Green Belt, should inform any such review.

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- 1.12 Supporting development through the established approach accords more widely with the core principles of the planning system set out through the NPPF (bullets 5 and 11 in-particular). Paragraph 50 supports identification of the size, type, tenure and range of housing required to meet local demand. To achieve a prosperous rural economy, it is important that approaches are positive and support the retention and development of local services and community facilities (Paragraph 28). Consistent with the role of Main Rural Settlements, Paragraph 55 of the NPPF outlines: *“To promote sustainable development in rural areas, housing should be located where it will enhance or maintain the vitality of rural communities. For example, where there are groups of smaller settlements, development in one village may support services in a village nearby.”*
- 1.13 Planning Practice Guidance establishes that the Local Plan can direct the approach to meeting needs at a strategic level, especially to guide the measures appropriate for different communities and ways to address housing supply and affordability in rural areas (NPPG ID: 50-001-20160519).
- 1.14 As discussed in the Council’s Rural Sustainability Study (2015) (LP28) it is important to emphasise the distinction that can be identified between Main Rural Settlements and other tiers of the settlement hierarchy. In updating earlier work from preparation of the 2006 Local Plan and 2011 Core Strategy it is unsurprisingly those villages currently designated as Main Rural Settlements have scored highest overall in terms of the availability of ‘Key’ (e.g. Doctor’s Surgery) and ‘Other’ services. The scores at different tiers of the settlement hierarchy are also largely defined by access to Public Transport.
- 1.15 The existing support for the role of the Main Rural Settlements has maintained their sustainability through previous iterations of the development plan. The Council considers that future opportunities for sustainable development can best be supported by recognising the existing group of 9 settlements within a single category. This is particularly true because individual settlements are likely to be on the ‘cusp’ of any subdivision and the reasons for different scoring need to be understood in context.
- 1.16 For example, Ryton-on-Dunsmore achieves the lowest combined score from the Main Rural Settlement category, but scores higher than Long Lawford in terms of availability of services (the difference arising due to availability of public transport) (LP28 pp.14). There is also likely to be some relative differences

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between the nature of services or land uses between Main Rural Settlements even though each might qualify for relevant scoring within the Rural Sustainability Study – for example employment areas in one settlement might be materially larger than those in another.

- 1.17 It is also important to note the similarity between Main Rural Settlements in terms of 'Key' facilities that are absent from each location due to the characteristics of locations across Rugby. For example, all lack provision of secondary education or medical services beyond Doctors' surgeries. Considerations for preparation of the development strategy indicated that it was not realistic to deliver a significant uplift in the order of services or sphere of influence of one Main Rural Settlement relative to the others. This demonstrates the need for a consistent approach to assessing development options.
- 1.18 It is also important to recognise that the roles of Local Needs Settlements are much more varied – whilst all 25 settlements have a combined score below those in Main Rural Settlements these scores span a much greater range. The opportunities for maintaining or enhancing a particular level of services in these settlements are much more varied. This is summarised in the assessment of options within the Housing Background Paper and in-particular the finding that no Local Needs Settlements could be upgraded to a Main Rural Settlement (LP11 Paragraph 3.84). Furthermore, the options for development identified through plan-making are more limited – only 33 site options are assessed as reasonable alternatives at Local Needs Settlements as part of the Sustainability Appraisal process (LP03 Paragraph 4.13).
- 1.19 The existing approach towards Main Rural Settlements within Rugby makes it entirely sound, and consistent with considering Green Belt boundaries through the preparation of Local Plans, to undertake a review of site constraints and alternatives at the settlement level. As part of supporting the established spatial strategy, the fact that sites are achievable and deliverable within the timescales envisaged provides a material consideration towards the demonstration of exceptional circumstances to allow the Local Plan to provide for development requirements in these locations. This provides a justified approach, taking account of national policy, to provide focal points for some growth and taking into account the scale of overall requirements and the form and character of individual settlements. Upon adoption, the sites will provide supply in locations where

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recently opportunities for development have been more restricted. The contribution towards overall requirements and the first five years of the plan period is considered further in the response to Matter 4a (b) below.

- 1.20 The level of growth at each Main Rural Settlement indicated through preparation of the development plan (of 100 units) broadly takes account of levels of growth achieved in the past that has been found to safeguard the role and sustainability of these locations. Within this framework, the assessment of individual sites considers the specific suitability for development. This includes the ability of outcomes to avoid harm and maintain the role of the Green Belt in order to establish whether exceptional circumstances exist to support specific amendments to boundaries. This includes assessment against the purposes of the Green Belt (contained in NPPF Paragraph 80) as part of the site selection process.
- 1.21 It is consistent with the spatial strategy for the overall scale of allocations at each settlement to be determined flexibly, based on the absence or ability to control adverse impacts or demonstrate where these are clearly outweighed by the associated benefits. The consultation on the approach to proposed allocations in the Main Rural Settlements at each stage of plan preparation and in issuing 'Development Packs' has provided input from a wide range of stakeholders to refine development options, minimise harm and reinforce the deliverability of sites to contribute towards the overall strategy.
- 1.22 The overall approach to the spatial strategy recognises that the benefits associated with sustaining and enhancing the role of Main Rural Settlements cannot be achieved through development options that exist within existing development boundaries. This is established through an assessment of sites identified in the SHLAA and is summarised in the Housing Background Paper [LP11 see Paragraph 3.76]. This includes, in-particular, that allocation of sites through the development plan provides the most feasible route to achieve the provision of additional affordable housing at Main Rural Settlements and meet the widest possible range of needs where they arise locally.
- 1.23 The contribution towards the delivery of affordable housing as part of the case for exceptional circumstances relates to the Council's overall approach to the settlement hierarchy. In-particular this relates to the re-designation (in Policy GP2) from 'Local Needs Settlements' to 'Rural Villages', where going forward

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development will be supported within existing development boundaries. Such schemes will often be below the threshold for affordable housing contributions. Paragraph 3.78 – 3.79 of the Housing Background Paper (LP11) explains the background to this change, whereby previous support to provide for only locally identified affordable housing needs has yielded very limited development. Whilst the Local Plan continues to provide support for Rural Exception Sites, the combined role of these changes illustrates the more significant and deliverable contribution that allocations at Main Rural Settlements can provide towards meeting affordable housing needs.

- 1.24 The Council considers that the diversification of supply achieved through allocations at Main Rural Settlements is a benefit that contributes to the case for exceptional circumstances. These benefits are emphasised where a realistic prospect exists for housing to be delivered in the five-year period, taking account of the need to achieve higher rates of development upon adoption. Where supply has been much more restricted in recent years (i.e. in the Green Belt) the availability of sites is likely to support meeting demand as soon as possible upon adoption and therefore considered to be a benefit.

b. Are the proposed MRS allocations necessary to meet the borough's housing requirement and what would be their overall contribution to maintaining a deliverable 5 year housing land supply

Contribution to overall requirements and as a proportion of total supply

- 1.25 The Council's Housing Background Paper (Paragraphs 3.3 – 3.5 in-particular) considers the recent strategy for distributing housing across Rugby Borough across the various iterations of the development plan. Although the focus on Rugby town is clearly well-established development beyond the main settlement has made a consistent contribution towards overall requirements. Within the 2006 Local Plan around 13% of development towards annual requirements was expected to occur outside of Rugby, Dunchurch and Long Lawford.
- 1.26 Paragraph 3.4 of the Housing Background Paper refers to the Core Strategy commitment to provide 90% of development in and around Rugby town. In order to properly understand this distribution, it is relevant to consider the pipeline of supply accounted for in the Housing Trajectory developed as part of preparation

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of the Core Strategy (and included as Core Strategy Appendix 2). This does not include an allowance for windfall and shows past completions in the period 2006 to 2010. It also includes a colour-coded annotation of capacity at Main Rural Settlements – totalling 702 dwellings for the remaining years 2010 to 2026.

- 1.27 Another feature of the Core Strategy trajectory (consistent with the pattern of supporting strategic extensions to the Rugby urban edge) is the identification of a forecast surplus against Core Strategy requirements to 2026. Taking account of completions already recorded to March 2010, the supply identified in rural areas represented 8.9% of the residual Core Strategy requirement 2010 to 2026; or a proportion of 7.5% of supply identified by the Core Strategy over the same period.
- 1.28 Paragraph 3.5 of the Housing Background Paper allows comparison of the position of existing supply in the rural areas as part of the committed supply and also provides an evaluation of delivery of the strategy to-date. It is evidently clear that at 310 dwellings the proportion of supply in the rural area is significantly lower than the point at which the Core Strategy was adopted (only 3.16% of the residual Local Plan requirements to 2031). What is also clear is that whilst supply in the rural areas has reduced more quickly a very large level of committed supply remains in place at Rugby town. This shows the effect of delays to the delivery of the major sites relied on in the Core Strategy (despite the surplus in overall supply), carried forward into the Local Plan. It also illustrates the role of other markets in meeting the overall requirement for development.
- 1.29 Paragraph 3.5 of the Housing Background Paper shows supply in the rural areas as a component of existing commitments (9,248 dwellings). This includes capacity on areas such as the Rugby Radio Station Site anticipated outside of the plan period but excludes windfall and proposed Local Plan allocations. To provide a meaningful comparison with the position on adoption of the Core Strategy it is necessary to exclude windfall and capacity outside the plan period but to include allocations proposed by the Local Plan. This is illustrated in the table below for the 'Publication' Housing Trajectory (LP54.116) from 1st April 2017:

Table 1: Identified Supply in Rural Areas as a Proportion of Residual Plan Requirements

		1	2	3	4
		Core Strategy 2011 (@ April 2010) ¹	Local Plan 2031 (Committed @ April 2017) [LP11 Para 3.5]	Local Plan 2031 (Plan Period) (inc MRS ² Sites) (@ April 2017)	Local Plan 2031 (Plan Period) (less MRS Sites) (@ April 2017)
A	Target	10800	12400	12400	12400
B	Residual Target (at April in year indicated)^{\$}	7898	9823	9823	9823
C	Total Plan Period Supply (For Columns 1 and 2 Figure includes Windfall [630] and 2011-17 Completions [2577])	12226		15369	14822
D	Supply* less Windfall	9324		12162	11615
F	Committed Supply[#]	-	9248	-	-
H	Rural Areas	702	310	857	310
I	Rugby Town	8622	8938	11305	11305
J	% Outside Rugby Town (as % of supply) (H / D)	7.5%		7.0%	2.7%
K	% Outside Rugby Town (as % of requirement) (H / B)	8.9%	3.16%	8.7%	3.16%
	<i>*Supply also Excludes Rugby Town Commitments beyond plan period</i>				
	<i>[#]Calculation from Paragraph 3.5 of LP11 - includes total capacity of sites with permission at Rugby Town, including capacity beyond plan period</i>				
	<i>^{\$}Requirement less completions since start of plan period (2,902 in column A (2006-2010) and 2,577 in Columns B-C (2011-2017))</i>				

¹ Based on Appendix 2 of the adopted Core Strategy (2011)

² MRS: Main Rural Settlement

- 1.30 It can clearly be seen in the table above that with the inclusion of 547 dwellings' supply on proposed allocations at Main Rural Settlements the overall distribution would be similar to that set out in the Core Strategy. The figure of 8.7% excludes development of a new Main Rural Settlement at Lodge Farm, which will add further diversity to supply later in the plan period. Equally it can be seen that without further allocations at the existing Main Rural Settlements the proportion of supply in the rural areas would remain at just 3.16% of the residual requirement (2.7% of total supply).
- 1.31 The Council considers that the distribution of supply excluding allocations at Main Rural Settlements would not be effective and would not be consistent with the overall approach to the spatial strategy and supporting the role of these centres. Furthermore, it would expose the Local Plan to considerable risk in terms of the ability to achieve a step-change in housing delivery and maintain a rolling Five Year Supply of land for housing, based on the experience of performance against the Core Strategy requirements. The proposed allocations at Main Rural Settlements are part of a positive response to meeting overall housing needs in a manner consistent with the overall strategy for development. The allocations are justified as they are a fundamental part of the overall housing trajectory and represent an adaptation to the urban focussed growth of the previous Core Strategy which did not deliver the expected growth as quickly as was envisioned.

Contribution towards maintaining a Five Year Supply

- 1.32 The overall approach to preparation of the Council's Housing Trajectory has been set out in detail in the response to questions for Matters under Stage 1 of the Local Plan Examination (see especially Matter 3 Issue 3(a) question 1(h) and 1(ii)). The evidence base for this discussion is largely provided by the Council's Housing Background Paper [LP11] and Housing Market Delivery Study (2015) [LP42]. However, the previous sub-section to this response provides greater detail on the contribution of sites outside the Rugby town area (including urban edge locations) to overall supply. It can therefore be demonstrated more clearly why the proposed allocations at Main Rural Settlements make a particularly important contribution to the Local Plan's availability to maintain sufficient supply against identified housing requirements, particularly in the early years following adoption.

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- 1.33 The Council has previously acknowledged its predicament of being unable to demonstrate a Five Year Supply of land for housing in recent years, even with the over-allocation of sites through the Core Strategy. Whilst this position has improved in recent years through the evolution of the planning commitments pipeline and progress towards delivery the Council can only demonstrate a Five Year Supply against the requirements on adoption by including forecast delivery on sites within the 'Publication' housing trajectory. This includes the proposed Main Rural Settlement allocations, which provide flexibility and a robust view on deliverability early in the plan period; individually these sites are subject to relatively less risk than strategic extensions to the urban edge.
- 1.34 Allowing the Main Rural Settlements to expand in a genuinely plan led manner, in line with paragraph 17 of the NPPF, means that new housing markets can be established within a relatively short time frame. Due to the fact that the sites have little or no constraints, their development would come forward relatively quickly, thereby securing delivery early in the plan period. This approach is a direct response to analysing past delivery rates where sites such as the Radio Station site have not delivered according to forecasts. The decision to allocate within the Main Rural Settlements positively and proactively seeks to manage delivery and allow the plan to succeed as opposed to the prospect of 'planning by Appeal' in the absence of a Five Year Supply.
- 1.35 The 'Publication' Housing Trajectory [LP54.116] forecasts a total of 5,229 completions for the five-year period 2018-2023 (the first five years following adoptions). This represents a 126% increase in the recorded and forecast completions of 2,318 dwellings in the 2013 to 2018 period. The Publication Trajectory also includes delivery of the full 527 dwellings within proposed Main Rural Settlement Allocations forecast within the 2018-2023 period.
- 1.36 For the purposes of demonstrating the contribution of Main Rural Settlement allocations towards supply in the five-year period the following two points are relevant:
- 1) Based on the Council's response to Matter 4: Issue 4b (Site Allocations) it is concluded that the capacity of all proposed Main Rural Settlement allocations except Policy DS3.9 'Leamington Road, Ryton-on-Dunsmore' should be considered deliverable within the five-year period. This does not affect the conclusion of developability within the plan period but means the contribution

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of Main Rural Settlement allocations towards the five-year requirement is expected to be 452 dwellings. An equivalent reduction is made to total supply forecast in the five-year period (5154 dwellings); and

- 2) The calculation is updated from that derived through Paragraph 3.96 and Figure 8 of the Council's Housing Background Paper [LP11]. This takes account of the reduced forecast for completions in 2017/18 (530 dwellings rather than 596) and the resulting impact on accumulated shortfall and residual needs over the plan period.

1.37 The calculation below illustrates the position of supply against the five-year requirement on adoption (in the year from 1 April 2018) including and excluding Main Rural Settlement allocations:

Table 2: Updated Calculation of Projected Housing Land Supply upon Adoption

Position @ 1st April 2018)		Sedgefield
A	Housing Target (2011-2031) (including unmet needs)	12,400
B	Annual Requirement to Point of Adoption	540
C	Housing Requirement to Point of Adoption (B *7)	3780
D	Recorded and Forecast Completions to Adoption	3112
E	Under supply (C-D)	-668
F	Contribution to Coventry's Unmet Needs	2800
G	Annualised Unmet Need for Coventry - Residual Post Adoption (F - SUM((B-H)*7)/13)	183.08
H	Rugby OAN - Annualised (9,600 total)	480
I	Annualised Plan Target Post-Adoption 2018/19 - 2030/31 (G + H)	663.08
J i	5 year requirement + undersupply [Sedgefield: I*5 + E]	3983
J ii	5 year requirement + undersupply [Liverpool: I*5 + (E/13)*5]	
K	5 Year Requirement + undersupply + 20% buffer (J * 1.20)	4780
L	Annualised requirement (K/5)	956.01

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X	Forecast Completions 2018-2023	5154
	<i>Five Year Supply</i>	5.39
	<i>Surplus/Deficit against Five-Year Requirement</i>	+374
X i	Forecast Completions 2018-2023 less Main Rural Settlement Sites)	4702
	<i>Five Year Supply</i>	4.92
	<i>Surplus/Deficit against Five-Year Requirement</i>	-78

1.38 The overall conclusion from Table 2 above is that if the Main Rural Settlement allocations were omitted from the trajectory, the Council would not be able to demonstrate a Five Year Supply at the point of adoption and would not have a sound plan. Furthermore, as detailed in Paragraphs 1.88 and 1.89 of the council statement for Matter 3, the role of the MRS allocations is to vary the size and location of sites to ensure the continuous supply of housing and therefore to negate the need for speculative and unplanned development to occur due to the 'tilted balance' for the presumption in favour of sustainable development being engaged through paragraph 49 of the NPPF.

1.39 Successful delivery of the Main Rural Settlement allocations is important to secure delivery against the plan's housing requirements and look to avoid any increase in accumulated shortfall. This is particularly important for maintaining performance against the Local Plan's monitoring indicators and to provide a degree of flexibility should some elements of the main urban extension sites be delayed outside of current forecasts.

1.40 The forecast delivery of 452 dwellings on Main Rural Settlement allocation sites in the five-year period amounts to around 8-10% of total supply between these years. This component is nonetheless critical to whether sufficient supply can be demonstrated to achieve the five-year requirement at April 2018.

1.41 It should be kept in-mind that it is a function of the Housing Trajectory on adoption that it will begin to reflect the delivery of numerous sites proposed for allocation in the Local Plan including those without planning permission

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(LP54.116). This additional supply totals 1,687 units in the five-year period (including the contribution from Main Rural Settlement sites). This supply also includes the early stages of development at South West Rugby and the Coton Park East allocations. The proposed allocation at Lodge Farm is forecast to make only a very limited (25 unit) contribution to supply over the same period. The nature of the larger allocations proposed by the Local Plan is that they will take a number of years to establish and delivery the majority of supply beyond the first five-years. On this measure, the Main Rural Settlement allocations represent 25% of supply introduced through the Local Plan and deliverable within the early part of the plan period. This shows their critical contributions towards a Five Year Supply upon adoption.

- 1.42 The remainder (and vast majority) of the forecast supply within the five-year period is drawn predominantly from existing commitments (including strategic allocations at Rugby Gateway and Rugby Radio Station). The overall assumptions for delivery in Rugby town and the urban edge are provided by the latest monitoring information and summarised in the Housing Background Paper [Paragraphs 3.60 – 3.62 of LP11]. These are broadly in accordance with (although slightly exceed) the expected operation and improvement in the number of developer outlets forecast by the 2015 Housing Delivery Study [LP42].
- 1.43 It is also the case that whilst the proposed allocation of a new Main Rural Settlement offers a sustained opportunity to diversify supply from the middle of the plan period, in-line with the Delivery Study recommendations, this pattern of delivery will take some years to establish. The experience of a slight shortfall between forecast completions and recorded output for the 2017/18 monitoring year, even during a period of relatively strong performance, indicates that risks within the housing trajectory are predominantly centred on Rugby town and the urban edge.
- 1.44 The proposed Main Rural Settlement allocations contribute to a more balanced trajectory and provide important contingency in the early years of the plan period. This allows additional time for the main Sustainable Urban Extensions to deliver the majority of units later in the plan period. Due to minimal constraints to development (following allocation) and the range of locations covered the MRS components provide support to diversify supply in areas where demand is likely to be strong. The characteristics of the settlements and allocations is also significant

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in terms of their ability to provide policy-compliant schemes, including the delivery of affordable housing, which is likely to boost overall levels of output.

c. Are the proposed MRS allocations consistent with the spatial strategy and settlement hierarchy for the borough as set out in Policy GP2?

1.45 The detailed assessments relied upon to inform the site selection for allocations at each Main Rural Settlement provided by the relevant 'Development Packs' illustrate the consistency of this work within the plan-making process and overall spatial strategy. In-particular these follow the introduction of the accommodation of some development at this level of the settlement hierarchy as part of the Local Plan Preferred Option consultation. Preparation of the Development Packs takes account of the findings from that consultation, including the responses received and discussions held with a number of statutory consultees who are responsible for infrastructure provision.

1.46 In reviewing the responses received and holding further discussions with the consultees, the context for site selection and preparation of Development Packs continued to support the level of growth indicated as around 100 dwellings at each Main Rural Settlement. The evidence to support this element of the spatial strategy suggests no overall constraints likely to prevent the delivery of this total. However, the approach to plan-making also demonstrates that higher levels of growth would not provide reasonable alternatives to fundamentally change the services and facilities at any one Main Rural Settlement compared to the others across the Borough.

1.47 The proposed allocations are therefore entirely consistent with the spatial strategy and settlement hierarchy within policy GP2 because they have been carefully selected when considered against reasonable alternatives (the assessment process and criteria are covered further under Question (d) and Matter 4b). This has ensured that the sites selected are the most suitable, including being of an appropriate size and scale and location at or close to the village boundary with consideration given to strengthening the village boundaries to further protect the Green Belt.

1.48 The assessment of rural sustainability and process of site selection has allowed consideration of the outcomes at individual locations in the settlement hierarchy.

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The contribution towards sustainable development seeks to ensure that appropriate opportunities are taken to sustain and enhance the role of Main Rural Settlements within the hierarchy without changing their overall role. This is also supported by the overall conclusions from considering different options for development as part of preparing the plan. In-particular, Paragraphs 3.83 and 3.84 of the Housing Background Paper confirm that no Local Needs Settlements were considered capable of upgrading to a Main Rural Settlement, based on development potential and the level of services that could be supported. It would therefore not be appropriate to distribute further significant development to lower tiers of the settlement hierarchy.

1.49 It should also be noted that, during the period for preparation of the Local Plan, the distribution of committed supply and the position of current shortfall against housing requirements have both evolved to reinforce the case for exceptional circumstances to support proportionate allocations at Main Rural Settlements. During the 2015/16 Monitoring Year (the timing for the Preferred Option consultation) the housing shortfall amounted to -499 dwellings to the period ending 31st March. In the following year (during consultation on the Publication Draft) the shortfall had increased to around -650 units at 31st March 2017. At the same time, completions continued to be achieved on remaining outstanding commitments in the rural area, reducing the remaining proportion of supply outside of Rugby town.

d. Was the process for the selection of the MRS site allocations robust? Was an appropriate range and selection of sites assessed and were reasonable alternatives considered? Were appropriate criteria taken into account in deciding which sites to select? Was the assessment against those criteria robust?

1.50 Rugby Borough Council has taken a robust and consistent approach towards identifying the proposed Main Rural Settlement Site Allocations. The proposed Main Rural Settlement site allocations draw upon the extensive evidence base for the Local Plan as a whole to inform the site selection process. The synthesis of this evidence is provided across 5 key sources which interact to comprehensively illustrate the case for exceptional circumstances in each specific case. There are:

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- The Rugby Borough Council Strategic Housing Land Availability Assessment(s) (LP09, LP10 and LP10a)
- Coventry and Warwickshire Joint Green Belt Review (LP30)
- Main Rural Settlement Development Packs (LP44 – LP50)
- Preparation of the Housing Background Paper (LP11)
- Preparation of the Sustainability Appraisal for the Local Plan (LP03)

1.51 It is important to recognise that the preparation of each of these sources has been part of an active and ongoing process. The key outcomes that demonstrate support for the allocations at Main Rural Settlements have been subject to consultation at two specific stages of plan preparation (Preferred Option and Publication Draft). However, further opportunity for community and stakeholder engagement has been provided as part of specific work on relevant Development Packs.

1.52 The requirement to support proportionate growth at the Main Rural Settlements to maintain and accord with the overall spatial strategy formed part of the Council's consultation on the 'Preferred Option' for the Local Plan (December 2015). The Council also undertook a further 'Call for Sites' alongside the Preferred Option consultation. This provided a further opportunity to ensure that all possible options to accommodate residential development had been explored.

1.53 That Preferred Option consultation followed completion of the Joint Green Belt Study and was supported by the findings of the Strategic Housing Land Availability Assessment (SHLAA) in terms of the land potentially suitable to provide for needs over the plan period. In setting out the Preferred Option there was recognition of the important role of the Main Rural Settlements, and also that insufficient land was identified within development boundaries to support the sustainable role of these settlements. It was further recognised, however, that careful consideration and work in partnership with local communities would represent the correct approach towards making appropriate allocations for development and illustrating that exceptional circumstances exist to release identified sites from the Green Belt.

1.54 Notwithstanding that the Preferred Option consultation did not identify specific site allocations, the Sustainability Appraisal prepared alongside this version nevertheless provided assessment matrices for each of the site options considered as reasonable alternatives in the urban and rural areas. This provides

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a consistent view of significant effects alongside relevant appraisal objectives, taking into account features such as location within the settlement hierarchy, distance from key services and indicators for the built, natural and historic environment (e.g. relationship to Flood Risk or Conservation Areas). 142 residential site options were assessed at this stage, with 97 being in the Rugby urban area or Main Rural Settlement. This stage represented an important contribution to plan-making.

- 1.55 The conclusions of the Housing Background Paper [LP11] explain that with regards smaller allocations, further submissions at Preferred Options 'Call for Sites' stage did not identify significant additional opportunities assessed as suitable for development within Rugby town or (given the distribution of other settlements) in sustainable locations within the rural area outside of the Green Belt. An additional 46 reasonable residential site options are nevertheless assessed in the Sustainability Appraisal for the Publication Local Plan [LP03] alongside those originally assessed at Preferred Option stage.
- 1.56 A total of 139 sites were considered based on those located in either Rugby Town or at Main Rural Settlements. The assessment framework at the Publication stage follows the same process as the 'Preferred Option' version and continues to highlight the distinction in significant effects between different sites and taking account of features such as the level of services in Local Needs Settlements.
- 1.57 Paragraph 3.84 of the Housing Background Paper illustrates how this process of considering alternatives also took into account all possible non-Green Belt options including deliverable brownfield sites, estate regeneration and surplus public-sector land (as well as optimising densities). These were all dismissed before considering the release of Green Belt locations and the lack of sustainable supply outside the Green Belt reinforces the case for exceptional circumstances. Relevant conclusions include, for example, that no sites were submitted within the boundaries of Main Rural Settlements (LP11 3.76) and only two (with a capacity of 17 dwellings) inside Local Need Settlements (Paragraph 3.81). This consideration of alternative options to distribute development supports the more detailed assessment of opportunities for sustainable development focused at the Main Rural Settlements tier.
- 1.58 Between the outcomes of the two stages of plan-making consultation referred to above, preparation of the Council's 'Development Packs' for each Main Rural

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Settlement comprehensively illustrate the synthesis of relevant evidence and process for site selection. The Development Packs take account of submissions to the Call for Sites in December 2015 as well as sites already contained by the 2015 SHLAA (LP10).

- 1.59 These are provided in the Examination Library under references: LP44, LP45, LP46, LP47, LP48, LP49 and LP50. The changes are shown on the Proposals Map under reference LP.54.193 and Brinklow's modified map is under reference LP.54.196.
- 1.60 The Development Packs provide a clear overview of the process for identifying 'reasonable alternatives' based on location and development capacity and utilise consistent assessment criteria (8 in total) alongside relevant evidence to provide conclusions on the appropriateness of development at given sites. Whilst many of the assessment criteria complement and overlap with those in the Sustainability Appraisal assessment framework the level of analysis in planning terms is significantly enhanced. This includes - for example - consideration of existing land uses, findings from the Green Belt Study and Landscape Character Assessment, deliverability and the options (where available) that may provide for an acceptable layout.
- 1.61 The Development Packs also provide planning judgement regarding the achievability of mitigation at both the site-specific and settlement level. This includes the outcome of consultation with statutory consultees and the local community through Parish Councils (prior to consultation on the Publication Draft). As part of this process the Development Strategy team met with Parish Councils following distribution of Main Rural Settlement packs of 'suitable' sites. This demonstrates that the assessment criteria were applied robustly and consistently across the tier of Main Rural Settlements.

e. Is Policy DS3 justified and effective in setting upper limits for the number of dwellings to be accommodated on each site?

- 1.62 The Council is of the view that the process of preparing its Strategic Housing Land Availability Assessment provides estimates of development capacity that provide a consistent and accurate reflection of a given site's suitability for development based on the opportunities and constraints assessed. However,

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these estimates may in some cases rely on a broad indicator of density and utilise the best information available at the time the SHLAA was produced.

- 1.63 In view of similar discussions at the stage 1 hearings, in order to display flexibility there would be no objection to substituting the words 'up to' within the policy with the word 'around' or 'about' to demonstrate that the figures are indicative. This can be accommodated in a proposed modification.

2. Is Policy DS6 justified and will it be effective in ensuring sustainable development at each of the proposed MRS sites?

- 2.1 DS6 as drafted will ensure that application proposals on allocated sites at Main Rural Settlements accord with the relevant principles supporting the allocation of each individual site.
- 2.2 The final sentence confirms that as part of the development management process sites will not be considered on Policy DS6 alone; each site will need to be considered against the plan as a whole, depending upon the impacts of each site and its individual characteristics e.g. in terms of landscape or housing mix. Providing sustainable development outcomes will also involve the application of material considerations in the NPPF as a whole. It is further important to note that for proposals that fundamentally depart from the principles of individual allocations (e.g. development beyond the boundaries of proposals maps) Very Special Circumstances would be required to support additional development.
- 2.3 The policy confirms it will be applied specifically in the context of individual allocations at Policies DS3.6 to DS3.14. The principles that support how sustainable development can be achieved have been considered in detail as part of the evidence base for preparing each allocation. The individual allocations are informed by an overview of the characteristics and requirements for development at each location, for example through estimates of development capacity and the specific allocation boundaries for Green Belt release.
- 2.4 More broadly, the allocations at DS3.6 to DS3.14 provide for proportionate and planned growth to meet need in settlements that have adequate levels of services ensuring that the sites have access to the facilities offered by each village. The sites have been selected partly as a result of their proximity to those services,

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considered against reasonable alternatives. Other sites were discounted because the sites that were selected were considered on the whole more suitable. Policy DS6 allows an assessment of this relationship at the development management stage.

- 2.5 Policy DS6 is appropriate in seeking a clear demonstration of how the principles supporting Green Belt release are set out through individual proposals on specific sites (particularly bullets 1 and 2). Bullets 3 to 7 look more broadly at whether development is able to secure the outcomes that will sustain and enhance the role of Main Rural Settlements and accord with the overall approach to supporting development at this tier of the settlement hierarchy.
- 2.6 In preparing Policy DS6, the Council recognised that consultation responses to the Publication Draft Local Plan (September 2016) provided relatively few representations that stated the Council was unable to demonstrate the exceptional circumstances to amend Green Belt boundaries. Furthermore, once specific allocations were proposed the responses of prescribed bodies indicated few 'showstoppers' to development or significant infrastructure requirements to prevent the development of sites in Main Rural Settlements. This is reflected, for example, in the Council's response to representations (e.g. to Stretton-on-Dunsmore Parish Council under ID:1397) to explain that no harmful effects not capable of mitigation are likely to arise in terms of Highways or Education. Policy DS6 (particularly the reference to viability assessment) seeks to ensure that sites remain viable and deliverable and are able to meet the overall requirements for development.
- 2.7 The Council recognises that more substantial evidence will be reflected in the work to prepare applications that will be reflected in, for example, development parameters and the relationship between gross and net developable areas. The Council also recognises that the response of consultees at the development management stage may reflect more detailed or up-to-date information than at the plan-making stage that will influence the specific requirements for mitigation or planning contributions. It is therefore important that Policy DS6 provides an opportunity for assessment of these matters. Finally, Policy DS6 emphasises the importance of community engagement for specific applications because the initial principles for development are themselves arrived at following extensive consultation. It is important to continue this role in the interests of effective plan-

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making. Accordance with Policy DS6 will demonstrate that proposals successfully maintain their relationship with and support the role of Main Rural Settlements.

Issue 4b: Introduction

3.1 Local Plan policy DS3: Residential Allocations proposes allocation in the Main Rural Settlements of Binley Woods, Brinklow, Long Lawford, Ryton on Dunsmore, Stretton on Dunsmore, Wolston and Wolvey and at Coton House. In answering issue 4b 1 below each MRS. However, this introductory should be read alongside each sub section of this answer.

3.2 Development Packs (LP44 – 49) have been produced for each Main Rural Settlement. These applied a framework for site selection against all of ‘reasonable alternatives’ SHLAA (LP10 and 10a) sites which were determined by size, location and the SHLAA 2015 and preliminary assessment of 2016 call for sites. The MRS considered each site in turn against the criteria in the ‘Approach to Site Selection’. The numbers of sites considered for each village are listed below:

Binley Woods - 8 sites considered. 1 site selected.

Brinklow - 11 sites considered. 1 site selected.

Long Lawford – 4 sites considered. 1 site selected.

Ryton on Dunsmore – 7 sites considered. 1 site selected.

Stretton on Dunsmore – 10 sites considered. 2 sites selected.

Wolston – 8 sites considered. 1 site selected.

Wolvey - 10 sites considered. 2 sites selected.

3.3 The MRS Development Packs were used in engagement with those Parish Councils where development was proposed in the Preferred Options Local Plan. This took place in Summer 2016 on the selection of the sites in advance of the Publication Consultation which took place from September 2016 to January 2017.

3.4 A comprehensive assessment of the Green Belt for Rugby Borough as part of a sub-regional assessment of the Coventry and Warwickshire Green Belt was undertaken in 2015. The study (LP30) divided land adjacent to Main Rural Settlements into parcels for assessment. The methodology that defined each parcel is contained in section 3 of LP30. The study sets out the five Green Belt purposes and the criteria used to assess the parcels together with notes as to how

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the judgements associated with each criterion were made. The study demonstrated that the majority of the Green Belt continues to serve its purpose well with defensible boundaries.

- 3.5 A table which can be found in appendix A of this statement contains a summary of the quantitative and a qualitative assessment on each of the purposes of the Green Belt provided in LP3 for each parcel where policy DS3 proposes development.
- 3.6 DS3 also proposes allocation at Coton House through DS3.1. This is addressed separately at the end of this statement.
- 3.7 With regard viability, the Local Plan Viability Assessment (LP22) considers sites of between 50 and 100 homes in main rural settlements (known as Test Area 2). It concludes in paragraph 3.4.46 that “relatively straightforward greenfield development (sub-strategic in nature) is generally amongst the more viable forms of development”. However, at paragraph 3.4.48 LP22 also confirms testing of previously developed sites.
- 3.8 The testing across both Rugby town and the rural areas led to a recommendation of a 20% affordable housing target for previously developed land and 30% for greenfield sites. This recommendation has been taken forward into policy H2 and will apply to applications for residential development at the Main Rural Settlements and Coton House.

Binley Woods

Issue 4b: Site Specific Issues for the MRS and Coton House Allocations

- 1. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and would they be consistent with national policy, which particular regard for the following for each site:**
- a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?**

- 4.1 Land at Sherwood Farm, Binley Woods (site reference DS3.6) is proposed for the development of up to 62 dwellings.
- 4.2 The site area for the DS3.6 allocation covers 4.7ha and sits in the south-eastern corner of parcel BW1 in the 2015 The Green Belt Review (LP30). This assessment

concludes that the parcel overall is mostly undeveloped and open, that the southwestern corner of the parcel is only 250 metres from Coventry and that there is presently no barrier stopping Binley Woods from encroaching into the parcel.

- 4.3 The comments above apply to the whole parcel of which the site only makes up a very small area in the south east of the parcel. Many of the buildings noted in the Green Belt review for the whole parcel are found specifically within the proposed site. This includes a brick built farmhouse and several agricultural buildings. These do have a greater impact on the openness of the site when compared to the impacts of existing development on the parcel as a whole.
- 4.4 The specific site is in the south eastern corner of the parcel, to the west of the vast majority of built development already existing in Binley Woods. Development on this side of the village would not impact the distance between Binley Woods and Coventry.
- 4.5 Additionally all of the sites submitted as part of the 2015, 2016 and 2017 SHLAA (LP10 and LP10a) for Binley Woods are in the Green Belt and therefore would require Green Belt release in order to allocate in this Local Plan.
- 4.6 The Green Belt Review (LP30) concludes that the parcel plays a medium role in fulfilling the purposes of the Green Belt and that there will be an impact if it were to be removed from the Green Belt. However, within this parcel it can be argued that the site plays a lesser role and therefore while there will be some impact to the Green Belt due to release of this site, this impact is less than the loss of the parcel in its entirety.

f. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable?

- 4.7 The map of parcel BW1 in appendix A of the LP30 shows the area of Ancient Woodland that runs along the north-eastern boundary of the site creating a clear new Green Belt boundary. To the south of the site runs Rugby Road and beyond this the wider village which is currently not within the Green Belt. To the east is a short piece of road which adjoins Rugby Road. This provides access to two properties and acts as the short eastern boundary to the proposed allocated site. While the property to the west of this road, Sherwood Farm, will form part of the allocation, this access will remain in place for 309 Rugby Road, the property to the

east of the road. As such the road will form a boundary between the allocated site and the Green Belt.

4.8 The line marking the western boundary of the site is currently a division between two fields. A consistent line of trees and hedges currently sit along this boundary. An indicative masterplan for the site provided by the agent in June 2016 shows this existing tree line being made wider, joining up to the ancient woodland to the north. Policies NE4 and SDC2 will provide Development Management Officers with the ability to ensure this landscaping is included through bullet points four and five. Additionally this will be further strengthened through the application of Policies DS5 and NE3 which ensure that new developments link into the GI network (as shown on LP02.19), which includes New Close Wood to the north of the site. This therefore will ensure a clearly defined Green Belt boundary to the west of the site.

4.9 The Green Belt Review (LP30) provides examples of strong and weaker Green Belt boundaries. The strongest boundaries are features such as dual carriageways and rivers. In this instance the Ancient Woodland, with its national level of protection could be seen to be a strong boundary. The road and tree line would be a weaker boundary but given that at present the boundary is along Rugby Road the strength of the new boundary is not significantly weaker.

g. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities?

4.10 Landscape Character- The site is situated within the Dunsmore Parkland character type (from LP34 Landscape Assessment 2006). The assessment states that this character type is generally in decline with the area in which Binley Woods is situated being of high overall sensitivity. Dunsmore Parkland is characterised by its well-wooded character tree and hedge lined streams and roads. The Sustainability Study (LP03) acknowledges that there is the potential for a small negative impact on the landscape but that this could be mitigated.

4.11 The Ancient Woodland to the north east (New Close Wood) and an almost unbroken line of hedging and trees along Rugby Road on the South of the site fit within these characteristics. Based on advice from WCC Ecology, provided at pre-application stage, a 30 metre buffer has been proposed along the length of the

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Ancient Woodland in the June 2016 indicative plan for the site. TPOs protect the row of trees and hedges along Rugby Road.

- 4.12 There is opportunity to improve the sensitivity of the landscape through tree planting which is proposed in the indicative masterplan for the site. Compliance with Policies NE3, NE4, DS5 and SDC2 will ensure that this tree planting is included to provide adequate landscaping and Green Infrastructure Links to New Close Wood.
- 4.13 Heritage- Binley Woods does not have a conservation area. The closest conservation area to the site is one covering part of the village of Brandon. This is situated over 1km from the edge of the site. The land immediately south east of the site is higher than the site itself, this is an area known as Brandon Hill. Beyond this moving into the village of Brandon where the conservation area is situated, the land then drops down by over 10m in height. The position of Brandon Hill between the site and the conservation area means that development on this site is unlikely to have any significant impacts on the conservation area or its setting.
- 4.14 There are no listed buildings, registered parks or gardens or other designated historic assets in the village itself. As with the conservation area the closest listed buildings are found in the village of Brandon.
- 4.15 The 2016 Heritage Asset Review (LP38) focused on the potential impacts of development of the site on the surrounding heritage assets. It stated that there were three assets which could be affected; Old Lodge Farmhouse and Barn which are both individually grade 2 listed and are situated around 0.5km from the northern boundary of the site and the grade 2* Registered Park and Garden surrounding Coombe Abbey, the edge of which is around 1.2km from the northern edge of the site. The review acknowledges that there will be some impact on the Farmhouse and Barn with some limited views of any new housing and a potential urbanising effect on the surrounding landscape. The tree lined avenue is deemed to be the key feature with the Registered Park and Garden and given that there are no longer trees along the remaining track the review states that there would not be any significant impacts from development of the site. The review goes on to conclude that appropriate screening of the development from the north-west would allow housing development to be appropriate from a historic environment perspective.

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- 4.16 Policy SDC3 looks to ensure that heritage assets are conserved and protected and outlines the requirements for the application to ensure a full assessment and justification of the proposal are provided. This is in line with NPPF paragraphs 128 – 134 which provides a clear process for dealing with application that may affect heritage assets.
- 4.17 Biodiversity- According to the Habitat and Biodiversity Audit 2017 (LP41) the only designated biodiversity assets in close proximity to the site is the Ancient Woodland New Close Wood which adjoins the site to the north and east. The Sustainability Appraisal (LP03) does acknowledge that there could be a negative impact on biodiversity but that there is potential for this to be mitigated.
- 4.18 Guidance was provided to the agent in 2016 from Warwickshire County Council Ecology team which stated that a minimum buffer of 30m was required to be left in a semi-natural state which meant it could not include gardens or any hard surfacing. This would help to prevent both chemical pollution and physical damage to the woodland and help to maintain connectivity between the woodland and the wider countryside. The requirement was back up by a Woodland Trust report. Compliance with Policy SDC2 which requires features of ecological significance to be protected will ensure protection of the Ancient Woodland in line with the latest guidance. This has been acknowledged by the agent who has included a 30m buffer on their 2016 indicative masterplan for the site.
- 4.19 Policy NE2 specifically addresses biodiversity and further strengthens the requirement in SDC2 by requiring biodiversity assets to be protected or enhanced with a net gain in biodiversity highlighted as a desired outcome of development.
- 4.20 Further out from this site, Coombe Pool is a SSSI located to the north west of the village. DEFRA Magic Maps online and the associated guidance document released in October 2017 concludes that the site is unlikely to pose a risk to SSSI as it consists of development of less than 100 dwellings.
- 4.21 In addition the Green Infrastructure map for the Borough (LP02.19) shows the area of Ancient Woodland to sit within the Green Infrastructure Network. By extending this network into the site the development will comply with Policy NE3 which seems to enhance Green Infrastructure assets. The indicative masterplan provided by the agent in 2016 shows an area in the west of the site is to be planted with vegetation. As well as helping to mitigate the impacts on and landscape and

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heritage assets this will contribute to the enhancement of the Green Infrastructure network in the village.

4.22 Agricultural Land- The site is situated on grade 3 agricultural land. The whole village is covered by the same grade 3 agricultural land classification. If the land is classified as 3a this would fall into the definition of the best and more versatile agricultural land. While land in agricultural land classifications 1 or 2 is valued more than 3a, it is acknowledge that loss of land in agricultural land classification 3a cannot be mitigated and therefore would be lost were development to go ahead on the site. However, in line with NPPF paragraph 112 it has been illustrated that the use of this land is necessary to ensure that the identified housing need, and more specifically the five year supply of housing, for Rugby is met sustainably.

4.23 Flood Risk- The entirety of the site is in Flood Zone 1 and therefore complies with paragraph 100 of the NPPF which seeks to direct development away from areas at highest risk.

4.24 Highway Safety- A new access to the site is proposed via a new junction of the A428 Rugby Road, just to the west of the existing Sherwood Farm residential dwelling. The Highways Authority have stated that they would have no objection to the site being considered for development providing an adequate level of visibility can be achieved. Work to achieve visibility splays would need to consider the TPOs along Rugby Road.

4.25 Infrastructure & Facilities - The Rural Sustainability Study (LP28) shows Binley Woods is well served by local facilities with a village hall, village shop, post office, pharmacy and primary school among the essential services and a pub, a restaurant, a mobile library, place of worship, recreational open space, dentist and petrol station among the non-essential services. There is no doctor's surgery in the village but there is access to surgeries in Wolston and Coventry.

4.26 Education - Warwickshire operates a system of priority areas for school admissions. This system will only come into play when a school is over-subscribed.

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- 4.27 If a place is available in a school, irrespective of whether or not the child lives within the priority area, then a child can be admitted. Schools will often be at or close to capacity but because of parental choice, this does not mean that the school will have filled from in area.
- 4.28 At times of growth, children joining school in reception from within the priority area will, as the name suggests, be given preference for admission from those outside the area. However, for older children moving into an area of growth it can be a little more difficult for pupils to gain a place at their local school.
- 4.29 Infant class size regulations apply at Years 1 and 2, restricting the number of pupils to a teacher. In other year groups though such limits don't apply and it isn't unusual for a school to admit above PAN to allow an older child moving into the area to access the school. It is also possible for a parent to lodge an appeal to secure a place at a school.
- 4.30 Binley Woods geographic location close to Coventry means that a number of out of county children have secured places at the school in the past. This is a result of parental preference and the school having spaces available. Of the 205 pupils on roll in October 2017 approximately 60% were from within the school's own priority area with the remaining 40% made up with pupils from elsewhere in the Borough as well as almost 50 children from across the border in Coventry.
- 4.31 Based on current pupil yield indicators Warwickshire County Council (WCC) would estimate 62 homes generating in the region of 20 additional primary age pupils. With children from the proposed development site falling within the priority area for the school, WCC do not envisage a problem with the school being able to cater for the impact of the proposed housing.
- 4.32 At secondary age, pupils would receive free home to school transport into the town as is currently the case for other secondary age pupils from Binley Woods. For the purpose of clarity, the impact of additional housing in the Main Rural Settlements on secondary school places has been included in the figures presented in the education note (OTH/038).
- 4.33 The 86 bus route currently runs between Coventry and Rugby. During the week (Mondays to Fridays) and on Saturdays the 86 service operates on an approximately half hourly basis between 09.00 and 18.00, with an hourly evening service which ends at 21.30. There is a limited Sunday service which operates between 09.00 and 17.00 on a two hourly basis. The proposed development will

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bolster existing patronage on this service, but also would support a potential improvement to evening and Sunday bus service provision. The Sustainability Appraisal (LP03) scores a strong positive for service and facilities for the site.

h. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

4.34 Development on the site will be adjacent to existing residential development along Rugby Road and opposite residential properties. The housing on the opposite side of the road continues further east than the boundary of the site and therefore development on the site will not be the most easterly in the village.

4.35 The Binley Woods development pack (LP44) states that other than the surgery the post office is the further from the site at 610 metres. This means that all of the essential services are within a ten minute walk of the site. As such the site is well connected to the existing settlement.

i. The evidence to support the sites 'deliverability' as defined in footnote 11 of the NPPF?

4.36 The Housing Trajectory (as appended to LP11) shows 30 homes being complete in 2020-21 and the final 32 homes complete in 2021-22. The site is primarily greenfield with some agricultural buildings in place on the site which will be demolished prior to construction on site. These are limited in number and basic in structure and therefore are not deemed to impact deliverability. The site could only be developed after the adoption of the Local Plan owing to its greenbelt status.

4.37 Indicative plans have been produced for the site, these have taken into account information provided by WCC Ecology and incorporate a green buffer alongside the Ancient Woodland. The agent has stated their intention to carry out full pre application discussions within the next six months with a full application likely to be submitted prior to the Local Plan adoption.

4.38 There is already a developer on board; Lioncourt Homes. Supporting documentation is proposed to be submitted with the statement from Pegasus. As

such the site is deemed to be deliverable and the timescales in the housing trajectory reflect the latest position.

j. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements?

4.39 The site is greenfield. Demolition required is only for very simple agricultural structures. Other than onsite infrastructure including roads, access and SuDs there are no major infrastructure costs for the site. The viability testing (LP22) suggests greenfield sites are among the most viable with those closer to 50 dwellings the most viable compared to those that are closer to 100. At 62 dwellings this sits toward the lower value. The testing at 30% affordable housing showed this to be viable for greenfield sites.

Brinklow

1. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and would they be consistent with national policy, which particular regard for the following for each site:

a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?

5.1 Land off Lutterworth Road, Brinklow (site reference DS3.7) is proposed for the development of up to 100 dwellings.

5.2 The site area for the DS3.7 allocation covers 7.2ha and sits in the north western corner of parcel BR2 in the 2015 The Green Belt Review (LP30). A table which can be found in appendix A of this statement contains a summary of the quantitative and a qualitative assessment on each of the purposes of the Green Belt provided in LP30 for this parcel. This assessment concludes that the vast majority of the parcel is open and free from development, that the village of Brinklow is only 1.8km from the neighbouring village of Stretton under Fosse and that the M6 to the north and the railway line to the east help prevent encroachment

of the parcel into the wider countryside but these are a considerable distance from the edge of the village.

- 5.3 The comments above apply to the whole parcel of which the site only makes up a very small area in the north west of the parcel. Directly opposite the site, on the other side of Lutterworth Road are a row of residential properties that are situated within the Green Belt which do have a more localised impact on the openness. While the distance to the M6 and railway line will be decreased there are more localised natural boundaries to the site itself include the Oxford Canal and the Smite Brook. These are discussed in more detail in the response to 1b.
- 5.4 All of the sites submitted as part of the 2015, 2016 and 2017 SHLAA (LP10 and LP10a) for Brinklow are in the Green Belt and therefore would require Green Belt release in order to allocate in this Local Plan.
- 5.5 The Green Belt Review (LP30) concludes that the parcel plays a medium role in fulfilling the purposes of the Green Belt and that there will be an impact if it were to be removed from the Green Belt. However, within this parcel it can be argued that the site plays a lesser role and therefore while there will be some impact to the Green Belt due to release of this site, this impact is less than the loss of the parcel in its entirety.

b. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable?

- 5.6 The site is bounded to the north by the Smite Brook which would form a new natural Green Belt defensible boundary. In addition to this, the north of the site, to the south of the Smite Brook is an area in Flood Zones 2 and 3 which will limit development to the north of the site. An indicative masterplan submitted as part of a landscape assessment of the site by the agent in March 2016 shows no development in the 65 metre strip to the north of the site which covers this area.
- 5.7 To the west is Lutterworth Road, the main road running through the village which again would form a new boundary.
- 5.8 To the east and south are the Oxford Canal and Canal towpath. Attention was drawn to the importance of the Canal in the representation made to the pre-submission consultation by the Canal and Rivers Trust. To the south the Canal towing path is surrounded on both side by trees and hedges with Tree Protection

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Orders and to the west is an area of forested land sitting to the east of the Canal. The site boundary is around 100m to the west of the Canal which protects the area of trees. As such between the vegetation and the canal there is a well-established site boundary that can form a clear new Green Belt boundary both to the east and south of the site.

5.9 As for Binley Woods policies DS5, NE3, SDC2 and NE4 will ensure that landscaping and Green Infrastructure links are carefully considered to support the creation of clear, defensible boundaries.

5.10 The Green Belt Review (LP30) provides examples of strong and weaker Green Belt boundaries. The strongest boundaries are features such as dual carriageways and rivers. In this instance the new boundaries would fall within the weaker category but the canal to the east is a potential local wildlife site, providing some additional protection and the heritage assets to the south including the Morgan canal bridge and Brinklow Castle also have national level protection which support the strength of the boundary.

c. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities?

5.11 Landscape Character- The site situated within the Dunsmore Parkland character type (from LP34 Landscape Assessment 2006). The assessment states that this character type is generally in decline. Dunsmore Parkland is characterised by its well-wooded character and tree and hedge lined streams and roads.

5.12 The Landscape Assessment provided by the agent in June 2016 provides an assessment of the topography of the village and wider area and shows that the land to the north and south of the site is higher than that of the site itself. This will decrease any impacts on the wider landscape and will soften views of the site from the remainder of the village to the south. In line with this the Landscape Sensitivity Study 2016 (LP35) states that residential development on the site would be appropriate providing there is an adequate landscape buffer around the perimeter and that boundary hedgerows and trees are protected. The trees surrounding the cemetery are also noted to provide a strong sense of visual continuity.

- 5.13 There are natural landscape buffers along the east and southern boundaries of the site. The eastern site boundary sits around 100m to the west of the Canal which protects an area of trees which sit to the west of the Canal. The Brinklow Arm of the Canal runs to the south of the site. The Landscape Sensitivity Study 2016 (LP35) states that this has a continuous vegetated corridor including strong tree cover. It also notes that this is a potential Local Wildlife Site. The trees and hedgerow along the southern site boundary are protected with TPOs.
- 5.14 The Sustainability Assessment (LP03) acknowledges that there will be a small negative impact to the landscape but that there is scope for mitigation. There is opportunity to improve the sensitivity of the landscape through tree planting which is proposed in the indicative masterplan for the site. The trees around the cemetery are shown to be protected in the indicative masterplan with extra vegetation planted around the cemetery to create a larger vegetation buffer.
- 5.15 Compliance with Policies NE3, NE4, DS5 and SDC2 will ensure that adequate landscape mitigation is provided.
- 5.16 Heritage- Brinklow village has a designated conservation area covering the core historic area of the settlement. At its closest point the site sits around 200m from the edge of the conservation area. The 2016 Heritage Asset Review (LP38) identifies the conservation area as one of the heritage assets which requires consideration due to the prominent position within the village and their relationship to the subject area. Each relevant asset is listed below with a brief summary of the anticipated impacts to the asset of development of the site.
- Brinklow Motte and Bailey Castle- Schedules Ancient Monument- This is the highest point above the village with views across the countryside. This would be changed by the introduction of housing which urbanises the environment. This harm would be less than substantial and there is potential for mitigation to avoid an objection on conservation grounds.
 - Church of St John the Baptist- Grade 2* listed & heritage at risk- The church provides views of the surrounding landscape. Its' 'at risk' status would not be further impacted by the development. Due to the existing screening of the

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boundaries of the church the development of the site would not cause harm to the assets significance.

- Morgans Bridge- Oxford Canal- Grade 2 listed- There are views into the site from the bridge which mean that development will affect the significance of the bridge. This harm is deemed to be less than substantial.
- Brinklow conservation area- The conservation area represents the historic core of the settlement and its setting would be affected by the development of the site. The conservation area would become sandwiched between two modern housing sites losing its connection to the surrounding rural area.

5.17 The review concludes that screening and the site layout have the potential to provide some mitigation against the impacts but it does acknowledge that some impacts will not be able to be mitigated. The Sustainability Appraisal (LP03) gives the site a small negative impact with the potential for mitigation and is therefore in line with the results of the review.

5.18 The Landscape Assessment submitted by the agent in March 2016 acknowledges the close proximity of the Motte and Bailey, Brinklow Castle and the Brinklow Conservation Area illustrating that the agents are aware of the heritage considerations required to make the development acceptable. In terms of providing opportunities to improve the setting of heritage assets, the Landscape Assessment submitted by the agent in March 2016 highlights the opportunity to create a circular footpath connecting the scheduled ancient monument to the existing public footpath which runs along the Oxford Canal on the east and the river to the north. This could tie in with the recommendations in the Brinklow Conservation Area Appraisal (available on the Rugby Borough Council website) which recommends the establishment of a management plan for the castle.

5.19 Policy SDC3 looks to ensure that heritage assets are conserved and protected and outlines the requirements on the application to show a full assessment and justification of the proposal are provided. This is in line with NPPF paragraphs 128 – 134 which provides a clear process for dealing with application that may affect heritage assets. As part of meeting the requirements of these policies the proposal will need to ensure sufficient mitigation is provided.

- 5.20 Biodiversity- There are no biodiversity designations in or close to the village of Brinklow other than the Potential Local Wildlife Site along the Oxford Canal to the east of the site as identified in the Landscape Sensitivity Study 2016 (LP35). The site itself doesn't directly adjoin this Potential Local Wildlife Site; the eastern site boundary sits around 100m to the west of this leaving a buffer of undeveloped land between the two.
- 5.21 The Sustainability Assessment (LP03) has stated that there is a potential negative impact on the biodiversity and geodiversity of the site but that there is potential for this to be mitigated to a degree. The agent has proposed that the Public Open Space which will primarily be situated in the area of flood zones 2 and 3 to the north of the site could be used to provide biodiversity enhancements and maintain the Green Corridor function of the land. Policy NE2 specifically addresses biodiversity and further strengthens the requirement in SDC2 by requiring biodiversity assets to be protected or enhanced with a net gain in biodiversity highlighted as a desired outcome of development and therefore this approach would be compliant with this policy.
- 5.22 In terms of Green Infrastructure the Green Infrastructure Map (LP02.19) shows the network running across the north of the village taking in the Oxford Canal and the site. By extending this network into the site the development will comply with Policy NE3 which seems to enhance Green Infrastructure assets.
- 5.23 Agricultural Land- The site is situated on grade 3 agricultural land. The whole village is covered by the same grade 3 agricultural land classification. If the land is classified as 3a this would fall into the definition of the best and more versatile agricultural land. While land in agricultural land classifications 1 or 2 is valued more than 3a, it is acknowledge that loss of land in agricultural land classification 3a cannot be mitigated and therefore would be lost were development to go ahead on the site. However, in line with NPPF paragraph 112 it has been illustrated that the use of this land is necessary to ensure that the identified housing need, and more specifically the five year supply of housing, for Rugby is met sustainably.
- 5.24 Flood Risk- The majority of the site is situated in Flood Zone 1. A band around 60m in width at the very north of the site is in Flood zones 2 and 3. This areas forms the flood plain of the Smite Brook. The Sustainability Appraisal (LP03)

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acknowledges that the site does have flooding issues. As such rather than mitigation the agent has produced an indicative masterplan (provided in March 2016) which looks to avoid development in the areas in Flood Zone 2 and 3. The masterplan shows a 65m buffer at the north of the site which incorporates all of this Flood Zone 2 and 3 land and includes a further area of Flood Zone 1 land. This buffer is proposed to remain as open space with no built development. Furthermore the council would consult both the Environment Agency and the Local Lead Flood Authority on any application on the site which would be also be subject to SDC5.

5.25 Highway Safety- Indicative plans illustrate that there will be two accesses into the site, both off Lutterworth Road. The most northern entrance would be 70 metres from the change of speed limit from 30mph to 50mph. The Highways Authority indicated that an assessment of highway safety would be undertaken as part of the assessment of a planning application and that a change of the speed limit down to 30mph could be undertaken if necessary.

5.26 Infrastructure & Facilities- The Landscape Assessment provided by the agent in March 2016 indicates a parcel of land to the north of the cemetery could be gifted to the Parish Council to act as an extension to Brinklow Cemetery.

5.27 The Rural Sustainability Study (LP28) listed the essential facilities located in the village of Brinklow. These were a village hall, village shop, post office and doctor's surgery. There is a Primary School in the village but this is part of The Revel, a school split between three local villages. The reception class is based in Brinklow with years 1-6 being taught at the main school site in Monks Kirby. Eden Park (Gateway) currently falls within the priority area for The Revel, however with a proposed new Primary School opening at Eden Park in September 2021 or 2022, priority areas are likely to change and/or patterns of attendance alter to a certain extent.

5.28 Of the 305 children on the roll at The Revel in October 2017, 207 were from within the school's priority area with the remainder travelling from elsewhere in the Borough. Based on current pupil yield indicators we would estimate 100 homes generating in the region of 31 additional primary age pupils. With children from the proposed development site falling within the priority area for the school,

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Warwickshire County Council do not envisage a problem with the school being able to cater for the impact of the proposed housing. In addition, with the proposed new primary school at Gateway, local growth could be of benefit in maintaining pupil numbers at The Revel.

5.29 At secondary age, pupils would receive free home to school transport into the town as is currently the case for other secondary age pupils from Brinklow. For the purpose of clarity, the impact of additional housing in the Main Rural Settlements on secondary school places has been included in the figures presented in the education note (OTH/038).

5.30 As well as the essential facilities Brinklow hosts numerous non-essential facilities which would be better supported with the addition of new residents. This includes more than one pub, a café, a mobile library, a church, recreational spaces and an early years nursery. All of the essential and non-essential services will be better supported with more users.

5.31 The 585 bus service, operated by De Courcey Travel, runs from Coventry to Rugby 7 days a week. During the week the service operates on an half hourly service during the hours 08.30 and 14.30, after which it reduces to an hourly service until the last service at 20.00. A Saturday service operates on an hourly basis between 09.00 and 19.15. A Sunday service operates with a two hour frequency between 09.00 and 19.00.

5.32 More people using this service would assist in maintaining the service and improve the evening and weekend provision. The Sustainability Appraisal (LP03) scores a strong positive for service and facilities for the site.

d. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

5.33 The sensitive historic core of Brinklow village has shaped the settlement in its current form with developed running up along Lutterworth Road. The site, if developed, will contain the most northerly housing in the village. However, there are houses almost directly opposite the site on the western side of Lutterworth Road and the Development Pack for Brinklow (LP45) states that the furthest essential service from the site within the village is the post office at 700m in

distance which is with a ten minute walk time. The site already connected to pavements that run through the village.

5.34 In addition there is an existing public footpath network that runs around the periphery of the site meaning that the site sits within this existing village infrastructure. As such the site is well connected with the main village.

5.35 It is also important to note that of the 11 sites assessed this was the only one to be deemed suitable for development.

e. The evidence to support the sites 'deliverability' as defined in footnote 11 of the NPPF?

5.36 The Housing Trajectory (as appended to LP11) shows 50 homes being complete in 2020-21 and the final 50 homes complete in 2021-22. The site is greenfield with no built development present meaning no demolition is required.

5.37 Initial assessments on landscape, heritage and indicative layouts have been produced and were submitted as part of the submission to the pre-submission consultation. Further studies are being produced to be submitted alongside a statement from the site promoter to include landscaping, highways, trees, topography and drainage. The site could only be developed after the adoption of the Local Plan owing to its greenbelt status. The site promoter has stated they expect an application to be submitted in line with the Local Plan process. As such the site is deemed to be deliverable and the timescales in the housing trajectory reflect the latest position.

f. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements?

5.38 The site is greenfield. Other than onsite infrastructure including roads, access and SuDs there are no major infrastructure costs for the site. The viability testing (LP22) suggests greenfield sites are among the most viable. The testing at 30% affordable housing showed this to be viable for greenfield sites.

Long Lawford

1. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and would they be consistent with national policy, with particular regard to the following for each site:

a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?

6.1 Land North of Coventry Road, Long Lawford (site reference DS3.8) is proposed for the development of up to 100 dwellings. The 6.5 hectare site is situated within the West Midlands Green Belt circa 1.9 miles from Rugby Town Centre.

6.2 4 sites in Long Lawford were assessed as part of the Long Lawford Development Pack. Land North of Coventry Road was the only site deemed suitable for allocation owing to the other sites constraints around access and flood zone designations.

6.3 LP30 made a full assessment of DS3.8 under the reference LL2. The Joint Green Belt Review (LP30) concludes at paragraph 4.28 that the site has been identified as a low performing green belt parcel which makes a less than significant contribution to the green belt. The study identifies that low performing parcels such as LL2 could be considered for greenbelt removal, subject to wider NPPF considerations relating to housing allocations.

6.4 Appendix A sets out the results of the JGBR and scoring in detail against the proposals of the green belt as set out in Paragraph 80 of the NPPF. In summary, site LL2 does not fulfil the purposes of the green belt owing to development on its Eastern elevation limiting its contribution to preventing coalescence, the site has a clearly recognisable southern boundary, does not contribute to the setting and special character of Rugby and all sites contribute to urban regeneration, it is not possible to isolate the effect of a single parcel.

6.5 Subject to the parcel being removed from the greenbelt on the basis of the Joint Green Belt Review, NPPF Paragraph 80 would no longer be applicable to the development of the site.

b. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable?

6.6 LP30 considers the significant boundaries of the site in relation to the assessment of NPPF Paragraph 80 Purpose 3 (To assist in the safeguarding of the countryside from encroachment).

6.7 The assessment identifies that the site is bordered by residential development to three sides. To the southern edge is the Coventry Road (an A Road) with open countryside beyond. LP30 considers the Coventry Road to represent a significant boundary preventing further encroachment to the south of the site. The Joint Green Belt Study Paragraph 3.8 identifies that significant man-made features- such as A Roads- are “readily recognisable and likely to be permanent and, therefore, potentially suitable for delineating Green Belt boundaries.” (Green Belt Review, Page 9).

6.8 Furthermore, there is hedging along the southern edge of the parcel with the Coventry Road, as such there are both physical features considered to be both permanent and less permanent defining the site. LP30 Paragraph 3.9 of the study identifies “...hedgerows and tree lines...” to be recognisable but less permanent boundaries.

6.9 With reference to LL2, LP30 concludes that “Development in these locations would effectively be ‘infill’ and would be well contained by existing significant features and the landscape.” (Green Belt Study, P.27). As such, it is considered that the site has clearly defined physical features that are readily recognisable.

c. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities?

6.10 Landscape - LP35 Landscape Sensitivity Study 2016 made a full landscape character assessment of the site on Page 103 (reference number LL_11). The study identifies the site as being of “medium sensitivity”. The study identifies “medium sensitivity” as having “...potential to accommodate relevant type of development in some situations without significant character change or adverse impacts”.

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- 6.11 The site was originally identified in the Long Lawford Development Pack (LP46) as being of high landscape sensitivity. However, this has been downgraded in conjunction with a habitats assessment of the site concluding that the site is not a Local Wildlife site and is therefore less sensitive. This is set out in LP46.
- 6.12 The site lies within National Character Area 96 Dunsmore and Feldon and at County level the Dunsmore local landscape types as defined within the Warwickshire Landscapes Guide (1993). The site is considered to consist of small scale pastoral landscape with some remnant ridge and furrow. Outgrown hedgerows and dense hedges/stream trees generally enclose views. Views of the site are limited owing to the relatively flat topology within the site itself and around the immediate vicinity of the site. To the south views are only as far as the first (adjacent) field as rising landform prevents any further views.
- 6.13 LP35 identifies that development could be accommodated providing a landscape buffer is included along the Coventry Road (LP35, P.103). This is considered to support the development of the site, subject to the aforementioned mitigation. Therefore landscape character is not considered to be a constraint to the development of the site.
- 6.14 Whilst separate from the Local Plan process, a Planning Application for 153 dwellings submitted for the site (reference number R17/1089). R17/1089 does include a proposed green buffer which serves to illustrate this point.
- 6.15 Heritage - DS3.8 is not within a Conservation Area, there are no heritage assets within the site or immediately adjoining the site. To the South Western edge of the site, on the opposite side of the Coventry Road, sits the Grade II Listed Avon Lodge.
- 6.16 The Heritage Asset Review (LP38) provides a full assessment of the heritage assets in all of the MRS proposed for extension. The conclusions of the assessment for DS3.8 are contained in LP38 pages 15-17. The assessment states that harm from development could be considered 'less than substantial' but significant enough to warrant an objection from a heritage perspective.
- 6.17 In terms of the assets significance and contribution, the assets value is identified by LP38 as being greatest within the immediate surrounds which frame it. In particular, key to understanding the asset is its relationship with the nearby crossroads at Coventry Road and Lawford Heath Lane. Specifically, the potential

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impact of development on significance has been identified as a potential loss of context and isolation for the asset.

6.18 DS3.8 is situated to the North East of Avon Lodge, which is the opposite direction to the crossroads from which the site derives its significance and thus enjoys a degree of separation from Avon Lodge itself. This physical separation and site orientation is considered to mitigate potential impacts, especially within the context of existing residential development to the North of Avon Lodge. The heritage asset study states that the assets context and isolation has already been affected by existing residential development to the North of the asset on the Coventry Road.

6.19 LP38 identifies potential mitigation through the positioning of open space or natural grassland to the South West of the site, maintaining openness around the heritage asset. It is considered by the LPA that the potential impact is considered far less significant than were the site directly adjoining the asset or situated to the south of the Coventry Road.

6.20 Planning Application R17/1089 includes open space positioned to the South West of DS3.8 and the Planning Application has been subject to consultation with statutory heritage bodies. Therefore it is considered that final details could be resolved through the development management process and therefore the asset is not considered a constraint on the development of DS3.8.

6.21 Biodiversity - Local Plan allocation sites have been subject to assessment by the Warwickshire County Council Habitat and Biodiversity Audit (LP41). The development of the site is not considered to affect any biodiversity assets.

6.22 The site was previously identified as a potential Local Wildlife Site (LWS). However, the assessment undertaken found the site as having limited meadowland diversity value remaining, in common with the site to the North (Long Lawford meadows) which had lost biodiversity value owing to over grazing and nearby development. The assessment concluded the site was not a LWS.

6.23 Furthermore, a full range of Ecological surveys have been submitted with the Planning Application. The current Planning Application would be subject to consultation with Warwickshire County Council Ecology, so the Development Management process has a degree of control should any mitigation measures be

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identified. The results of Ecological consultation responses to the Planning Application are not yet known.

6.24 Agricultural land – LP46 identifies the land as Grade 3 agricultural land. The site has recently been in agricultural use for cattle grazing.

6.25 If the land is classified as 3a this would fall into the definition of the best and more versatile agricultural land. While land in agricultural land classifications 1 or 2 is valued more than 3a, it is acknowledge that loss of land in agricultural land classification 3a cannot be mitigated and therefore would be lost were development to go ahead on the site. However, in line with NPPF paragraph 112 it has been illustrated that the use of this land is necessary to ensure that the identified housing need, and more specifically the five year supply of housing, for Rugby is met sustainably. Further, the loss of agricultural land has to be viewed in the context of reasonable alternatives in Long Lawford as detailed in LP46 and the strategic objectives of the Local Plan. The site is within a highly sustainable location in relative close proximity to the urban area and a wider range of local services.

6.26 Therefore it is considered that the strategic benefits of the allocation outweigh the loss of agricultural land.

6.27 Flood risk - The entirety of the site is in Flood Zone 1 and therefore complies with paragraph 100 of the NPPF which seeks to direct development away from areas at highest flood risk.

6.28 There is a small brook running through the centre of DS3.8. The development site to the North (R12/1188) has incorporated the Northern section of this brook in to the schemes design, as is proposed in the Planning Application submitted for DS3.8 under R17/1089. A flood risk assessment has been submitted with Planning Application R17/1089.

6.29 Therefore the site is not known to contain any significant flood risks and any potential impact will be assessed through the Development Management process.

6.30 Highway safety - Warwickshire County Council Highways were consulted as part of the SHLAA process. LP46 details that the scale of development proposed would require multiple access points and improvements to existing footpaths. It

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was also considered that an access could be possible from the A428 although concerns were raised that there would be potential problems with visibility and frontage access due to the alignment of the carriageway.

- 6.31 A development of 112 dwellings to the North of the site (R12/1188) is currently under construction and includes a direct vehicular access into R17/1089.
- 6.32 A new junction design has been submitted with Planning Application R17/1089. This proposes diverting the access to Back Lane from the Coventry Road, effectively removing the crossroads Coventry Road/Back Lane/Bilton Lane Crossroads. The effect of this is that vehicles turning out of Bilton Lane will have a greater degree of separation from vehicles turning from Back Lane.
- 6.33 An assessment of the junction proposals will be made as part of the development management process, through consulting Warwickshire County Council Highways. In terms of the Local Plan allocation, the information submitted with the Planning Application cannot be seen as evidence that the highways work is submitted is evidence that highways constraints can be overcome given that is a development management issue. However, the development of the site to the North of the proposed allocation site- as well as development the other side of Back Lane- is considered to demonstrate that highways infrastructure within the immediate vicinity could be provided in principle. When combined with the initial highways assessment that access to the site is possible, there is no evidence before the Council to suggest that highway safety would be compromised.
- 6.34 Infrastructure and Facilities - The site is considered to enjoy a strong spatial relationship to a full range of infrastructure and facilities within Long Lawford itself and the Rugby Urban Area. LP28 (Rural Sustainability Study) outlines the facilities available within Long Lawford whilst LP46 (Main Rural Settlement Development Pack) identifies the distances to local facilities.
- 6.35 LP46 identifies the site as being within walking distance of all village services with the furthest service (the local Primary School) still being within a ten minute walk and all other services within 5 minutes.
- 6.36 An assessment of Long Lawford's school provision carried out by Warwickshire County Council Education has concluded that there would be sufficient capacity to accommodate the estimated 31 additional primary age pupils a 100 home development is likely to generate. Long Lawford's primary school is a

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two form entry school and is undergoing further expansion to become a three form entry school, and is therefore assessed as having the capacity for this additional growth.

6.37 As aforementioned, a Planning Application has been submitted for the site which proposes substantial highways alterations to mitigate any potential highways impacts. Therefore this will be resolved through the Development Management process.

6.38 Consultation with stakeholders such as Warwickshire County Council as part of the Local Plan process has not identified any constraints on local facilities. Further detail is provided in question 'd'.

d. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

6.39 DS3.8 is situated along the southern edge of the current settlement boundary. The site is considered to have a strong relationship to the existing settlement owing to the relative close proximity of local services and facilities. This is evidenced through the LP46 Addendum, which provides a detailed breakdown of the walking distances to local services and facilities. Long Lawford lacks a defined settlement centre and as such key services are dispersed throughout the village, but LP46 details how DS3.8 still performs strongly when assessed relative to reasonable alternatives.

6.40 LP28 Rural Sustainability Study identifies Long Lawford as a 'top tier' Main Rural Settlement and as such it is amongst the most sustainable rural locations within the Borough owing to having a wider choice of services than other settlements. The study identifies how whilst Long Lawford has fewer services than Dunchurch, it is closer to Rugby Town Centre and public transport to Rugby and Coventry.

6.41 Long Lawford is in relative close proximity to the Rugby Urban Area (circa 800metres) and Rugby Town Centre itself (circa 1.9 miles). DS3.8 is physically linked to the Rugby Urban Area by public footpaths and is well served by public transport. Long Lawford has the highest scoring for public transport (8) of any Main Rural Settlement, putting Long Lawford at 6 out of the 9 top tier settlements. DS3.8

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sits on one of the key routes between Rugby and Coventry with a regular bus service to Rugby Town Centre, Coventry City Centre and Walsgrave Hospital.

6.42 To the east of the site, the nearest bus stop is circa 100 metres whilst to the west the nearest bus stop is circa 70 metres (both measurements are taken from their respective closest points.) Therefore public transport is readily accessible on foot, making the site highly sustainable.

6.43 The 86 bus route currently runs between Coventry and Rugby 7 days a week. Increased usage of this route would better support the service. During the week (Mondays to Fridays) and on Saturdays the 86 service operates on an approximately half hourly basis between 09.00 and 18.00, which an hourly evening service which ends at 21.30. There is a limited Sunday service which operates between 09.00 and 17.00 on a two hourly basis.

6.44 The proposed development will bolster existing patronage on this service, but also would support a potential improvement to evening and Sunday bus service provision.

6.45 LP03 sustainability appraisal assessed DS3.8 on Pages 474-476. This details significant positive affects for the sites location in a MRS, access to public transport and good access to public space.

e. The evidence to support the site's 'deliverability' as defined in footnote 11 of the NPPF?

6.46 NPPF footnote 11 identifies that for sites to be deliverable, "sites should be available now, offer a suitable location for development now and be achievable with a realistic prospect that housing will be delivered on the site within five years and in particular that development of the site is viable". (NPPF, Page 12).

6.47 The site is currently undeveloped. A Planning Application has been submitted, which demonstrates a degree of confidence that in principle the site could be brought forward. There are no known constraints on site which would prevent development coming forward, save for any appropriate mitigation such as junction works. This would be controlled by condition through the Development Management process.

6.48 The site could only be developed after the adoption of the Local Plan owing to its greenbelt status. Land immediately to the North of the site is currently being

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developed by the same developer promoting this parcel. As such, it can be demonstrated that there is a track record of delivery within the immediate vicinity.

6.49 Part of proposed allocation DS3.8 is dependent upon access from the site currently under construction (planning application reference R17/1089) but given that it is the same developer in question, this is not considered to be an obstacle to delivery.

6.50 The level of development seen in Long Lawford over the recent years shows demonstrates that a strong housing market. The fact that the site promoter is currently developing the site to the North of the allocation is considered evidence of this. This development is well progressed since receipt of planning permission in R12/1188, with 19 of the site total complete at the time of writing. This on the ground evidence strongly demonstrates that site has a realistic prospect that housing will be delivered on the site within 5 years.

f. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements?

6.51 LP22 - Rugby Borough Council Local Plan and Community Infrastructure Levy (CIL) Viability Assessment examines the viability of the proposed plan and establishes the Policy basis for Policy H2, which requires 30% affordable housing on greenfield sites. The viability assessment tests proposed schemes for sensitivity based upon geographical variations, including main rural settlements LP22 identifies that “relatively straightforward greenfield development (sub-strategic in nature) is generally amongst the more viable forms of development.” (LP22, P77).

6.52 No viability constraints have been identified in the delivery of the allocation. A viability assessment is expected to be submitted with the Planning Application at a later stage.

Ryton on Dunsmore

1. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and would they be

consistent with national policy, which particular regard for the following for each site:

a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?

- 7.1 Land at Leamington Road, Ryton on Dunsmore (site reference DS3.9) is proposed for the development of up to 75 dwellings.
- 7.2 The site area for the DS3.9 allocation covers 2.9ha and sits on the western edge of parcel RD2 in the 2015 The Green Belt Review (LP30). A table which can be found in appendix A of this statement contains a summary of the quantitative and a qualitative assessment on each of the purposes of the Green Belt provided in LP30 for this parcel. This assessment concludes that the parcel plays a role in preventing ribbon development, that there is presently 2km between Ryton and Stretton on Dunsmore, that there are several pockets of development scattered through the parcel but some remaining areas are still in rural character and relatively open and that the parcel does not overlap with a Conservation Area within an historic town but the high ground within the parcel does have some impact on the setting of Coventry.
- 7.3 The comments above apply to the whole parcel of which the site only makes up a very small area in the north west of the parcel. Although within the Green Belt and being used for a recreational purpose the site itself is currently developed and has buildings on both sides of it along Leamington Road. It is within the more developed area of parcel RD2. The Landscape Sensitivity Study 2016 (LP35) looks at the site rather than the wider parcel and concludes that this site relates more to the settlement than the surrounding countryside.
- 7.4 The development of the proposed site would not change the distance between Ryton and Stretton on Dunsmore.
- 7.5 In relation to the impacts on historic Coventry, the site itself is not in the higher area but is set down from Leamington Road.
- 7.6 All but one of the sites submitted as part of the 2015, 2016 and 2017 SHLAA (LP10 and LP10a) for Ryton on Dunsmore are in the Green Belt and would require Green Belt release in order to be allocated for growth in this Local Plan. The site that is not in the Green Belt (SO61) is designated Open Space and the village has a deficit of Open Space and therefore the site is not suitable for development.

7.7 The Green Belt Review (LP30) concludes that the parcel plays a medium role in fulfilling the purposes of the Green Belt and that there will be an impact if it were to be removed from the Green Belt. However, within this parcel it can be argued that the site plays a lesser role and therefore while there will be some impact to the Green Belt due to release of this site, this impact is less than the loss of the parcel in its entirety.

b. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable

7.8 To the north west of the site is Leamington Road with a wooded area beyond which will form the new Green Belt boundary. The site sits lower than Leamington Road enhancing the boundary effect of the road. To the south west is a leisure venue and an employment site, both are within the Green Belt but form a clear boundary. To the south east is the Meadowlands Fishery which covers a large area and forms a clear boundary.

7.9 The Green Belt Review (LP30) provides examples of strong and weaker Green Belt boundaries. The strongest boundaries are features such as dual carriageways and rivers. In this instance the new boundaries would fall within the weaker category. However the current boundary runs along Leamington Road and down the side of the properties to the north east of the site and as such the new boundary would not be significantly less strong than the current one.

c. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities?

7.10 Landscape Character- The site and the wider village are within the Dunsmore Plateau Fringe character type (from the LP34 Landscape Assessment 2006) which is mostly a large scale intensively farmed agricultural landscape. This parcel is an exception to this description due to the fact it has previously been developed and so the direct impacts to the landscape character of the site are minimal. In the context of the wider landscape area Ryton on Dunsmore is in an area of moderate overall sensitivity.

- 7.11 The Landscape Sensitivity Study 2016 (LP35) notes the importance of the roadside hedgerows and trees which soften the impact of development and contribute to a continuous landscape buffer and help to enclose the zone. These are outside of the development site and so will remain in place. Compliance with Policies NE3, NE4, DS5 and SDC2 will ensure that adequate landscape mitigation is provided to ensure this buffer is strengthened.
- 7.12 The Study suggests that the semi-urban nature of the site relates more to the settlement than to the surrounding countryside and that it would be appropriate for residential development.
- 7.13 The Sustainability Appraisal (LP03) acknowledges that the development of the site could have a small negative impact on the landscape but that this could be mitigated.
- 7.14 Heritage- There is no conservation area in the village of Ryton on Dunsmore. The Heritage Asset Review 2016 (LP38) states that the site does not lie within the setting of any designated or non-designated heritage assets and that therefore there would be no historic environment implications.
- 7.15 Biodiversity- There is a Potential Local Wildlife Site covering the land on the opposite side of Leamington Road to the proposed site. Until a survey has been undertaken by the Habitat Biodiversity Audit Partnership the status of the land cannot be confirmed. However, Local Plan policy SDC2 will require appropriate boundary treatment. Additionally consultation with WCC Ecology will be required if the site is designated a Local Wildlife Site.
- 7.16 Ryton and Brandon Gravel Pits is a SSSI located to the north west of the village and Ryton Woods SSSI to the south west. The Magic Maps online application from DEFRA and the associated guidance document released in October 2017 concludes that the site is unlikely to pose a risk to SSSI as it consists of development of less than 100 dwellings. The Sustainability Appraisal (LP03) acknowledges that there may be a negative impact on biodiversity but that this could be mitigated.
- 7.17 Agricultural Land- The site is situated on grade 3 agricultural land. The whole village is covered by the same grade 3 agricultural land classification. Further

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assessment would be required to understand whether this is grade 3a or 3b. It is also important to note that this site is currently in use as a football training ground with sports pitches and ancillary buildings and is therefore not currently in agricultural use.

7.18 Flood Risk- The entirety of the site is in Flood Zone 1 and therefore complies with paragraph 100 of the NPPF which seeks to direct development away from areas at highest risk.

7.19 Highways Safety- Access is proposed off Leamington Road. The site will require suitable minimum visibility splays and the Highways Authority have indicated this can be provided at this location.

7.20 Infrastructure & Facilities - The Rural Sustainability Study (LP28) states that under the essential services category the village has a village hall, shop, post office and primary school. Under the non-essential category Ryton also has a café, a pub, a mobile library, a place of worship, recreational space and an early year's nursery. All of these facilities will be supported by an increase in residents.

7.21 In October 2017 the primary school (Provost Williams C E Primary School) had 188 children on roll with a total of 210 places. The majority of children on roll were from the schools own priority area with some limited travelling from elsewhere in the Borough. Based on current pupil yield indicators we would estimate 75 homes generating in the region of 24 additional primary age pupils. With children from the proposed development site falling within the priority area for the school, Warwickshire County Council envisage the school being able to cater for the impact of the proposed housing.

7.22 At secondary age, pupils would receive free home to school transport into the town as is currently the case for other secondary age pupils from Ryton on Dunsmore. For the purpose of clarity, the impact of additional housing in the Main Rural Settlements on secondary school places has been included in the figures presented in the education note (OTH/038).

7.23 The 580 bus service, operated by iGo runs between Coventry and Rugby 6 days a week on a limited frequency. Additional users will help to support this

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service to make it a more sustainable and frequent service to serve the village and surrounding communities. The Sustainability Appraisal scores the site as a strong positive in terms of supporting service and facilities.

7.24 In addition there is a large and growing employment site adjacent to the village (Prologis Ryton) which could offer local employment opportunities for new residents.

d. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

7.25 The Landscape Sensitivity Study 2016 (LP35) looks at the site rather than the wider parcel and concludes that this site relates more to the settlement than the surrounding countryside. It has built development on three sides and although the site in its current form is not open to the public there are residential buildings adjacent to the north east and a public gym, leisure centre and function venue immediately adjacent to the south west. As such it already forms part of the urban area and is well connected to the main settlement on footpaths and the road.

7.26 As outlined in the Development Pack for Ryton (LP47), all essential services in the village are within a ten minute walk from the site.

e. The evidence to support the sites 'deliverability' as defined in footnote 11 of the NPPF?

7.27 The Housing Trajectory (as appended to LP11) shows 25 homes being complete in 2020-21 and the final 50 homes complete in 2021-22. The site is previously developed but with very little demolition required. An indicative layout has been provided by the land agent.

7.28 The site is currently in use as the training ground for Coventry City Football Club. The club have been looking for an alternative site after deeming the current site no longer fit for purpose. In consultation with Sport England policy DS3 contains a clause which states that the site cannot be implemented until the adequate replacement of the pitches have been made to the satisfaction of Sport England. Having reviewed this allocation it is deemed to no longer fall under the definition of deliverable (as defined in footnote 11 of the NPPF). Based on the

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information currently available an alternative site has not yet been secured and as such the site is not available for development now. The site is still deemed to be developable and is still proposed for allocation but is now in the trajectory outside of the first five years. Work to find an alternative site for the training ground is ongoing and the deliverability of the site will be reviewed as part of the annual monitoring and updates to the housing supply.

f. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements?

7.29 The site has been previously developed and therefore will be subject to a 20% affordable housing contribution. This is lower than the percentage required on greenfield sites which provides a greater margin for viability.

7.30 Other than onsite infrastructure including roads, access and SuDs there are no major infrastructure costs for the site.

Stretton on Dunsmore

Issue 4b: Site Specific Issues for the MRS and Coton House Allocations

- i. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and would they be consistent with national policy, which particular regard for the following for each site:**
 - a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?**

8.1 There are two sites proposed for allocation within this Main Rural Settlement.

8.2 The first is at The Old Orchard, Plott Lane, through policy DS3.10 for 25 dwellings. The SHLAA (LP 10a) assessed the site under reference S14/004. This is a former orchard and is 0.9 hectares in area and adjoins the settlement boundary on 3 sides. It is outside of the settlement boundary and located entirely within the Green Belt. This site has been refused planning permission previously due to the impact upon the Green Belt under reference R14/2164 (decision notice attached as appendix B).

- 8.3 The second site is land off Squires Road, Stretton-on-Dunsmore, through policy DS3.11 for 50 dwellings. The SHLAA (LP10a) assessed the site under reference S14/122. This is a 1.83 hectare site and proposed for allocation under DS3.11. This site is in the northeast corner of a larger SHLAA site (S16/065) which is 7.6 hectares in total. It is immediately adjacent to, but outside of the settlement boundary and located entirely within the Green Belt. The site has previously been refused outline planning permission (R13/0250) due to the lack of a great crested newt survey and as a result of the proposal being situated within the Green Belt and there being no special circumstances justifying the presumption against inappropriate development in the Green Belt (decision notice attached as appendix C).

DS3.10 Plott Lane, Stretton on Dunsmore

- 8.4 The site is situated within Green Belt parcel SD4 as assessed in the Joint Green Belt Review (JGBR) (LP30). The site is situated in the north eastern corner of this parcel.
- 8.5 The JGBR states that this parcel acts to prevent ribbon development along Fineacre Lane and Plott Lane and that the parcel is mostly open with some agricultural buildings. There are no significant boundaries beyond this parcel to prevent encroachment into the countryside. The nearest village is Princethorpe which is 1.5km away from the parcel. SD4 is rated as a mid-performing Green Belt parcel which was given mixed scores between the five purposes is the JGBR.
- 8.6 The JGBR makes clear that the parcel does not overlap with a Conservation Area and that all Green Belt makes a strategic contribution to urban regeneration by restricting land available for development.
- 8.7 The comments apply to the whole parcel of which the site only makes up a very small area. This site is bounded by existing development on 3 sides and a recreation ground to the west.
- 8.8 Given the requirement to meet housing needs, whilst taking this site out of the Green Belt will have an effect, the impact is less than the loss of the parcel in its entirety and since a defensible boundary can be created (see the answer to part b), this is considered to be reasonable and will not result in the unrestricted sprawl of large built up areas.

DS3.11 Land off Squires Road, Stretton on Dunsmore

- 8.9 This Site is within Green Belt parcel SD1 (as defined in LP30). The Green Belt review states the parcel helps to prevent ribbon development along Fineacre Lane and Plott Lane. There are some agricultural buildings but these do not compromise the openness of the parcel.
- 8.10 The A45 dual carriageway to the north forms the northern boundary of the parcel and serves as a significant boundary.
- 8.11 The JGBR makes clear that the parcel does not overlap with a Conservation Area and that all Green Belt makes a strategic contribution to urban regeneration by restricting land available for development.
- 8.12 The impact of taking this part of the site out of the Green Belt is less when compared with taking out the entirety of parcel SD1. The overall effect upon the Green Belt is considered acceptable given the need to accommodate increased housing needs across the borough, and given that this allocation will square off the built form of the village. As a result it is not considered it will increase unrestricted sprawl and a defensible boundary can be created (see part b).
- 8.13 When developed the site would include the northern-most housing in the village and would decrease the distance between Stretton on Dunsmore and both Ryton on Dunsmore and Wolston. However the distance between the villages will still remain over 2km.
- 8.14 The Green Belt could continue to function with a defensible boundary along the plot edge, whilst at the same time allowing the site to contribute to housing needs. As a result it is considered that taking out DS3.11 is justified.

b. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable?

DS3.10 Plott Lane, Stretton on Dunsmore

- 8.15 There are mature hedgerows to the south and north, and a defined boundary to the east where existing development currently ends. To the west is an access road and parking area for the adjoining recreation ground. If removed from the Green Belt and developed, it is considered that the resulting Green Belt boundaries could be clearly defined using physical features, especially if further landscaping was to be employed along the western boundary, and existing mature

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vegetation is retained along the northern and southern boundaries. This would be consistent with LP30 Paragraph 3.9 of the study identifies "...hedgerows and tree lines..." to be recognisable but less permanent boundaries.

8.16 Development of this site would arguably provide a more defensible boundary that exists at present as it squares off the existing built form of the village. This is with the proviso that landscape buffers are implemented along the boundary edge and delivered through the implementation of policy DS6.

8.17 As a result the site is capable of being developed whilst maintaining a clear Green Belt boundary using physical features so that the adjoining site can perform a Green Belt function.

DS3.11 Land off Squires Road, Stretton on Dunsmore

8.18 This site is SHLAA site S14/122 which is part of one field. The field itself has a mature hedgerow around the perimeter, as detailed in the MRS Development Pack – Stretton on Dunsmore (LP48). Within S14/122 there is a smaller hedge to the west which could be strengthened to form a more defensive boundary if further landscaping was conditioned on any planning permission.

8.19 The JGBR (LP30) states that woodland, hedgerows and tree lines are capable of being recognisable boundaries, albeit less permanent ones compared with water courses, roads and railway lines.

8.20 Removal of this site from the Green Belt for redevelopment can be undertaken so that a clearly defined Green Belt boundary can be strengthened by conditions requiring a thicker and more mature hedgerow on its western side, through the implementation of DS6. The remaining field can still perform a Green Belt function.

c. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities.

8.21 Landscape Character - As identified in LP34, DS3.10 and DS3.11 are located within Stretton on Dunsmore which is characterised in landscape terms as the Dunsmore landscape character type – Plateau Farmlands, Plateau Fringe, Parklands and River Meadows.

- 8.22 Stretton on Dunsmore itself sits within a landscape that comprises medium scale mixed farmland on very gently undulating terrain, framed to open landscape with small copses with tree/shrub vegetation around field ponds and water courses in places. The western part of the zone creates a more enclosed, smaller scale character. There is a mix of trimmed and outgrown roadside hedgerows.
- 8.23 The selected sites were considered most appropriate for development providing a substantial landscape buffer of native trees and shrubs is planted along the western and northern edges and that the settlement edges are indented. The alternative sites S14/049, S14/154, S14/157 and S16055 are all considered inappropriate for development due to the need to preserve the historic landscape character, irregular outline of the settlement, the setting of the Conservation Area and the transition zone between the settlement and the wider countryside.
- 8.24 Heritage - The Conservation Area encapsulates the historic core of the settlement, and has historic value due to its Anglo Saxon origins. The Conservation Area occupies low lying land; the surrounding land rises to the north, south and west. Buildings and trees on the higher land create a sense of enclosure; the focal point being the series of village greens, punctuated by mature trees, in the centre, dissected by the stream. The sense of enclosure is key to the communal value, with a strong village identity around tight knit buildings. Developing DS3.11 is unlikely to create the need for mitigation measures in this respect, as the development is unlikely to substantially impact upon the character and appearance of the Conservation Area.
- 8.25 In relation to DS3.10, there are four heritage assets that are relevant to the assessment of the site:
- Church of All Saints – grade II* - almost intact example of a church from the early 19th century.
 - Manor House – grade II – forms part of a group of large historic buildings forming a ground around the church on Church Hill, dating from the 16th Century with a later 19th Century extension, surrounded by large grounds which contributes to its setting
 - Stretton House – grade II – like a manor house, both on Church Hill
 - Church Farmhouse – grade II

- 8.26 This site was formally part of the Manor House which was once the orchard. Appropriate consideration of these assets through the implementation of DS6 and SDC3 will ensure appropriate mitigation will be made.
- 8.27 Biodiversity - LP41 – the Habitat and Biodiversity Audit – June 2017 provides a detailed and robust audit of the Borough’s biodiversity resource, including the extent and condition of habitats so that species and habitats can be taken into account in all stages of the planning process.
- 8.28 Stretton on Dunsmore is within a SSSI Impact Zone for both Brandon March SSSI and Ryton Wood SSSI. This would require consultation with Natural England at the detailed planning application stage.
- 8.29 The location of SSSIs has been a consideration in the selection of sites and no concerns have been identified by statutory consultees during consultation phases. As a result it is unlikely that the SSSI’s will be affected by development at these locations. However the application of policies within the Natural Environment chapter and consultation with relevant consultees at the application stage will ensure appropriate mitigation is made.
- 8.30 Stretton on Dunsmore lies within the Princethorpe biodiversity area (LP02.19).
- 8.31 Policy NE2 specifically addresses biodiversity and further strengthens the requirement in SDC2 by requiring biodiversity assets to be protected or enhanced with a net gain in biodiversity highlighted as a desired outcome of development.
- 8.32 There is opportunity to improve the sensitivity of the landscape through retaining the hedgerow trees and supplementing them, and incorporating landscaping. Compliance with Policies NE3, NE4, DS6 and SDC2 will ensure that this tree planting is included.
- 8.33 Agricultural Land - The Agricultural Land Classification for both sites within Stretton on Dunsmore is 3. It is acknowledged that loss of land in agricultural land classification 3a cannot be mitigated and therefore would be lost were development to go ahead on the site. However, in line with NPPF paragraph 112 it has been illustrated that the use of this land is necessary to ensure that the

identified housing need, and more specifically the five year supply of housing, for Rugby is met sustainably.

8.34 Flood Risk - Both selected sites are located within Flood Zone 1 and consequently have a low probability of flooding. There is therefore unlikely to be any significant impact as a result of development of these sites. The site therefore complies with paragraph 100 of the NPPF which seeks to direct development away from areas at highest risk.

8.35 Highways Safety - DS3.11 – Land of Squires Road, Stretton on Dunsmore. A new access will need to be created from Squires Road (an existing residential cul-de-sac) from parts of the front gardens of numbers 32 and 33. The Highways Authority considers the proposed access to be suitable.

8.36 DS3.10 – Plott Lane, Stretton on Dunsmore. There is a potential access off Plott Lane which would need to be widened with an extension of the footway to the site access. The Highway Authority would require an extension to the 30mph speed limit area to the west of the site.

8.37 There is no evidence that the required mitigation would prevent development in highway terms and as a result the impact on highways is considered acceptable.

8.38 Infrastructure - Stretton on Dunsmore has one form of entry (FE) primary school, Knightlow Church of England Primary School which is often over-subscribed. In October 2017 there were 216 children on roll with a total of 210 places. Approximately half of the total children on roll were from the school's priority area, with the remainder travelling from elsewhere in the Borough. Based on the current yield it is estimated that 75 homes from the allocation would generate approximately 24 children. With children from the proposed development site falling within the priority area for the school, this means that the school can accommodate this level of pupil numbers. At secondary age, pupils would receive free home to school transport into the town as is currently the case for other secondary age pupils from Stretton on Dunsmore. There is therefore no advice effect on school places.

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- 8.39 The addition of 75 homes is likely to positively benefit bus services, in particular route 580 which runs 6 buses per day between 7:42 to 18:29 by increasing the potential customer base. This service runs between Coventry and Rugby.
- 8.40 Facilities - Stretton on Dunsmore is ranked 7th in the sustainability rankings (LP 28). The rankings were weighted depending upon the importance of the service and public access to public transport. Stretton on Dunsmore scored 32 points in terms of access to services.
- 8.41 Stretton on Dunsmore has a score of 3 in terms of access to public transport. Overall, Stretton on Dunsmore had an overall score of 35 in terms of sustainability.
- 8.42 DS3.11 Land of Squires Road, Stretton on Dunsmore is within walking distance of all village services, the furthest being 740m away but still within a 10 minute walking distance. The site is considered sustainable.
- 8.43 DS3.10 Plott Lane, Stretton on Dunsmore is a sustainable location within walking distance of all the village services (the furthest being only 560m away).
- 8.44 There is a potential access off Plott Lane which would need to be widened with an extension of the footway to the site access. The Highway Authority would require an extension to the 30mph speed limit area to the west of the site.
- 8.45 There is no evidence that the required mitigation would prevent development in highway terms and as a result the impact on highways is considered acceptable.
- 8.46 Infrastructure- Stretton on Dunsmore has one form of entry (FE) primary school, Knightlow Church of England Primary School which is often over-subscribed. In October 2017 there were 216 children on roll with a total of 210 places. Approximately half of the total children on roll were from the school's priority area, with the remainder travelling from elsewhere in the Borough. Based on the current yield it is estimated that 75 homes from the allocation would generate approximately 24 children. With children from the proposed development site falling within the priority area for the school, this means that the school can accommodate this level of pupil numbers. At secondary age, pupils would receive free home to school transport into the town as is currently the case for other secondary age pupils from Stretton on Dunsmore. There is therefore no adverse effect on school places.

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8.47 The addition of 75 homes is likely to positively benefit bus services, in particular route 580 which runs 6 buses per day between 7:42 to 18:29 by increasing the potential customer base. This service runs between Coventry and Rugby.

8.48 Developing the two proposed sites is unlikely to have an adverse impact upon Infrastructure.

8.49 Facilities- Stretton on Dunsmore is ranked 7th in the sustainability rankings (LP 28). The rankings were weighted depending upon the importance of the service and public access to public transport. The Main Rural settlements rank higher than the generally smaller Local Needs Settlements. Stretton on Dunsmore scored 32 points in terms of access to services.

8.50 Stretton on Dunsmore has a score of 3 in terms of access to public transport. Overall, Stretton on Dunsmore had an overall score of 35 in terms of sustainability

d. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

8.51 Site DS3.11 is within walking distance to all of the village services, the furthest is the surgery which is 740 metres away. As this is within a 10 minute walking distance this is considered acceptable. The site is considered sustainable.

8.52 Site DS3.10 is within walking distance of all the village services and is deemed particularly sustainable in this regard. The furthest facility, the surgery, is 560 metres away which is well within a 10 minute walking distance.

8.53 LP28 rates Stretton on Dunsmore as 8th in terms of sustainability rankings within the Main rural settlements with an overall score of 35.

8.54 The 580 bus service runs between Coventry, Rugby and Dunchurch via Stretton on Dunsmore, 6 times a day. The Flexibus also operates once daily on a Tuesday.

e. The evidence to support the sites 'deliverability' as defined in footnote 11 of the NPPF?

8.55 The Housing Trajectory (as appended to LP11) shows 30 homes being complete in 2020-21 and the final 45 homes complete in 2021-22. The sites are

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primarily greenfield with no agricultural buildings in place on the site. Therefore they are not deemed to adversely affect deliverability. The sites can only be developed after the adoption of the Local Plan owing to their greenbelt status.

8.56 Both sites (DS3.10 and DS3.11) have developers on board and there are no known issues to prevent development coming forward within 5 years of the adoption of the Local Plan.

f. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements?

8.57 Both DS3.10 and DS3.11 are greenfield. There are no structures to demolish in either of the proposed allocations. Other than onsite infrastructure including roads, access and SUDs there are no major infrastructure costs for either site. The viability testing (LP22) suggests greenfield sites are among the most viable with those closer to 50 dwellings the most viable compared to those that are closer to 100. At 50 dwellings for DS3.11 and 25 dwellings for DS3.10 these sit toward the lower value. The testing at 30% affordable housing showed this to be viable for greenfield sites.

Wolston

Issue 4b: Site specific Issues for the MRS and Coton House allocations.

1. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and; would they be consistent with national policy, with particular regard to the following for each site:

9.1 Land at Wolston Lane, Wolston (site reference DS3.12) is proposed for the development of up to 15 dwellings.

9.2 Development Packs were produced for the main Rural Settlements (LP44-50), which provided an assessment of all the SHLAA (LP10 and 10a) sites for each village and identified the most appropriate sites.

9.3 For Wolston, 8 sites were considered, however only one was considered suitable for allocation (S14/064). An alternative site was initially identified as suitable for

allocation (S16/067), however deliverability issues were found due to housing development being unable to start until minerals development had been completed. It was therefore concluded that this alternative site would be unsuitable for allocation.

a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?

9.4 The Coventry and Warwickshire Joint Green Belt Review 2015 (JGBR) (LP30) assessed parcels of land against the five purposes of including within the Green Belt. The following conclusions were made regarding parcel of land in which the proposed allocation sits (parcel WN2). The site is in the north of this parcel.

To check the unrestricted sprawl of large built up areas:

9.5 The parcel plays a role in preventing ribbon development along Wolston Lane and Stretton Road. However the site in question does not perform this role due to its location within the parcel. In terms of openness, it was noted that the parcel contains several buildings, most notably Ryton Gardens. In addition there are a few farms and residential dwellings on the south eastern corner of the parcel. These were considered to compromise the openness of the Green Belt within their immediate vicinity, however other areas of the parcel, particularly in the South, are open fields free from development.

9.6 With consideration to the findings of the study, the proposed allocation would not extend along the highway and would not therefore contribute towards ribbon development. In terms of openness, the site is located adjacent to the existing residential area to the North and there is garden land immediately to the East, followed by allotments. Fields are located to the South, with a rising gradient, and a Paddock and Orchard area are located to the West. Due to being adjacent to the existing settlement the effect on openness is reduced to a degree, however it is acknowledged that there would be some impact on the openness of the Green Belt.

To prevent neighbouring towns merging into one another:

9.7 The JGBR states that the village of Stretton-on-Dunsmore is located roughly 2km to the south of the parcel and Ryton on Dunsmore 2km to the west. A substantial area of open space would remain with the allocation of the parcel and the intervening barrier of the A45 is also present, although it is acknowledged the distance between Wolston and Stretton on Dunsmore would be reduced to a degree.

To assist in safeguarding the countryside from encroachment:

9.8 The JGBR notes that whilst the parcel contains some clusters of development there are significant portions of the Green Belt within the parcel which are still rural in character and open.

9.9 With consideration to the findings of the JGBR, the proposed allocation is an extension onto the existing settlement, and as such is consistent within the context of this. It is acknowledged however that notwithstanding this, by its nature the development would result in a degree of encroachment into the countryside. The site would not affect the parcel boundaries to the South or West.

To preserve the setting and historic character of towns:

9.10 The JGBR states that the parcel does not overlap with a Conservation Area within an historic town but that long range but clear indivisibility is a factor in the contribution of the parcel to the setting of Coventry.

9.11 The topography of the land which dips down toward the village of Wolston, and the distance from the site to Coventry reduces any impact on the historic city considerably.

To assist in urban regeneration by encouraging the recycling of derelict and other urban land:

9.12 The JGBR states that all Green Belt makes a strategic contribution to urban regeneration by restricting the land available for development and encouraging developers to seek out and recycle derelict / urban sites. As the whole Housing

Market Area functions as one unit, this makes it difficult to accurately assess whether one individual parcel considered in isolation makes a more significant contribution than another to incentivising development on previously developed land. What can be said is that all parcels make an equally significant contribution to this purpose and are each given a score of 4.

9.13 With consideration to the findings of the JGBR, the sites submitted as part of the 2015, 2016 and 2017 SHLAA (LP10 and LP10a) Wolston are in the Green Belt and would require Green Belt release in order to be allocated for growth in this Local Plan. It is not considered that the availability of this site would prejudice the development of derelict and other urban land elsewhere.

9.14 In summary, the proposed allocation would have an effect on the purposes of the Green Belt, although the impact is considered acceptable when consideration is given to the special circumstances and strategic need of the allocation. The site would not contribute to unrestricted sprawl of a large built up area. The site is adjacent to residential development to the North and allotments within the settlement boundary to the East, and is not considered to make a significant contribution to the risk of neighbouring towns (or settlements) merging into one another. The site would encroach into the countryside to a degree, however it does not make a significant projection past the planes of the existing village envelope, has mature planting to reduce the visual impact, and is not considered to be unacceptable given the circumstances of the allocation. The site would preserve the setting and special character of historic towns considering its location and distance from heritage assets. The development of this site would not conflict with the aim of Green Belt policy to assist in urban regeneration by encouraging the recycling of derelict and other urban land.

9.15 The conclusions of the study, including comments in response to this as detailed above, are summarised in appendix A.

b. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable?

9.16 The site is immediately adjacent to residential development and the existing settlement boundary to the North. To the East of the site is garden land (albeit outside of the settlement boundary, with the existing settlement boundary and

allotments beyond this, forming a readily recognisable boundary. The South boundary of the site has an established hedgerow and is also clearly demarcated by a field drainage ditch that runs the length of the site and beyond. An established hedgerow is also present on the Western boundary to separate the site from the adjoining Paddock and Orchard areas.

c. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities?

9.17 Landscape character - In the Landscape Assessment 2006 (LP34), the site forms part of the Dunsmore Plateau Fringe landscape character area. The proposed allocation is located in an area that has a visibility rating of “Moderate”, which is generally due to the presence of small woods and trees superimposed over a rolling topography. The area has an overall sensitivity score of “Moderate”. The Landscape Sensitivity Study 2016 (LP35) considers site S14/064 within its assessment of landscape parcel WO_06. It states that the parcel comprises a predominantly large scale, open pastoral landscape on rolling terrain. Site S14/064 lies within a remaining part of a small scale field which has now been partially developed along the road. It is also enclosed by mature garden vegetation, and therefore could accommodate some development providing this vegetation is retained.

9.18 Due to being located in an area of moderate sensitivity, the Sustainability Appraisal Report (SA) (LP03) gives the site a score of a minor negative (-) on the objective of maintaining and where possible enhancing the quality of landscapes, although this is uncertain as effects will depend to some extent on the design of the development and the incorporation of mitigation measures such as screening.

9.19 Heritage - The proposed allocation would not adversely impact upon the historic environment. The site is considered to be a sufficient distance away from the Wolston Conservation Area and other heritage assets (including the Scheduled Ancient Monuments in the vicinity). The SA (LP03) gives the site a neutral score and comments that the site is most likely to have a negligible effect on the objective of conserving and enhancing the historic environment, heritage assets and their

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setting (although this is uncertain as effects will depend on factors such as the design and it may be possible to incorporate mitigation measures).

9.20 Biodiversity - There are no known specific biodiversity issues on the site itself or in the near vicinity. The Habitat Biodiversity Audit 2017 (LP41) identifies a SINC on the north-eastern edge of the village, however it is not considered that the proposed allocation would adversely affect this. Approximately 400 metres to the south of the site is Wolston Gravel Pits SSSI, which is classified due to geological interest, with the hollow itself a relatively small feature not visible from the site and separated by hedgerow and intervening agricultural land. A Local Wildlife Site, The Plantation, is approximately 350 metres to the north of the site. The owner of the proposed allocation site has commented in correspondence received February 2016 that there are many existing houses between the site and the Local Wildlife Site so the impact on such features is considered to be negligible.

9.21 Agricultural land - The site is on grade 3 agricultural land. Due to this, the SA gives the site a significant negative score (- -) for the objective of using and managing land, energy, soil, mineral and water resources prudently and efficiently, and increasing the energy generated from renewables, although this is uncertain depending on whether the site is within Grade 3a or 3b land which is unknown. If the land is classified as 3a this would fall into the definition of the best and more versatile agricultural land. While land in agricultural land classifications 1 or 2 is valued more than 3a, it is acknowledge that loss of land in agricultural land classification 3a cannot be mitigated and therefore would be lost were development to go ahead on the site. However, in line with NPPF paragraph 112 it has been illustrated that the use of this land is necessary to ensure that the identified housing need, and more specifically the five year supply of housing, for Rugby is met sustainably. No suitable areas of poorer quality agricultural land have been identified however, as described in Paragraph 112 of the NPPF.

9.22 The site owner has advised in correspondence received February 2016 that the site comprises of garden land and overgrown horticultural land dominated by evergreen trees, such as Leylandii. The owner also advises that therefore, unlike the surrounding farmland the site would highly probably have poor, if not very poor,

agricultural land in terms of condition and would certainly not have a significant negative effect on this greenfield land site.

9.23 Flood risk - The site is located outside of Flood Zones 2 and 3 and is not in an area identified at being at risk from surface water flooding (according the Environment Agency Surface Water Map). The SA gave the site a minor negative score (-) due to being located on greenfield land and outside of floodzones 3a and 3b. There are no known barriers however to implementing sustainable drainage or appropriate flood mitigation measures if required. Therefore the site complies with paragraph 100 of the NPPF which seeks to direct development away from areas at highest risk.

9.24 Highways - As detailed in the Wolston MRS (LP59) the site has been assessed by WCC Highways who have stated that there is the ability to implement a suitable access arrangement to the highway network. The site is considered to be adequately served by existing infrastructure within the settlement and would not place an undue demand on services.

9.25 Wolston is currently served by the 86 bus route, operated by Stagecoach, which currently runs between Coventry and Rugby 7 days a week. Increased usage of this route would better support the service. During the week (Mondays to Fridays) and on Saturdays the 86 service operates on an approximately half hourly basis between 09.00 and 18.00, which an hourly evening service which ends at 21.30. There is a limited Sunday service which operates between 09.00 and 17.00 on a two hourly basis.

9.26 The proposed development will bolster existing patronage on this service, but also would support a potential improvement to evening and Sunday bus service provision.

d. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

9.27 The site is adjacent to the existing settlement with residential development to the North. The site is within walking distance to all the village services including within a 10 minute walk to a bus stop, post office and convenience shop facilities.

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It is marginally beyond a 10 minute walk distance to three services (Primary School, Surgery and Village Hall), although this is not deemed to significantly affect the sustainability of the site. The Sustainability Appraisal Report (LP03) gives the proposed allocation a significant positive score (+ +) for the SA objective of providing good quality local services, leisure and cultural opportunities with good access for all sections of the community. The SA also gives the site a significant positive score (+ +) for the objective promoting a sustainable and accessible transport network, as the site is within 600m of three bus stops.

9.28 Warwickshire County Council's (WCC) Education department have commented that Wolston St Margaret's CE Primary School is a 1Form Entry school, recently having been expanded to support 7 teaching groups. The school previously had a Published Admission Number of 25 requiring mixed age classes. In October 2017 there were 212 children on roll with a total of 210 places. The majority of children on roll were from the schools own priority area with a limited number travelling from elsewhere in the Borough. Based on current pupil yield WCC estimate 15 homes generating in the region of 5 additional primary age pupils, with children from the proposed development site falling within the priority area for the school. WCC envisage the school being able to cater for the impact of the proposed housing. However, larger scale growth could cause concern. At secondary age, pupils would receive free home to school transport into the town as is currently the case for other secondary age pupils from Wolston.

e. The evidence to support the site's 'deliverability' as defined in footnote 11 of the NPPF?

9.29 The site was assessed via the Council's Strategic Housing Land Availability Assessment (SHLAA) (LP10 and 10a) and was found to be suitable, available and achievable. The site could only be developed after the adoption of the Local Plan owing to its greenbelt status. Further correspondence including an amended Call for Sites form was submitted in June 2016 by the owner confirming the site would be deliverable within five years. Recent correspondence received in March 2018 has confirmed there have been expressions of interest from builders and land promoters.

f. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements?

9.30 The site is a mixture of brownfield and greenfield being both garden land and former garden nursery. There are no major infrastructure costs for the site. The viability testing (LP22) at 30% affordable housing on greenfield sites. There are no known significant viability constraints that would prevent the development from coming forward. Infrastructure in the existing settlement is sufficient to support the development. No specific issues have been identified that would restrict affordable housing, or prevent the development from complying with policy requirements.

Wolvey

Issue 4b: Site Specific Issues for the MRS and Coton House Allocations

- 1. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and would they be consistent with national policy, which particular regard for the following for each site:**
 - a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?**

10.1 There are two sites that are subject to a proposed allocation within Wolvey, both of which are located outside the settlement boundary and are entirely within Green Belt.

DS.13 Land at Coventry Road

- 10.2 The first site proposed for allocation under DS3.13 is land at Coventry Road for 15 dwellings. This was assessed by the SHLAA (LP 10) reference: S14/011. As detailed in LP50 this is formed of greenfield pasture land.
- 10.3 A table which can be found in appendix A of this statement contains a summary of the quantitative and a qualitative assessment on each of the purposes of the Green Belt provided in LP30 for this parcel.

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- 10.4 DS3.13 is within Green Belt parcel WY2. LP30 concluded that parcel WY2 is a mid-performing parcel with mixed scores against the five purposes of the Green Belt. It helps to prevent ribbon development along Coventry Road and Wolds Lane with much of the parcel remaining open and free from development.
- 10.5 The closest large village is Monks Kirby which is over 5.5km away. There are no boundaries in close proximity to the parcel which help prevent encroachment.
- 10.6 The JGBR makes clear that the parcel does not overlap with a Conservation Area and that all Green Belt makes a strategic contribution to urban regeneration by restricting land available for development.
- 10.7 The infilling of this parcel would be small in scale and would not therefore prevent the wider Green Belt parcel WY2 from maintaining its purpose of the Green Belt.
- 10.8 The proposed allocation would not significantly extend the boundary of the village and therefore would not mean the village became any closer to neighbouring villages.
- 10.9 Developing this site would provide a more defensible boundary for the green belt than exists at present as it would infill the area between the settlement boundary and the agricultural barn to the south. Retaining the existing hedgerow and mature trees and supplementing it with additional planting would assist in strengthening the defensible boundary (see part b for further information).

DS3.14 Wolvey Campus

- 10.10 The second site proposed for allocation at Wolvey is the Wolvey Campus, Leicester Road, Wolvey, under DS3.14 for 85 dwellings. This site was assessed by the SHLAA (LP 10a) under reference: S16/039, and occupies an area of 3.7 hectares with a developable area of up to 2.8 hectares. This is an existing employment site within the Green Belt. The Joint Green Belt Review (LP30) assessed DS3.14 as part of Broad Area 1, acknowledging that it is not considered within a parcel attached to the village boundary of Wolvey. Broad Area 1 is considered to make a significant contribution to the Green Belt. However, due to its current built form the site would have an urbanising impact on the Green Belt.
- 10.11 DS3.14 is proposed to remain within the Green Belt in order to retain control of the redevelopment of the site so that it does not adversely impact upon the Green Belt to a greater extent than at present.

- 10.12 Site DS3.14 Wolvey Campus, Leicester Road, Wolvey - Site DS3.14 is brownfield land and the present occupiers are due to leave at the end of their lease. Paragraph 111 of the NPPF seeks to encourage the effective use of land by re-using land that has been previously developed. Paragraph 89 (bullet point 6) of the NPPF also encourages the complete redevelopment of previously developed sites which would not have a greater impact on the openness of the Green Belt. The historic employment use of the site would be unlikely to be approved had it been a new planning application based on current policies. Therefore this is an opportunity to seek to recycle the land by encouraging a more suitable alternative use.
- 10.13 The regeneration of a brownfield site is considered more positive compared with selecting an alternative greenfield site, despite the fact that the site is not contiguous to the settlement boundary. The evidence for this positivity is contained within LP3 and its SA scoring in comparison with reasonable alternatives. Given that the site is not contiguous to the settlement boundary, it is thought that retaining the site within the Green Belt will better control how the redevelopment takes place, so that openness is not worsened. Existing features on site which impact the Green Belt can be removed, storey heights would need to be controlled so that they do not exceed 2 stories. This is to ensure that no intensification occurs, thereby ensuring no increased impact on the Green Belt. With careful control over how the site is developed, the site will still perform a Green Belt function, and with better design its overall function within the Green Belt can be improved.
- 10.14 The site has large areas devoted to car parking at present, indicating a high level of usage of the site by car and a high number of trips to and from the site. The site promoters confirm that most of the existing employees do not live within Wolvey. As the site is previously developed land it is considered that development can take place without the need to remove the site from the Green Belt and as a result there are no exceptional circumstances to remove it from the Green Belt.
- 10.15 Developing this site would not lead to any further encroachment into the countryside than has existed historically given that the site is already developed. Given that the site is well contained, and is being retained within the Green Belt, it would not affect existing Green Belt boundaries.

b. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable?

DS3.13: Land at Coventry Road, Wolvey

10.16 Should this site be allocated, the resulting Green Belt boundaries would be clearly defined by the existing field patterns, which contain mature hedgerows (especially in the east) and south. These features will be retained to enable the boundary to enjoy permanence and be easily recognisable and defensible in the future. This would be consistent with LP30 Paragraph 3.9 of the study which identifies "...hedgerows and tree lines..." to be recognisable but less permanent boundaries.

10.17 As a result it is considered that the site is capable of being developed whilst maintaining a clear Green Belt boundary using physical features.

DS3.14: Wolvey Campus

10.18 For Wolvey Campus, this site is not intended to be removed from the Green Belt and therefore Green Belt boundaries will remain unchanged.

c. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities

10.19 Landscape Character – Both DS3.13 and DS3.14 are located within Wolvey which is characterised in landscape terms as a High Cross Plateau, Village Farmlands landscape. Belonging to the south western section of the Leicestershire Wolds, it forms high ground between Rugby and Hinkley. The rolling plateau is dissected by a series of streams, forming valleys separated by broad, round ridges. Specifically in relation to Wolvey, this consists of predominantly small to medium landscape, with pockets of immature pasture contrasting with more open, large scale plateau landscape to the north and south of the settlement. The River Anker runs through Wolvey.

10.20 Site DS3.14 sits within zone WV-08 in the Landscape Sensitivity Study (LP35). This zone consists of small to medium scale mixed farmland on undulating ground but with a significant proportion is already developed for commercial use (Galliford Try) and includes farmed landscaped grounds to the road frontage with a number of mature trees.

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- 10.21 The existing field pattern separates the village of Wolvey from the hamlet of Wolvey Heath - this separation should be safeguarded. This zone has high and medium sensitivity to development.
- 10.22 DS3.14 could be redeveloped providing this is limited to the existing developed site. Care must be taken to ensure that views from the listed cottages and their setting are not detrimentally affected by the development. The remainder of the zone has a high sensitivity to development.
- 10.23 DS3.13 is situated within zone WV-11 and comprises a small pocket of pastoral farmland to the south of the settlement. It is relatively enclosed by roadside hedgerow. The zone includes the adjacent farm building and the land immediately around it. The farm still reads as a separate unit from the settlement.
- 10.24 The site has medium sensitivity to development. It is a single enclosed field, on the village edge, and already detached from wider farmland to a degree. As long as the roadside hedge and field hedge with mature trees is retained, the site could be developed.
- 10.25 Amongst other relevant policies in the plan, compliance with policy SDC 2 will ensure that hedgerows and mature trees are retained. There are no effects on landscape that would preclude development.
- 10.26 Heritage - Site DS3.14 – Wolvey Campus. Zone WV-08 contains a row of listed cottages with an intimate pocket of pasture that contributes to their setting. Any development of this site would need to ensure that it does not result in substantial or less than substantial harm in line with paragraphs 133 to 134 of the NPPF and in consideration of SDC3. An indicative layout by the site promoter suggests a buffer from the listed buildings. It is considered that, particularly as it is proposed to retain the site within the Green Belt, that the site is capable of being redeveloped whilst safeguarding the nearby heritage assets.
- 10.27 There are no heritage assets in close proximity to DS3.13 or within zone WV-11.
- 10.28 Amongst other relevant policies in the plan, compliance with policy SDC 3 will ensure that the heritage assets in proximity to site DS3.14 are protected from harm. There are no effects on heritage that would preclude development.

- 10.29 Biodiversity - LP41 – the Habitat and Biodiversity Audit – June 2017(LP41) provides a detailed and robust audit of the Borough's biodiversity resource, including the extent and condition of habitats so that species and habitats can be taken into account in all stages of the planning process
- 10.30 DS3.14 – Wolvey Campus is adjacent to the Wolvey Rush pasture Local Wildlife Site. This was created by the community and includes rough grassland, reed beds and woodland. Trees around this wetland, and within the churchyard, provide a rural setting to views of the church from the north. The Magic Map dataset describes the Local Wildlife Site as purple moor grass and Rush Pastures Priority Habitat. Compliance with policies within the Natural Environment chapter of the Local Plan would ensure impacts are limited and any opportunities identified.
- 10.31 The pasture associated with the row of listed cottages is a potential wildlife site.
- 10.32 The indicative layout suggests biodiversity enhancements with the opening of the culvert through the proposed open space and a buffer to the potential wildlife site (which forms the setting to the listed buildings)
- 10.33 The redevelopment of the Wolvey Campus presents the opportunity to improve the setting of the adjoining Wildlife Site and it is therefore not considered that there would be any adverse impacts that could not be mitigated in biodiversity terms.
- 10.34 DS3.13 – Land at Coventry Road, Wolvey - Other than the mature trees and hedgerow there are no other biodiversity features that are within the site or within zone WV-11.
- 10.35 Applying the safeguards contained within policy NE1, NE2 and NE4, it is considered that the impacts on development in terms of biodiversity would not prevent development taking place.
- 10.36 There are no significant impacts upon green infrastructure and biodiversity enhancements to the Wolvey Campus may result in an improvement to the adjacent Local Wildlife Site.
- 10.37 Agricultural Land - Site DS3.13 consists of grade 3 agricultural land. If the land is classified as 3a this would fall into the definition of the best and more versatile agricultural land. While land in agricultural land classifications 1 or 2 is valued more than 3a, it is acknowledge that loss of land in agricultural land classification 3a cannot be mitigated and therefore would be lost were development to go ahead

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on the site. However, in line with NPPF paragraph 112 it has been illustrated that the use of this land is necessary to ensure that the identified housing need, and more specifically the five year supply of housing, for Rugby is met sustainably.

10.38 Site DS3.14 sits within a wider grade 2 agricultural land area but since the site is a brownfield site, it has not been worked for agricultural purposes for many years, and therefore its development would not lead to the loss of current grade 2 agricultural land.

10.39 Flood Risk - Both sites are situated within Flood Zone 1 which carries a low risk of flooding. DS3.14 is adjacent to Flood Zone 3 in the area of the wetland reserve close to the site. However, the land upon which the depot is situated is on higher ground. The Environment Agency have not raised issues with either of the sites being put forward for development.

10.40 Neither site selected will have an adverse impact on flooding. Therefore both sites comply with paragraph 100 of the NPPF which seeks to direct development away from areas at highest risk.

10.41 Highway Safety - Site DS3.13: Land at Coventry Road, Wolvey. A new access will be required off Coventry Road in a 30 mph zone. The site can link to the existing footpath network. Subject to the demonstrating adequate levels of visibility the Highway Authority has no objection to this site being developed. The site promoters have undertaken a detailed traffic and visibility study to demonstrate that accessing the site will be acceptable. This is attached as appendix D. This further illustrates the lack of constraints to development on this site.

10.42 Site DS3.14: Wolvey Campus. Highway Authority has no objection to development coming forward on this site and would provide some betterment to the highway network with a reduction in the existing access arrangements for the development site. However, due to the scale of the development, the double roundabout junction between the B4109/B4065/Hall Road/The Square will require a full assessment and may need mitigation. The site promoters have indicated that the mitigation to the roundabouts would be acceptable for them to undertake.

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- 10.43 As a result it is considered that the sites selected have no significant impact upon highway safety which cannot satisfactorily be mitigated.
- 10.44 Infrastructure - Site DS3.13: Land at Coventry Road, Wolvey. As the site is a paddock there are no known infrastructure constraints in terms of developing the site.
- 10.45 Site DS3.14: Wolvey Campus - There is no evidence to suggest that the development could not accommodate proportionate highway mitigation without affecting wider infrastructure.
- 10.46 Education - Wolvey Primary School operates a 1 form of entry school. It is close to traveller sites and an army barracks and so can have quite a transient pupil population.
- 10.47 In October 2017 there were 192 children on roll with a total of 210 places. The majority of children on roll were from within the school's priority area with a limited number travelling from Bulkington. Bulkington falls within the administrative boundary of Nuneaton and Bedworth Borough Council although Warwickshire County Council is still the appropriate Education Authority. Based on current pupil yield indicators, 85 units would generate approximately 26 additional primary age pupils. With children from the proposed development site falling within the priority area for the school, the additional pupil numbers can be catered for. At secondary age, pupils from Wolvey fall within the priority area of Nicholas Chamberlaine School in Bedworth. Pupils would receive free home to school transport as is currently the case for other secondary age pupils from Wolvey. As a result the allocations would not have an adverse impact on school places.
- 10.48 Facilities - Wolvey is ranked 8th in the sustainability rankings (LP28). The rankings were weighted depending upon the importance of the service. Wolvey scored 31 points in terms of access to services.
- 10.49 Wolvey has a score of 3 in terms of access to public transport. Overall, Wolvey had an overall score of 34 in terms of sustainability.
- 10.50 Given this ranking it is considered that development of the two sites in Wolvey can take place because development will support the existing services, due to

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greater economic activity of the new residents and that fact that the school can accommodate 85 additional houses.

10.51 LP03 considered the 2 sites in terms of the Sustainability Appraisal along with the 8 other sites within Wolvey. Wolvey Campus (DS3.14) scored the best against the sustainability criteria with 9 positive scores and only 2 negative scores. The site also scored the highest against the objectives associated with townscape and regeneration, with a neutral score against the objective of efficient resource use – whereas all other sites within Wolvey are on greenfield sites and therefore scored lower in these categories.

d. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

10.52 Site DS3.13 is within walking distance to all village services, the furthest service is only 490 metres away and therefore well within a 10 minute walk time, with other services being within a five minute walk time.

10.53 Site DS3.14 is within walking distance to all village services apart from the primary school is over 800 metres from the site. The latter is 860 metres away so it would take approximately 11 minutes to walk to the school which is not considered to be excessive. The site is considered sustainable as a result.

10.54 LP28 rates Wolvey as 5th in terms of sustainability rankings within the Main Rural Settlements with an overall score of 34.

10.55 The 74 bus service runs between Coventry to Nuneaton via Wolvey at a frequency of 3 buses a day between 7.28am to 18.10pm. The 74A bus service runs between Nuneaton and Bramcote via Wolvey at 9.28 and 11.45 daily. A Flexibus service operates on Monday at 11.00 am.

e. The evidence to support the site's 'deliverability' as defined in footnote 11 of the NPPF?

10.56 Site DS3.13, Land at Coventry Road, Wolvey - The Housing Trajectory (as appended to LP11) shows 50 homes being completed in 2020-21 and the final 50 homes completed in 2021-22. The site could only be developed after the adoption

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of the Local Plan owing to its greenbelt status. Site DS3.13 is greenfield with no agricultural buildings in place on the site. Therefore it is not deemed to adversely affect deliverability.

10.57 Site DS3.14, Wolvey Campus, Leicester Road, Wolvey - has been previously developed and therefore is brownfield land. Pre-application have taken place previously and it is anticipated that a planning application will be submitted later this year. There is nothing to indicate that despite the site being brownfield that the units cannot be delivered in accordance with the housing trajectory.

f. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements

10.58 Site DS3.13 is greenfield. There are no structures to demolish on this site. Other than on site infrastructure including roads, access and SUDs there are no major infrastructure costs for the site. The viability testing (LP22) suggests greenfield sites are among the most viable with those closer to 50 dwellings the most viable compared to those that are closer to 100. At 15 dwellings for DS3.13 this sits toward the lower value.

10.59 Site DS3.14 has been previously developed and therefore is brownfield land. This will be subject to a 20% affordable housing contribution according to policy H2. This is lower than the percentage required on greenfield sites which provides a greater margin for viability.

10.60 LP22 states that reducing affordable housing to 20% in respect of brownfield land would be a positive viability response for planned development. The site promoters have undertaken an initial viability appraisal for the site at 20% affordable housing. The appraisal shows that the site is viable at 20% in the current market for scheme for 90 dwellings which is slightly more than the proposed allocation.

Coton House

1. Are the proposed residential allocations at the Main Rural Settlements identified in Policy DS3 justified as the most appropriate sites when considered against the reasonable alternatives and would they be

consistent with national policy, which particular regard for the following for each site:

a. The effect of development on the purposes of the Green Belt as set out in paragraph 80 of the NPPF?

11.1 The proposed allocation (DS3.1) at Coton House is not located within the Green Belt and as such does not have an effect on the purposes of the Green Belt.

b. Whether the resulting Green Belt boundaries would be clearly defined using physical features that are readily recognisable?

11.2 The proposed allocation (DS3.1) at Coton House is not located within the Green Belt.

c. The effect of development on landscape character, heritage, biodiversity, agricultural land, flood risk, highway safety, infrastructure and facilities.

11.3 Landscape Character - The site is within the 'High Cross Plateau – Open Plateau' landscape character type, as identified in the 2006 Landscape Assessment (LP 34). This area is classed as being of moderate sensitivity overall.

11.4 The site comprises 10.6 hectares of historic parkland as part of a wider estate which contains a Grade II* listed building: Coton House and the former stables (Grade II). The site is not subject to any specific landscape related designations.

11.5 The proposed allocation is formed of two parcels of land separated by a mature Lime Tree avenue which are each subject to Tree Preservation Orders. In addition a woodland belt of trees runs along the north – western boundary.

11.6 A Landscape and Visual Impact Assessment (LVIA) has been undertaken on the land proposed for allocation as a result of the submission of a current planning application submitted to the local planning authority for consideration under reference R15/1195, the details of which are attached as appendix E. This has not been determined at the present time. Within the parcels proposed for allocation the landscape comprises rough grassland considered to be of low ecological value.

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- 11.7 Part of the grounds of Coton House has been the subject of residential development in the past. Planning permission previously granted for 82 residential units are being built out at present. The two parcels forming the proposed allocation would extend the developable area westwards towards the A426, situated at either side of the avenue of Lime trees. Development within the grounds of Coton House have impacted upon the setting of existing heritage assets but on balance planning permission was deemed acceptable.
- 11.8 Planning permission has been granted for a Motorway Service Station under reference R17/0011 (the details of which are attached as appendix F) which will alter the landscape on the western side of the A426 in proximity to the site.
- 11.9 Given that the site is influenced by existing urban fringe activity, including large scale commercial built form to the south and major highways infrastructure, it is therefore considered that the effects of DS3.1 on the wider landscape character would not be significant. Through applying the development plan as a whole localised impacts could be mitigated by detailed design and sympathetic landscaping, including replacing lost trees which are subject to tree preservation orders.
- 11.10 Heritage - In the south west corner of the site there is a scheduled ancient monument of a mound. This is outside of the developable area and it is not considered that development would have a significant impact upon the significance and setting of this asset.
- 11.11 Churchover Conservation Area is located 1km to the north-west. It is not considered that development would adversely affect the setting of this Conservation Area to prevent the site being allocated for development.
- 11.12 Of more significance to the proposed allocation is the presence of Coton House, which is a Grade II* listed building. DS3.1 falls within its setting. The listing description is attached to this statement as Appendix G. As part of previously approved application Coton House former stables and dairy have been restored and converted to residential use. The residential development that has taken place is physically closer to the heritage asset of Coton House than the proposed allocation.
- 11.13 The Design and Access Statement, submitted alongside a Heritage Impact Assessment with planning application R15/1195 associated with the current

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submitted scheme is attached as appendix H1 and H2. Part of the allocation also includes an additional 5 dwellings to the north of the Old Dairy. A Planning application is also pending for these 5 units, R14/1739, the details of which is attached in Appendix H3 together with the associated Heritage Statement in Appendix H4.”

11.14 As stated within section 3 of the Housing Background Paper (LP11) the council accepts that there is an impact upon the setting of Coton House are (a mix of substantial harm and less than substantial harm). In line with paragraph 134 of the NPPF when less than substantial harm is apparent, the scheme has to be considered against the public benefits of the proposal. In this case the public benefit of ensuring a 5 year housing supply is maintained and meeting strategic housing needs it is considered that the benefits of providing housing in line with paragraph 47 of the NPPF outweighs the less than substantial harm.

11.15 Biodiversity – In reference to the Habitat and Biodiversity Audit 2017 (LP41) the site is more than 1km away from any designated biodiversity or geodiversity site. Churchover Meadows Local Wildlife Site is approximately 780m north of this site and Caves Inn Farm Pool Local Wildlife Site is approximately 930m to the east. Due to the distance away from these features it is unlikely that significant adverse effects would occur, although consultation with Warwickshire County Council and appropriate mitigation may be necessary.

11.16 The site contains poor semi-improved grassland which is of low ecological value and preserved mature trees. The site promoters have offered a long term Landscape and Ecological Management Plan in order to improve biodiversity. Whilst there are no Great Crested Newts on site, a small population of Great Crested Newts has been identified 270m to the south. This is not expected to be affected by redevelopment.

11.17 Six trees which are subject to Tree Preservation Orders will be lost due to access arrangements being necessary at the site entrance and between development parcels. During the development management process, the effect on these trees will be assessed and suitable mitigation being required such as landscaping and/or replacement trees. Overall it is not expected that there will be an adverse effect on biodiversity.

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

- 11.18 Agricultural Land - The site is classified as Grade 2 agricultural land. Whilst it is accepted that this constitutes one of the most best and versatile agricultural land, there is a strategic need to ensure that the Council maintains a 5 year supply of housing and meets objectively assessed housing needs in line with paragraph 47 of the NPPF.
- 11.19 Unlike allocations within the Main Rural Settlements, this is a non-Green Belt site. Not allocating Coton House would mean that more sites would be needed for Green Belt release. As a result, taking all relevant factors into account, the site allocation is, on balance, consistent with national policy despite the fact that it is situated upon grade 2 agricultural land.
- 11.20 Flood Risk - Coton House is situated within Flood Zone 1 and therefore has a low probability of flooding. It is unlikely that redevelopment of this site would be adversely affected by flooding. Therefore the site complies with paragraph 100 of the NPPF which seeks to direct development away from areas at highest risk.
- 11.21 Highways Safety - The Highways Authority response to the current planning application is attached as appendix I. It concludes that 100 homes would not have a detrimental impact upon the effective operation of the highway network based on the mitigation identified. An improvement to the M6 Junction has been committed as part of the planning permission granted for the Motorway Service Area (appendix F) providing additional capacity, and signalling the approaches. The A426 roundabout will re-align the existing crossroad junction which has a poor safety record. The Highway Authority conclude that there will be no adverse impacts upon the highway network as a result of the proposed allocation.
- 11.22 Infrastructure - The Highway Authority have requested a £200,000 contribution towards pedestrian and cycling improvements and a £100,000 contribution for a signalised pedestrian crossing linking to the MSA which will be able to provide convenience goods for the residents of the Coton House development once implemented, which is expected to be 2020/21. In addition a roundabout is also proposed to access the site from the A426 to make it safe in highway terms.

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

Subject to these pieces of infrastructure being implemented the Highway Authority has no objection.

11.23 Facilities - The X84 bus runs along the A426 and has a frequency of an hour. There is an Aldi store and a butchers off Central Park drive 900m to the south. Rugby Free School is approximately 1000m away to the south. Another primary school, Broughton Leigh Infant and Nursery School, is located on Hollowell Way to the south. A GP surgery is being constructed close proximity on Bow Fell. A Tesco Express is located on the corner of Helvellyn Way and Hollowell Way. There is a Post Office and a chemist behind this. A Spar exists on Hollowell Way. It is not considered that there will be an adverse effect on facilities as a result of the proposed development.

d. The relationship of the site to the existing settlement and its accessibility to local services and facilities?

11.24 The services provided as part of the Motorway Service Area (R17/0011) will alter the accessibility to the residents of Coton House. Within that application is a retail unit and petrol filling station which will have the ability to sell convenience goods to residents. The Highway Authority are seeking a Section 106 contribution towards a pedestrian crossing to access these facilities to improve accessibility to them.

11.25 Whilst there are no healthcare facilities within 600m of the site a GP surgery is being constructed at Bow Fell to the south. There are 3 bus stops within 600m of the site, two of which are approximately 190m south on the A426. Central Park employment site is approximately 310 m to the south. It is therefore considered that DS3.1 has relatively good access to local services and facilities and therefore reasonably sustainable.

11.26 Education - Coton House falls within the priority area for Rugby Free Primary School, Brownsover Community School (Infant), Broughton, Leigh Infant and Junior Schools. All of the schools have some limited capacity available across the year groups although it should be noted that Rugby Free Primary School has only been open for 3 years and is growing a year group at a time so currently does not have any children in Key Stage 2 (year 3 and above). Based on current pupil yield

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

indicators it is estimated that 100 homes would generate in the region of 31 additional primary age pupils. With children from the proposed development site falling within the priority area for schools with existing spare capacity, we would not envisage a problem with the school being able to cater for the impact of the proposed housing.

e. The evidence to support the sites ‘deliverability’ as defined in footnote 11 of the NPPF?

11.27 Coton House is identified within the housing trajectory as appended LP11 to deliver 100 houses by 2022-23. The site promoters support this view and have already submitted planning application R15/1195 for 100 homes and are pressing for a decision without delay. As stated in LP11 the allocation of land at Coton House will be a continuation of development within the area providing an opportunity for housing provision throughout the plan period at a relatively consistent rate. Notwithstanding the heritage constraints described above, there are no actual development constraints or infrastructure requirements which prevent the site being developed within the first five years of the Local Plan following adoption.

f. Their viability having regard to the provision of any infrastructure, affordable housing and other policy requirements

11.28 The site is a greenfield site, with no structures to demolish. Other than on site infrastructure including roads, access (which will require tree removal) there are no major infrastructure costs for delivery. The viability testing (LP22) suggests greenfield sites are among the most viable. At 100 dwellings when considered against Local Plan policy H2: Affordable Housing 30% affordable housing be required. The site promoters have confirmed the scheme would be policy compliant. There are no indications that this would not be able to be accommodated in viability terms.

2. Is the proposed allocation at Coton House (DS3.1) justified and consistent with national policy, particularly in respect of its effect on the setting of Coton House, agricultural land and landscape character?

12.1 The proposed allocation at Coton House (DS3.1) is located within countryside. Unlike the proposed MRS allocations it is therefore not located in the Green Belt

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

and there is therefore no need for demonstration of very special circumstances for release. The allocation has been positively prepared, since the allocation responds to strategic housing needs which have been objectively assessed. The allocation is a spatial expression of the need to provide for adequate housing which complies with 47 footnote 11 of the NPPF given that the site is available now, offers a suitable location for development now, and is achievable with a realistic prospect that housing can be delivered on site within 5 years and is viable.

12.2 The site has been selected in a positive manner because the site selections have been arrived at considering reasonable alternatives. The allocations for housing are a fundamental part of the overall housing trajectory and represent a variation on the urban focussed growth of the previous Core Strategy which did not deliver the expected growth as quickly as was envisioned. They are justified because they meet the objectively assessed needs in line with paragraph 47 of the NPPF. They are effective because they specifically have been selected as a result of discussions with willing developers in line with the requirements of paragraph 54 of the NPPF since this approach is responsive to local circumstances by planning housing responsive to local needs. As a result the allocations are consistent with national policy and the most appropriate given the need to diversity the housing market of Rugby central area

12.3 It is correct that the allocation is in proximity to Coton House, a Grade II* listed building and former Stable block, a Grade II listed building and therefore consideration of paragraph 134 of the NPPF is relevant. This paragraph relates to the fact that where a proposal would lead to less than substantial harm to the significance of the heritage asset. As a result this harm should be weighed against the public benefits of the proposal.

12.4 It is acknowledged that development within the proposed allocations would have an impact upon the setting of the listed buildings. There is public benefit in ensuring that Rugby Council complies with paragraph 47 of the NPPF by providing for its housing needs and maintains a 5 year supply of housing. This site, together with the Main Rural Settlements will contribute towards the housing trajectory and is especially important for the Council in terms of maintaining a 5 year supply of housing. The economic activity associated with the build, and the economic activity of the new residents will benefit the local economy

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

- 12.5 In terms of sustainability, the granting of planning permission of a Service Station opposite the site (see appendix F) will benefit local residents at the Coton House development as they would be able to access convenience goods at that location which is within walking distance.
- 12.6 As detailed in LP22 at paragraph 3.58 it is considered that DS3.1 will make a valuable contribution to housing land supply at the point of adoption. There are no other alternative sites of the scale and location identified in the SHLAA that are considered to be suitable and deliverable. Subject to appropriate design including retention of key views and approaches, it is considered that harm can be mitigated through the application of relevant policies in the local plan including SDC3 and engagement with relevant consultees. The site is therefore appropriate for allocation following consideration of the alternatives and the benefits that the site can provide in meeting housing needs within the plan period.

**Appendix A- Green Belt Study Summary Tables- Adapted from LP30 Coventry
and Warwickshire Joint Green Belt Review (2015)**

Binley Woods- Green Belt Parcel BW1

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	1	<ul style="list-style-type: none"> The parcel plays a role in preventing ribbon development along Rugby Road. The parcel has some development in form of a farm house and buildings. This does compromise the openness of the Green Belt in the immediate vicinity but the majority is undeveloped and open.
To prevent neighbouring towns merging into one another.	4	<ul style="list-style-type: none"> The south western corner of the parcel is less than 250m from Coventry.
To assist in safeguarding the countryside from encroachment	1-2	<ul style="list-style-type: none"> The land within the parcel has the characteristics of countryside with the exception of some limited urbanising development. There is no barrier to prevent the encroachment of development from Binley Woods into this parcel however development here would represent a significant breach of the A46 defensible boundary between Binley Woods and Coventry.
To preserve the setting and special character of historic towns	0	<ul style="list-style-type: none"> The parcel does not overlap with a Conservation Area within an historic town.
To assist in urban regeneration by encouraging the recycling of derelict and other urban land.	+4	<ul style="list-style-type: none"> All Green Belt makes a contribution to urban regeneration by restricting the land available for development and encouraging developers to seek out and recycle derelict/urban sites.

Brinklow- Green Belt Parcel BR2

Matter 4- Non- Strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	1-2	<ul style="list-style-type: none"> The parcel plays a role in preventing ribbon development along Lutterworth Road. The vast majority of the parcel is open and free from development.
To prevent neighbouring towns merging into one another.	2	<ul style="list-style-type: none"> Measured from the northernmost tip of the village, the closest neighbouring settlement to the north east of Brinklow is Stretton under Fosse 1.8km.
To assist in safeguarding the countryside from encroachment	1-2	<ul style="list-style-type: none"> The land within the parcel has the characteristics of countryside with only agricultural or isolated dwellings. The M6 to the north and the railway line to the east help prevent encroachment of the parcel into the wider countryside but these are a considerable distance from the edge of the village.
To preserve the setting and special character of historic towns	0	<ul style="list-style-type: none"> The parcel does not overlap with a Conservation Area within an historic town.
To assist in urban regeneration by encouraging the recycling of derelict and other urban land.	+4	<ul style="list-style-type: none"> All Green Belt makes a contribution to urban regeneration by restricting the land available for development and encouraging developers to seek out and recycle derelict/urban sites.

Long Lawford- Green Belt Parcel LL2

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	0-2	<ul style="list-style-type: none"> The study identifies that the parcel plays a limited role in limiting ribbon development as ribbon development has already occurred along the Coventry Road to the West and East of the parcel. As the sites surroundings have

Matter 4- Non- Strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

		<p>already been developed, the parcel will in no way contribute to unrestricted sprawl owing to the sites existing confines. The study identifies that the parcel is heavily screened and there is no development within the parcel itself.</p>
To prevent neighbouring towns merging into one another.	0	<ul style="list-style-type: none"> The study assesses the gap between Rugby and Long Lawford, which is identified as being 800 metres at its closest point. Development has already occurred either side of the parcel along the Coventry Road. The eastern elevation of the site is closest to the Rugby Urban Area, but as this has already been subject to development any development within the parcel would not contribute in any way to narrowing the gap between Rugby and Long Lawford. The study therefore concludes that the parcel plays a limited role in maintaining the separation between Rugby and Long Lawford.
To assist in safeguarding the countryside from encroachment	0-2	<ul style="list-style-type: none"> The parcel itself is undeveloped, although there is development to three sides. Owing to the parcel having not been developed, it is not considered to be encroached upon. The Coventry Road to the south provides a significant boundary to any further development.
To preserve the setting and special character of historic towns	0	<ul style="list-style-type: none"> The parcel is not within a Conservation Area and is not in close proximity to one. Therefore it is not considered to contribute to the setting and special character of Rugby.
To assist in urban regeneration by encouraging the	+4	<ul style="list-style-type: none"> LP30 identifies that all green belt land makes a contribution to urban regeneration, although it is not possible to quantify the impact of a single

Matter 4- Non- Strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

recycling of derelict and other urban land.		site owing to how the HMA functions as a single entity.
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Ryton on Dunsmore- Green Belt Parcel RD2

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	1-2	<ul style="list-style-type: none"> The parcel plays a role in preventing ribbon development to the east and west of Ryton on Dunsmore. The site contains several pockets of development scattered through it which compromise the openness of the parcel.
To prevent neighbouring towns merging into one another.	2	<ul style="list-style-type: none"> Measured through the centre of the parcel the village of Ryton on Dunsmore lies roughly 2km away from the village of Stretton on Dunsmore.
To assist in safeguarding the countryside from encroachment	1	<ul style="list-style-type: none"> There are several pockets of development scattered through the parcel but some remaining areas are still in rural character and relatively open. The Meadowlands fisheries are important but do not represent a significant boundary to encroachment to the east for the whole parcel.
To preserve the setting and special character of historic towns	2	<ul style="list-style-type: none"> The parcel does not overlap with a Conservation Area within an historic town but the high ground within the parcel does have some impact on the setting of Coventry.
To assist in urban regeneration by encouraging the recycling of derelict and other urban land.	+4	<ul style="list-style-type: none"> All Green Belt makes a contribution to urban regeneration by restricting the land available for development and encouraging developers to seek out and recycle derelict/urban sites.

Stretton on Dunsmore, The Old Orchard- Green Belt Parcel SD4

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	1-2	<ul style="list-style-type: none"> Green Belt review states that this parcel will prevent ribbon development along Fineacre Lane and Plott Lane. Some agricultural buildings compromise the openness of the green belt but not to a large extent.
To prevent neighbouring towns merging into one another.	2	<ul style="list-style-type: none"> The nearest village of Princethorpe is 1.5 km away.
To assist in safeguarding the countryside from encroachment	2	<ul style="list-style-type: none"> The parcel has characteristics of countryside consisting of agricultural land and buildings which do not have an urbanising effect. There are no significant boundaries that would prevent encroachment of development into the countryside within the parcel or directly beyond it.
To preserve the setting and special character of historic towns	0	<ul style="list-style-type: none"> The parcel does not overlap with a Conservation Area.
To assist in urban regeneration by encouraging the recycling of derelict and other urban land.	+4	<ul style="list-style-type: none"> All Green Belt makes a strategic contribution to urban regeneration by restricting land available for development.

Stretton on Dunsmore, Land off Squires Road- Green Belt Parcel SD1

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	1-2	<ul style="list-style-type: none"> Green Belt review states that this parcel will prevent ribbon development along Fineacre Lane and Plott Lane. Some agricultural

Matter 4- Non- Strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

		buildings compromise the openness of the green belt but not to a large extent.
To prevent neighbouring towns merging into one another.	2	<ul style="list-style-type: none"> There is a gap between Stretton –on-Dunsmore and Ryton-on-Dunsmore to the north-west.
To assist in safeguarding the countryside from encroachment	1-2	<ul style="list-style-type: none"> The parcel has characteristics of countryside consisting of agricultural land and buildings which do not have an urbanising effect. The A45 dual carriageway which forms the northern boundary of this parcel serves as a significant boundary to prevent the encroachment of development into the countryside.
To preserve the setting and special character of historic towns	0	<ul style="list-style-type: none"> The parcel does not overlap with a Conservation Area.
To assist in urban regeneration by encouraging the recycling of derelict and other urban land.	+4	<ul style="list-style-type: none"> All Green Belt makes a strategic contribution to urban regeneration by restricting land available for development.

Wolston- Green Belt Parcel WN2

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	2-1	<ul style="list-style-type: none"> The parcel plays a role in preventing ribbon development along Wolston and Stretton Road. The parcel contains several buildings some of which are significant in scale, most notably Ryton Gardens. In addition there are some farms and several residential dwellings which are concentrated in the south eastern corner.

Matter 4- Non- Strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

		The majority of the parcel is undeveloped and open agricultural fields.
To prevent neighbouring towns merging into one another.	2	<ul style="list-style-type: none"> The village of Stretton-on-Dunsmore is located roughly 2km to the south and Ryton on Dunsmore is 2km to the west.
To assist in safeguarding the countryside from encroachment	1-2	<ul style="list-style-type: none"> Ryton Gardens and its car park is the most urbanising feature within the parcel. All the buildings within the parcel compromise the openness of the Green Belt however the majority of the parcel is undeveloped and open agricultural fields. The southern border of the parcel follows the A45 dual carriageway which is a significant boundary however this is a significant distance from the village.
To preserve the setting and special character of historic towns	2	<ul style="list-style-type: none"> The parcel does not overlap with a Conservation Area within an historic town. However there is a clear long range intervisibility with the historic core of Coventry.
To assist in urban regeneration by encouraging the recycling of derelict and other urban land.	+4	<ul style="list-style-type: none"> All Green Belt makes a strategic contribution to urban regeneration by restricting land available for development.

Wolvey, Land at Coventry Road – Green Belt Parcel WY2

Purpose	Score	Notes
To check the unrestricted sprawl of large built up areas.	1-2	<ul style="list-style-type: none"> The parcel helps to prevent ribbon development along Coventry Road and Wolds Lane.

Matter 4- Non- Strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

		<ul style="list-style-type: none"> The large agricultural barn immediately to the south compromises the openness of the Green Belt in its immediate vicinity. Much of the parcel remains open and free from development.
To prevent neighbouring towns merging into one another.	0	<ul style="list-style-type: none"> Monks Kirby is 5.5km away.
To assist in safeguarding the countryside from encroachment	2	<ul style="list-style-type: none"> There are no boundaries within or in close proximity to the parcel that help prevent encroachment.
To preserve the setting and special character of historic towns	0	<ul style="list-style-type: none"> The parcel does not overlap with a Conservation Area.
To assist in urban regeneration by encouraging the recycling of derelict and other urban land.	+4	<ul style="list-style-type: none"> All Green Belt makes a strategic contribution to urban regeneration by restricting land available for development.

Wolvey, Land at Coventry Road – Green Belt Broad Area 1

Broad Area 1 lies between Nuneaton to the west, Coventry to the south west and Hinkley and Lutterworth to the east (with the A5 forming the outer Green Belt

boundary). It is one of the largest parcels and is not assessed in the same way as the others in this Appendix. The Green belt purposes to which this parcel makes more of a contribution are:

- Checking the sprawl of Coventry, Nuneaton and Bedworth;
- Preventing the merging of neighbouring towns in the long term;
- Safeguarding the countryside; and
- Assisting urban regeneration by encouraging the recycling of derelict and other urban land across the West Midlands.

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton
House (Policies DS3 and DS6)

**Appendix B- Decision Notice for R14/2164, Land at Former Allotment, Plott Lane,
Stretton-on-Dunsmore**



THE RUGBY BOROUGH COUNCIL

TOWN AND COUNTRY PLANNING ACT 1990 (AS AMENDED)

REFUSAL OF PLANNING PERMISSION

Notice is hereby given that the Borough Council in pursuance of its powers under the above mentioned Act, as amended and Rules, Orders and Regulations made there under, refuses planning permission for the development referred to hereunder for the reasons specified.

APPLICATION NUMBER

R14/2164

DATE APPLICATION VALID

05/11/2014

ADDRESS OF DEVELOPMENT

LAND AT FORMER ALLOTMENT
PLOTT LANE
STRETTON-ON-DUNSMORE

APPLICANT/AGENT

Mr Bob Faxon
Schoonberg Walker & Associates
3 Sunnyside
The Green
Broadwell
Rugby
Warwickshire
CV23 8HD

On behalf of Mr John Taylor

APPLICATION DESCRIPTION

FULL: NEW DWELLING HOUSE

REASON FOR REFUSAL & RELEVANT DEVELOPMENT PLAN POLICIES

REASON FOR REFUSAL 1:

The site is located in the Green Belt where there is a presumption against inappropriate development. It is the policy of the Local Planning Authority, as set out in the Development Plan and having regard to the NPPF not to grant planning permission except in very special circumstances, for new buildings other than for the purposes of agriculture and forestry, outdoor sports and recreation facilities, cemeteries and other uses which preserve the openness of the Green Belt and which do not conflict with the purposes of including land in it, for the limited extension, alteration or replacement of existing buildings and for limited infill in specified villages.

It is considered that the proposed dwelling. Therefore the proposed dwelling constitutes inappropriate development which is, by definition, harmful to the Green Belt and would have adverse impact on the openness of the Green Belt.

In the opinion of the Local Planning Authority, there are no special circumstances, which would justify the granting of planning permission for a dwelling in the face of a strong presumption against inappropriate development derived from the prevailing policies. The proposed development is therefore contrary to policy CS1 of the Rugby Borough Core Strategy 2011 and the NPPF.

REASON FOR REFUSAL 2:

The proposal would be detrimental to highway safety in that there is insufficient visibility available (to the left on egress) such that emerging drivers will have inadequate opportunity to determine whether it

IMPORTANT – PLEASE READ THE NOTES ATTACHED TO THIS DECISION NOTICE

is safe to manoeuvre, and approaching vehicles will have insufficient opportunity to slow down (or stop) if necessary.

REASON FOR REFUSAL 3:

The proposal is considered by virtue of its size, siting, massing, height and prominent location to be unsympathetic to the appearance and character of the street scene and if approved would constitute a prominent and obtrusive feature within the street scene which would be detrimental to the visual amenities of the area and character of the area and openness of the Green Belt. The proposal is therefore contrary to Policy CS1 and CS16 of the Rugby Borough Core Strategy June 2011 and the National Policy Framework (NPPF).

REASON FOR REFUSAL 4:

The proposed development does not constitute a rural exceptions site as defined by policy CS21 of the Rugby Borough Core Strategy 2011 as the proposal is for a market house, no local need for affordable housing has been demonstrated, the development would adversely affect the character of the area and the site is not located adjacent to an existing settlement. The proposal is therefore contrary to this policy.

RELEVANT DEVELOPMENT PLAN POLICIES & GUIDANCE:

The National Planning Policy Framework March 2012 (NPPF)

Policies CS1, CS16, CS21, and NPPF

The development plan policies referred to above are available for inspection on the Rugby Borough Council's web-site www.rugby.gov.uk or at the Council Offices.

STATEMENT OF POSITIVE ENGAGEMENT:

In dealing with this application Rugby Borough Council has actively sought to work with the applicant in a positive and proactive manner, in accordance with paragraphs 186 and 187 of the NPPF.



PLANNING DEPARTMENT,
TOWN HALL,
EVREUX WAY,
RUGBY,
CV21 2RR

NICK FREER
Development & Enforcement Manager

DATE: 11/12/2014

NOTES

Appeals to the Secretary of State

If you are aggrieved by the decision of your local planning authority to refuse permission for the proposed development or to grant it subject to conditions, then you can appeal to the Secretary of State for Communities & Local Government under Section 78 of the Town and Country Planning Act 1990.

If you want to appeal against a decision to refuse planning permission for a householder or minor commercial application, you must do so within 12 weeks of the date of this notice, in all other cases you must do so within six months of the date of this notice, using a Planning Appeal Form which you can get from the Planning Inspectorate at Temple Quay House, 2 The Square, Temple Quay, Bristol, BS1 6PN Tel: 0117 372 6372 or online at <http://www.planningportal.gov.uk/planning/appeals>. If requesting forms from the Planning Inspectorate please state the appeal form you require. The Secretary of State can allow a longer period for giving notice of an appeal, but he will not normally be prepared to use this power unless there are special circumstances which excuse the delay in giving notice of appeal.

If this is a decision on a planning application relating to the same or substantially the same land and development as is already the subject of an enforcement notice, if you want to appeal against your local planning authority's decision on your application, then you must do so within 28 days of the date of this notice. If an enforcement notice is served relating to the same or substantially the same land and development as in your application and if you want to appeal against your local planning authority's decision on your application, then you must do so within 28 days of the date of service of the enforcement notice, or within six months [12 weeks in the case of a householder appeal] of the date of this notice, whichever period expires earlier.

The Inspectorate will publish details of your appeal on the internet (on the Appeals area of the Planning Portal). This may include a copy of the original planning application form and relevant supporting documents supplied to the local authority by you or your agent, together with the completed appeal form and information you submit to the Planning Inspectorate. Please ensure that you only provide information, including personal information belonging to you that you are happy it will be made available to others in this way. If you supply personal information belonging to a third party please ensure you have their permission to do so. More detailed information about data protection and privacy matters is available on the Planning Portal.

The Secretary of State need not consider an appeal if it seems to him that the local planning authority could not have granted planning permission for the proposed development or could not have granted it without the conditions it imposed, having regard to the statutory requirements, to the provisions of the development order and to any directions given under the order.

In practice, the Secretary of State does not refuse to consider appeals solely because the local planning authority based its decision on a direction given by him.

Please note, only the applicant possesses the right of appeal.

Purchase Notices

If either the local planning authority or the Secretary of State for Communities & Local Government refuses permission to develop land or grants it subject to conditions, the owner may claim that he can neither put the land to a reasonably beneficial use in its existing state nor can he render the land capable of a reasonably beneficial use by the carrying out of any development which has been or would be permitted.

In these circumstances, the owner may serve a purchase notice on the Council in whose area the land is situated. This notice will require the Council to purchase his interest in the land in accordance with the provisions of Part VI of the Town and Country Planning Act 1990.

Compensation

In certain circumstances compensation may be claimed from the local planning authority if permission is refused or granted subject to conditions by the Secretary of State on appeal or on reference of the application to him. These circumstances are set out in Section 114 and related provisions of the Town and Country Planning Act 1990.

Other Legislation

This decision does not grant any right or approval under other legislation. You will have to apply separately for Building Regulations approval and for consent to undertake works, or place scaffolds, hoardings or skips within the highway.

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton
House (Policies DS3 and DS6)

**Appendix C- Decision Notice for R13/0250, Land at North of Squires Road, Squires
Road, Stretton-on-Dunsmore**



THE RUGBY BOROUGH COUNCIL

TOWN AND COUNTRY PLANNING ACT 1990 (AS AMENDED)

REFUSAL OF PLANNING PERMISSION

Notice is hereby given that the Borough Council in pursuance of its powers under the above mentioned Act, as amended and Rules, Orders and Regulations made there under, refuses planning permission for the development referred to hereunder for the reasons specified.

APPLICATION NUMBER

R13/0250

DATE APPLICATION VALID

12/02/2013

ADDRESS OF DEVELOPMENT

LAND AT NORTH OF SQUIRES ROAD
SQUIRES ROAD
STRETTON-ON-DUNSMORE

APPLICANT/AGENT

Mr Laurence Wilbraham
Wilbraham Associates
18a Regent Place
Rugby
Warwickshire
CV21 2PN

On behalf of Amberville Properties Ltd

APPLICATION DESCRIPTION

Outline application for residential development (up to 47 dwellings) with estate road and associated works (access not reserved).

REASON FOR REFUSAL & RELEVANT DEVELOPMENT PLAN POLICIES

REASON FOR REFUSAL: 1

The site is located in the Green Belt where there is a presumption against inappropriate development. It is the policy of the Local Planning Authority, as set out in the NPPF not to grant planning permission except in very special circumstances, for new buildings other than for the purposes of agriculture and forestry, outdoor sports and recreation facilities, cemeteries and other uses which preserve the openness of the Green Belt and which do not conflict with the purposes of including land in it, for the limited extension, alteration or replacement of existing buildings and for limited infill in specified villages.

The proposed residential development constitutes inappropriate development which is, by definition, harmful to the Green Belt. It would also cause harm to the visual amenity, openness and permanence of the Green Belt and the purposes of including land within it.

There are no special circumstances, which would justify the granting of planning permission for residential development in the face of a strong presumption against inappropriate development derived from the prevailing policies. The proposed development is therefore contrary to the NPPF, policies CS2 and CS16 of the Rugby Borough Core Strategy 2012, saved policy GP2 of the Rugby Borough Local Plan 2006 and the Stretton on Dunsmore Parish Plan 2005.

REASON FOR REFUSAL: 2

There are ponds in the vicinity of the site that are likely to provide suitable habitat for Great Crested Newts. A survey has not been provided to allow the impact on Great Crested Newts to be assessed. The application could therefore adversely impact this protected species and is contrary to the NPPF and saved policy E6 of the Rugby Borough Local Plan 2006.

IMPORTANT – PLEASE READ THE NOTES ATTACHED TO THIS DECISION NOTICE

RELEVANT DEVELOPMENT PLAN POLICIES & GUIDANCE:

Rugby Borough Core Strategy 2011 - CS1, CS2, CS10, CS11, CS16, CS17, CS19

Rugby Borough Local Plan 2006, Saved policies - GP2, E6, T5, H12, LR1

Sustainable Design and Construction SPD, 2012

Planning Obligations SPD, 2012

Affordable Housing SPD, 2012

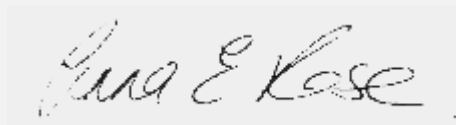
Stretton on Dunsmore Local Housing Needs Survey, 2011

Stretton on Dunsmore Parish Plan 2005

The development plan policies referred to above are available for inspection on the Rugby Borough Council's web-site www.rugby.gov.uk or at the Council Offices.

STATEMENT OF POSITIVE ENGAGEMENT:

In dealing with this application Rugby Borough Council has actively sought to work with the applicant in a positive and proactive manner, in accordance with paragraphs 186 and 187 of the NPPF, however, in this case it has not been possible to reach agreement.



PLANNING DEPARTMENT,
TOWN HALL,
EVREUX WAY,
RUGBY,
CV21 2RR

ANNA E. ROSE Head of Planning and Culture

DATE: 22/05/2013

NOTES

Appeals to the Secretary of State

If you are aggrieved by the decision of your local planning authority to refuse permission for the proposed development or to grant it subject to conditions, then you can appeal to the Secretary of State for Communities & Local Government under Section 78 of the Town and Country Planning Act 1990.

If you want to appeal against a decision to refuse planning permission for a householder application, you must do so within 12 weeks of the date of this notice using a Householder Planning Appeal Form, in all other cases you must do so within six months of the date of this notice, using a Planning Appeal Form which you can get from the Planning Inspectorate at Temple Quay House, 2 The Square, Temple Quay, Bristol, BS1 6PN Tel: 0117 372 6372 or online at <http://www.planningportal.gov.uk/planning/appeals>. If requesting forms from the Planning Inspectorate please state the appeal form you require. The Secretary of State can allow a longer period for giving notice of an appeal, but he will not normally be prepared to use this power unless there are special circumstances which excuse the delay in giving notice of appeal.

If this is a decision on a planning application relating to the same or substantially the same land and development as is already the subject of an enforcement notice, if you want to appeal against your local planning authority's decision on your application, then you must do so within 28 days of the date of this notice. If an enforcement notice is served relating to the same or substantially the same land and development as in your application and if you want to appeal against your local planning authority's decision on your application, then you must do so within 28 days of the date of service of the enforcement notice, or within six months [12 weeks in the case of a householder appeal] of the date of this notice, whichever period expires earlier.

The Inspectorate will publish details of your appeal on the internet (on the Appeals area of the Planning Portal). This may include a copy of the original planning application form and relevant supporting documents supplied to the local authority by you or your agent, together with the completed appeal form and information you submit to the Planning Inspectorate. Please ensure that you only provide information, including personal information belonging to you that you are happy it will be made available to others in this way. If you supply personal information belonging to a third party please ensure you have their permission to do so. More detailed information about data protection and privacy matters is available on the Planning Portal.

The Secretary of State need not consider an appeal if it seems to him that the local planning authority could not have granted planning permission for the proposed development or could not have granted it without the conditions it imposed, having regard to the statutory requirements, to the provisions of the development order and to any directions given under the order.

In practice, the Secretary of State does not refuse to consider appeals solely because the local planning authority based its decision on a direction given by him.

Please note, only the applicant possesses the right of appeal.

Purchase Notices

If either the local planning authority or the Secretary of State for Communities & Local Government refuses permission to develop land or grants it subject to conditions, the owner may claim that he can neither put the land to a reasonably beneficial use in its existing state nor can he render the land capable of a reasonably beneficial use by the carrying out of any development which has been or would be permitted.

In these circumstances, the owner may serve a purchase notice on the Council in whose area the land is situated. This notice will require the Council to purchase his interest in the land in accordance with the provisions of Part VI of the Town and Country Planning Act 1990.

Compensation

In certain circumstances compensation may be claimed from the local planning authority if permission is refused or granted subject to conditions by the Secretary of State on appeal or on reference of the application to him.

These circumstances are set out in Section 114 and related provisions of the Town and Country Planning Act 1990.

Other Legislation

This decision does not grant any right or approval under other legislation. You will have to apply separately for Building Regulations approval and for consent to undertake works, or place scaffolds, hoardings or skips within the highway.

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

Appendix D- Letter from Bancroft Consulting to County Highways regarding highways and transportation advice for 15 dwellings at Coventry Road, Wolvey.

Bancroft Consulting Limited

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North Warwickshire Area Office
County Highways
Coleshill Heath Road
Coleshill
Birmingham
B46 3HL
FAO: Mr Chris Lucas

Our Ref: AG/F14018/281114
Date: 5 December 2014

BY EMAIL ONLY

Dear Chris,

LAND AT COVENTRY ROAD, WOLVEY

We have been appointed to provide highways and transportation advice in respect of a development of up to 15 dwellings on land at Coventry Road, Wolvey. However, our Client has confirmed that there could be the potential to extend the development into a adjoining land (residential gardens) at a future date, therefore serving an additional 35 dwellings (50 dwellings in total).

In light of the above, the purpose of this letter is to identify whether an access could be provided at Coventry Road to serve the proposed development of 15 dwellings. It shall seek to confirm whether there could be the potential to serve adjoining land from the same proposed access at Coventry Road, to accommodate an additional 35 dwellings. Furthermore, the letter also includes an initial consideration of other pertinent issues such as off-site impact and access by non-car modes, for both potential schemes.

Existing conditions

The site measures a total area of approximately 0.36 hectares and consists of undeveloped land. It is served from a single gated access at the northern end of the site frontage, on Coventry Road. The access measures approximately 3.5 metres wide at the site frontage and includes a gate that is set back approximately 5.4 metres from the edge of carriageway. The site is bound by residential properties to the north, residential gardens and undeveloped land to the east (potential developable land), a farm to the south and Coventry Road to the west.

Coventry Road itself measures approximately 7 metres wide past the site frontage and includes street lighting. It is subject to a 30mph speed limit in the vicinity of the site, which increases to the national speed limit (60mph) approximately 48 metres south. It also includes a footway measuring between 1.8 and 2 metres wide along the western edge and a verge between approximately 2.4 and 4 metres wide along its eastern edge. To the north of the site, along the eastern edge, Coventry Road includes a footway with an average width of 1.6 metres extending

towards the Village Centre. Approximately 135 metres north of the site, Coventry Road includes an illuminated pedestrian refuge with dropped kerbs and tactile paving.

The nearest bus stops are located on Coventry Road approximately 160 metres north of the site. These consist of a flag and pole arrangement on the western side of the carriageway and a shelter at the eastern side. These stops are served by Route Numbers 74, 210 and 213, operating at a combined frequency of approximately one bus every two hours in each direction, from Monday to Saturday. These routes connect the site to areas including Coventry, Nuneaton, Bramcote and Rugby.

A site visit was undertaken on 25 November 2014, where key measurements were taken along Coventry Road and a vehicle speed survey was carried out between 1000 and 1155 hours, during which time the weather was clear and dry. A total of 200 vehicle speeds were recorded, comprising 100 vehicles in each direction. A copy of the speed survey results are enclosed in **Tables 1 and 2**, which demonstrate 85th percentile wet weather speeds of 31.78mph (51.13kph) in the northbound direction and 32.08mph (51.61kph) in the southbound direction.

Site access

Previous discussions with the local highway authority have confirmed that recommendations contained within 'Transport for Streets and Development: The Warwickshire Guide 2001' do not reflect current best practice guidance contained within Manual for Streets. The design guide is due to be updated, however until this document is available the local highway authority has advised that best practice guidance contained within Manual for Streets (MfS) and Manual for Streets 2 (MfS2), would be an appropriate reference for the proposed access design, whilst also taking into consideration local characteristics and issues.

In line with the local highway authority's previous advice it is recommended that the residential access should include a 5 metres wide carriageway, along with 8 metres junction radii. Figure 7.1 of MfS highlights 4.8 metres to be a sufficient width for a car and HGV to pass and Paragraph 6.8.7 of MfS confirms that a carriageway width of 5 metres is sufficient for refuse collection vehicles. Therefore, the proposed site access should be suitable to accommodate any potential HGV movements, such as deliveries to a property and internal refuse collection, should it be required. It is also proposed that 2 metres wide footways extend into the site on both sides of the carriageway, which should be suitable to accommodate the proposed development of 15 as well as the potential additional 35 dwellings (50 dwellings in total). The proposed site access layout is demonstrated within **Drawing Number F14018/01**. As per guidance contained in MfS2, there should be no requirement for a ghost-island right turn lane at the access.

Visibility requirements

The precise visibility splays at the proposed site access for the development have been calculated using the measured 85th percentile speeds and current best practice guidance contained within MfS2, which recommends within Paragraph 1.3.2 that 'as a starting point for any scheme affecting non-trunk roads, designers should start with MfS'. Paragraph 10.1.13 of MfS2 provides a detailed formula for calculating visibility splays, taking into account the percentage of HGV movements and vehicle speeds. During the speed survey, it was noted that more than 5% of vehicles were HGV's in both directions. The above results show that the vehicle speeds were below 60kph in both directions. Table 10.1 in MfS2 provides specific criteria for calculating the Stopping Sight Distance. Hence, the splay requirements have been calculated by adopting a 0.375g deceleration rate and 1.5 seconds reaction time. **Tables 3 and 4** show that by adopting this approach visibility splays of 52 metres to the north and 51 metres to the south should be provided, taken from a 2.4 metres set back distance.

Drawing Number F14018/01 demonstrates that the required visibility splays of 52 metres to the north and 51 metres to the south can be achieved in both directions from 2.4 metres setback

distance. These are taken to the edge of carriageway, within land that appears to be public highway based on on-site observations.

Change in traffic conditions

The TRICS database was examined to identify suitable trip rates to calculate the potential peak hour and daily traffic movements that could be generated by the proposed residential development. The category 'Residential – Houses Privately Owned' was searched, specifying a range of between 5 and 100 dwellings, excluding sites in Greater London and Ireland, as well weekend surveys. This search resulted in 19 surveys taken from 19 sites. None of the sites were considered to be a direct comparison. Therefore the 85th percentile site (TRICS reference CH-03-A-05) was used in this instance for robustness. Full details of the TRICS search are enclosed.

The following trip rates (per dwelling) were therefore deemed appropriate for the proposed development:

- morning peak (0800 to 0900 hours) 0.235 arrive 0.588 depart
- evening peak (1700 to 1800 hours) 0.353 arrive 0.412 depart
- daily 2.705 arrive 3.293 depart

Based on the above trip rates, the proposed development of 15 dwellings would generate the following vehicle movements:

- morning peak 4 arrive 9 depart 13 total
- evening peak 5 arrive 6 depart 11 total
- daily 41 arrive 49 depart 90 total

The above results are shown as a full daily profile in **Table 5**.

Based on the above trip rates, the potential overall development of 50 dwellings would generate the following vehicle movements:

- morning peak 12 arrive 29 depart 41 total
- evening peak 18 arrive 21 depart 39 total
- daily 135 arrive 165 depart 300 total

The above results are shown as a full daily profile in **Table 6**.

'Guidance on Transport Assessment' [DfT, March 2007] suggests that the analysis period for any proposed development should primarily address the weekday morning and evening peak periods. It advises that developments may only have a significant highway impact where increases of 30 or more two-way vehicle movements occur during peak hours. The above traffic generation calculations indicate that the proposed 15 dwelling development would result in an increase of only 13 movements during the morning peak and 11 movements in the evening peak. Hence it is evident that the small increase in traffic associated with the proposed development should not result in any detrimental impact on highway safety, or cause substantial congestion issues within the surrounding highway network. Therefore, no further detailed assessment of the impact of these increases on the surrounding highway network should be necessary.

The above traffic generation calculations indicate that the potential larger development of 50 dwellings would result in an increase of 41 movements during the morning peak and 39 movements in the evening peak. This exceeds the 30 two-way vehicle movements threshold highlighted within 'Guidance on Transport Assessment'. However, this would still be well below one vehicle per minute turning in/out of the access during peak hours, and so it is considered that the proposed access would operate satisfactorily in terms of capacity. Beyond the access, the site is located with Leicester, Nuneaton, Hinckley and the M1 to the north and Rugby, Coventry and the M6 (leading to Birmingham) to the south. It is therefore reasonable to assume that traffic associated with the development would distribute fairly evenly along routes to the north and south beyond the site access. Hence, beyond the site access the 30 two-way vehicle movements threshold should not be exceeded.

Highway safety

Data on the website www.crashmap.co.uk shows that there have been no recorded accidents on Coventry Road in the vicinity of the site, between 2005 and 2013. Taking into account the amount of direct accesses along Coventry Road, it is reasonable to conclude that the additional movements associated with either development scenario (15 or 50 dwellings) should not result in any highway safety issues.

Servicing

In line with the recommendations contained within 'Manual for Streets', the internal site layout should accommodate a refuse collection vehicle parking at the kerbside adjacent to all dwellings, or manoeuvring to within 25 metres walking distance for refuse workers of any designated collection points. Any bin collection points should also be located within a 30 metres walking distance for residents of the dwellings they serve. If the site layout should exceed these distances and internal refuse collection is required, then the masterplan should include suitable turning areas for service vehicles.

Parking

Previous discussions with the local highway authority have confirmed that recommendations contained within 'Transport for Streets and Development: The Warwickshire Guide 2001' do not reflect current best practice guidance. Therefore, 'Residential Car Parking Research' (Department for Communities and Local Government, [DCLG] May 2007) has been used to calculate the potential level of parking demand and the necessary parking provision to accommodate this demand. The 'Residential Car Parking Research' calculation methodology is based on the specific number of rooms within each dwelling. The size of dwellings has not yet been confirmed, so it has been assumed that all of the properties shall be three-bed houses (equivalent to 6 rooms) for the purpose of this letter. Therefore, the car parking requirements have been calculated based on the above figures for owner-occupied houses in a rural area. The document suggests the following levels of parking demand per dwelling, based on the number of allocated off-street spaces for each dwelling:

Six room dwelling (3 bed-rooms)

- No allocated bays per dwelling = demand of 1.8 vehicles per dwelling
- 1 allocated bay per dwelling = demand of 1.8 vehicles per dwelling
- 2 allocated bays per dwelling = demand of 2.2 vehicles per dwelling

Based on the above evidence, the proposed development is likely to generate an overall demand for between 27 and 33 car parking spaces for the 15 dwelling development, and between 90 and 110 spaces for the larger 50 dwelling development, depending on the number of allocated bays for each dwelling. To allow for vehicles parking within the garages, paragraph 3.225 of the 6C's Design Guide, confirms that any double garage should have internal dimensions of 6 x 6 metres, with a minimum door width of 4.2 metres. Single garages should have internal dimensions of 6 x 3 metres, with a minimum door width of 2.3 metres.

Access by non-car modes

The proposed development would generate only a minimal increase in pedestrian movements, which should be satisfactorily accommodated within the existing infrastructure. The site access layout includes footways to tie into existing footways on Coventry Road, to assist with pedestrian movements associated with the development. The Village Centre is approximately 200 metres north of site frontage and includes a Post Office, a General Store and 'The Blue Pig' public house and 'The Bull's Head' public house and restaurant. There are no formal cycle routes within the vicinity of the site. However, the surrounding roads are considered suitable to accommodate any minimal increase in cyclist movements within the carriageway.

The proposed development would only be likely to generate a minimal increase in bus passenger movements. Nevertheless, it is considered that any additional bus passenger movements could be satisfactorily accommodated by the existing services that operate on Coventry Road north of the site. The nearest bus stops are located on Coventry Road approximately 160 metres north of the site.

Summary

Hopefully, the above and enclosed details should provide adequate information for you to confirm the local highway authority's 'in principle' support for the proposed access arrangement, which should be suitable to serve the initial development of 15 dwellings with the potential to increase the development by 35 dwellings without the need for any amendment. This assessment also demonstrates that the proposed 15 dwelling development would generate a minimal traffic increases within the surrounding highway network and the increases associated with the potential 50 dwelling development should not result in any significant off-site impact. Residents would also have reasonable opportunities to travel by non-car modes within the existing infrastructure. It is therefore considered that the proposed development would comply with current planning policy and best practice design guidance.

Therefore, I look forward to receiving confirmation that the local highway authority would be in a position to support the proposals for the initial 15 dwelling development. In the meantime, please do not hesitate to contact us should you have any questions or require further information.

Yours sincerely

Chris Bancroft

Director

Bancroft Consulting

t: 0115 9602919

m: 07786 966615

e: office@bancroftconsulting.co.uk

enc. Table 1 - Coventry Road northbound speed survey results
Table 2 - Coventry Road southbound speed survey results
Table 3 - Visibility splay calculator – Coventry Road northbound speeds
Table 4 - Visibility splay calculator – Coventry Road southbound speeds
Table 5 - Proposed 15 dwelling development traffic generation profile
Table 6 - Potential 50 dwelling development traffic generation profile
Drawing Number F14018/01 - Proposed Site Access Layout

cc. Mr Phil Rowland - Landmark Planning

observed speed mph x	no. of readings f	fx	fx ²
10	0	0	0
11	0	0	0
12	0	0	0
13	0	0	0
14	0	0	0
15	0	0	0
16	1	16	256
17	0	0	0
18	0	0	0
19	0	0	0
20	1	20	400
21	1	21	441
22	2	44	968
23	5	115	2645
24	4	96	2304
25	7	175	4375
26	3	78	2028
27	11	297	8019
28	8	224	6272
29	13	377	10933
30	9	270	8100
31	6	186	5766
32	1	32	1024
33	2	66	2178
34	5	170	5780
35	7	245	8575
36	4	144	5184
37	5	185	6845
38	5	190	7220
39	0	0	0
40	0	0	0
41	0	0	0
42	0	0	0
43	0	0	0
44	0	0	0
45	0	0	0
46	0	0	0
47	0	0	0
48	0	0	0
49	0	0	0
50	0	0	0
51	0	0	0
52	0	0	0
53	0	0	0
54	0	0	0
55	0	0	0
56	0	0	0
57	0	0	0
58	0	0	0
59	0	0	0
60	0	0	0
61	0	0	0
62	0	0	0
63	0	0	0
64	0	0	0
65	0	0	0
66	0	0	0
67	0	0	0
68	0	0	0
69	0	0	0
70	0	0	0
71	0	0	0
72	0	0	0
73	0	0	0
74	0	0	0
75	0	0	0
76	0	0	0
77	0	0	0
78	0	0	0
79	0	0	0
80	0	0	0
Total Σ	100	2951	89313

SPEED READINGS

location: **Coventry Road, Wolvey**
direction: **Northbound**
day: **Tuesday**
date: **25.11.14**
time: **1000 to 1155**

SUMMARY

mean 29.51 mph 47.48 kph
85%ile 34.26 mph 55.12 kph
wet 85%ile 31.78 mph 51.13 kph

Mean speed

$$\bar{x} = \frac{\sum fx}{\sum f} = 29.51 \text{ mph}$$

Standard deviation

$$S_x = \sqrt{\frac{1}{\sum f - 1} \left[\sum fx^2 - \frac{(\sum fx)^2}{\sum f} \right]} = 4.75 \text{ mph}$$

85 percentile dry weather spot speed

$$\bar{x} + S_x = 34.26 \text{ mph}$$

85 percentile wet weather journey speed

$$\bar{x} + S_x - 2.478 = 31.78 \text{ mph}$$

checks: 85%ile/mean = 1.16
should be 1.1 to 1.25

S.D./mean = 0.16
should be approx 1/6 (0.17)

TABLE 1: COVENTRY ROAD NORTHBOUND SPEED SURVEY RESULTS

observed speed mph x	no. of readings f	fx	fx ²
10	0	0	0
11	0	0	0
12	0	0	0
13	0	0	0
14	0	0	0
15	0	0	0
16	0	0	0
17	2	34	578
18	0	0	0
19	1	19	361
20	2	40	800
21	1	21	441
22	3	66	1452
23	1	23	529
24	2	48	1152
25	6	150	3750
26	4	104	2704
27	9	243	6561
28	7	196	5488
29	13	377	10933
30	11	330	9900
31	8	248	7688
32	8	256	8192
33	4	132	4356
34	2	68	2312
35	2	70	2450
36	3	108	3888
37	6	222	8214
38	1	38	1444
39	2	78	3042
40	0	0	0
41	0	0	0
42	1	42	1764
43	1	43	1849
44	0	0	0
45	0	0	0
46	0	0	0
47	0	0	0
48	0	0	0
49	0	0	0
50	0	0	0
51	0	0	0
52	0	0	0
53	0	0	0
54	0	0	0
55	0	0	0
56	0	0	0
57	0	0	0
58	0	0	0
59	0	0	0
60	0	0	0
61	0	0	0
62	0	0	0
63	0	0	0
64	0	0	0
65	0	0	0
66	0	0	0
67	0	0	0
68	0	0	0
69	0	0	0
70	0	0	0
71	0	0	0
72	0	0	0
73	0	0	0
74	0	0	0
75	0	0	0
76	0	0	0
77	0	0	0
78	0	0	0
79	0	0	0
80	0	0	0
Total Σ	100	2956	89848

SPEED READINGS

location: **Coventry Road, Wolvey**
direction: **Southbound**
day: **Tuesday**
date: **25.11.14**
time: **1000 to 1155**

SUMMARY

mean 29.56 mph 47.56 kph
85%ile 34.55 mph 55.60 kph
wet 85%ile 32.08 mph 51.61 kph

Mean speed

$$\bar{x} = \frac{\sum fx}{\sum f} = 29.56 \text{ mph}$$

Standard deviation

$$S_x = \sqrt{\frac{1}{\sum f - 1} \left[\sum fx^2 - \frac{(\sum fx)^2}{\sum f} \right]} = 4.99 \text{ mph}$$

85 percentile dry weather spot speed

$$\bar{x} + S_x = 34.55 \text{ mph}$$

85 percentile wet weather journey speed

$$\bar{x} + S_x - 2.478 = 32.08 \text{ mph}$$

checks: 85%ile/mean = 1.17
should be 1.1 to 1.25

S.D./mean = 0.17
should be approx 1/6 (0.17)

TABLE 2: COVENTRY ROAD SOUTHBOUND SPEED SURVEY RESULTS

Vehicle speeds	31.78 mph 51.13 kph 14.20 v (m/s) 201.75 v ²	Formula: $SSD = vt + v^2 / 2(d + 0.1a)$	<table border="1"> <tr> <th colspan="2">Manual for Streets 2</th> <th>DMRB</th> </tr> <tr> <td>Light Vehicles (less than 5% HGVs)</td> <td>HGVs/Buses (over 5% of total vehicles)</td> <td>All traffic</td> </tr> <tr> <td>1.5s</td> <td>1.5s</td> <td>2s</td> </tr> <tr> <td>0.45g</td> <td>0.375g</td> <td>0.25g</td> </tr> </table>		Manual for Streets 2		DMRB	Light Vehicles (less than 5% HGVs)	HGVs/Buses (over 5% of total vehicles)	All traffic	1.5s	1.5s	2s	0.45g	0.375g	0.25g
Manual for Streets 2			DMRB													
Light Vehicles (less than 5% HGVs)	HGVs/Buses (over 5% of total vehicles)	All traffic														
1.5s	1.5s	2s														
0.45g	0.375g	0.25g														
Driver Perception-Reaction time	1.5 t (s) 21.31 v x t	<table border="1"> <tr> <td>Perception-Reaction Time (t)</td> <td></td> </tr> <tr> <td>Deceleration Rate (g = 9.81m/s²)</td> <td></td> </tr> </table>	Perception-Reaction Time (t)		Deceleration Rate (g = 9.81m/s ²)											
Perception-Reaction Time (t)																
Deceleration Rate (g = 9.81m/s ²)																
Deceleration Rate	0.375 g 3.68 d (m/s) 7.36 2d	Enter gradient as positive for uphill towards junction and negative for downhill towards junction														
Gradient	0.00 a* 3.68 d+0.1a 7.3575 2(d+0.1a)															
Stopping Sight Distance (SSD) =	v t + v ² / 2(d+0.1a) 21.31 + 27.42 = 48.73															
SSD Bonnet Adjusted (SSD+2.4)**	51.13															

* for simplicity, gradient will be given as zero where details of levels are unavailable and observed gradients are deemed to be insignificant in terms of the effect on vehicle braking

** 2.4 metres added to splay to allow for bonnet length of approaching vehicles

TABLE 3: VISIBILITY SPLAY CALCULATOR - COVENTRY ROAD NORTHBOUND

Vehicle speeds	32.08 mph 51.62 kph 14.34 v (m/s) 205.58 v ²	Formula: $SSD = vt + v^2 / 2(d + 0.1a)$	<table border="1"> <tr> <th colspan="2">Manual for Streets 2</th> <th>DMRB</th> </tr> <tr> <td>Light Vehicles (less than 5% HGVs)</td> <td>HGVs/Buses (over 5% of total vehicles)</td> <td>All traffic</td> </tr> <tr> <td>1.5s</td> <td>1.5s</td> <td>2s</td> </tr> <tr> <td>0.45g</td> <td>0.375g</td> <td>0.25g</td> </tr> </table>		Manual for Streets 2		DMRB	Light Vehicles (less than 5% HGVs)	HGVs/Buses (over 5% of total vehicles)	All traffic	1.5s	1.5s	2s	0.45g	0.375g	0.25g
Manual for Streets 2			DMRB													
Light Vehicles (less than 5% HGVs)	HGVs/Buses (over 5% of total vehicles)	All traffic														
1.5s	1.5s	2s														
0.45g	0.375g	0.25g														
Driver Perception-Reaction time	1.5 t (s) 21.51 v x t	<table border="1"> <tr> <td>Perception-Reaction Time (t)</td> <td></td> </tr> <tr> <td>Deceleration Rate (g = 9.81m/s²)</td> <td></td> </tr> </table>	Perception-Reaction Time (t)		Deceleration Rate (g = 9.81m/s ²)											
Perception-Reaction Time (t)																
Deceleration Rate (g = 9.81m/s ²)																
Deceleration Rate	0.375 g 3.68 d (m/s) 7.36 2d	Enter gradient as positive for uphill towards junction and negative for downhill towards junction														
Gradient	0.00 a* 3.68 d+0.1a 7.3575 2(d+0.1a)															
Stopping Sight Distance (SSD) =	<table> <tr> <td>v t</td> <td>+</td> <td>v² / 2(d+0.1a)</td> <td>=</td> <td>SSD</td> </tr> <tr> <td>21.51</td> <td>+</td> <td>27.94</td> <td>=</td> <td>49.45</td> </tr> </table>	v t	+	v ² / 2(d+0.1a)	=	SSD	21.51	+	27.94	=	49.45					
v t	+	v ² / 2(d+0.1a)	=	SSD												
21.51	+	27.94	=	49.45												
SSD Bonnet Adjusted (SSD+2.4)**	51.85															

* for simplicity, gradient will be given as zero where details of levels are unavailable and observed gradients are deemed to be insignificant in terms of the effect on vehicle braking

** 2.4 metres added to splay to allow for bonnet length of approaching vehicles

TABLE 4: VISIBILITY SPLAY CALCULATOR - COVENTRY ROAD SOUTHBOUND

Time Period	Trip Rates (per dwelling)		Traffic Generation (15 dwellings)		
	Arrive	Depart	Arrive	Depart	Total
0700 - 0800	0.059	0.294	1	4	5
0800 - 0900	0.235	0.588	4	9	13
0900 - 1000	0.176	0.294	3	4	7
1000 - 1100	0.059	0.235	1	4	5
1100 - 1200	0.059	0.235	1	4	5
1200 - 1300	0.176	0.118	3	2	5
1300 - 1400	0.235	0.176	4	3	7
1400 - 1500	0.294	0.059	4	1	5
1500 - 1600	0.353	0.412	5	6	11
1600 - 1700	0.412	0.176	6	3	9
1700 - 1800	0.353	0.412	5	6	11
1800 - 1900	0.294	0.294	4	4	8
Daily	2.705	3.293	41	49	90

TABLE 5: PROPOSED 15 DWELLING DEVELOPMENT TRAFFIC GENERATION PROFILE

Time Period	Trip Rates (per dwelling)		Traffic Generation (50 dwellings)		
	Arrive	Depart	Arrive	Depart	Total
0700 - 0800	0.059	0.294	3	15	18
0800 - 0900	0.235	0.588	12	29	41
0900 - 1000	0.176	0.294	9	15	24
1000 - 1100	0.059	0.235	3	12	15
1100 - 1200	0.059	0.235	3	12	15
1200 - 1300	0.176	0.118	9	6	15
1300 - 1400	0.235	0.176	12	9	21
1400 - 1500	0.294	0.059	15	3	18
1500 - 1600	0.353	0.412	18	21	39
1600 - 1700	0.412	0.176	21	9	30
1700 - 1800	0.353	0.412	18	21	39
1800 - 1900	0.294	0.294	15	15	30
Daily	2.705	3.293	135	165	300

TABLE 6: POTENTIAL 50 DWELLING DEVELOPMENT TRAFFIC GENERATION PROFILE

TRIP RATE CALCULATION SELECTION PARAMETERS:

Land Use : 03 - RESIDENTIAL
 Category : A - HOUSES PRIVATELY OWNED
 VEHICLES

Selected regions and areas:

02	SOUTH EAST	
	ES EAST SUSSEX	1 days
	SC SURREY	1 days
03	SOUTH WEST	
	WL WILTSHIRE	1 days
05	EAST MIDLANDS	
	DS DERBYSHIRE	1 days
06	WEST MIDLANDS	
	SH SHROPSHIRE	2 days
	WK WARWICKSHIRE	1 days
	WM WEST MIDLANDS	1 days
	WO WORCESTERSHIRE	1 days
07	YORKSHIRE & NORTH LINCOLNSHIRE	
	NY NORTH YORKSHIRE	3 days
08	NORTH WEST	
	CH CHESHIRE	1 days
	GM GREATER MANCHESTER	1 days
09	NORTH	
	CB CUMBRIA	2 days
11	SCOTLAND	
	EA EAST AYRSHIRE	1 days
	HI HIGHLAND	2 days

This section displays the number of survey days per TRICS® sub-region in the selected set

Filtering Stage 2 selection:

This data displays the chosen trip rate parameter and its selected range. Only sites that fall within the parameter range are included in the trip rate calculation.

Parameter: Number of dwellings
 Actual Range: 9 to 99 (units:)
 Range Selected by User: 7 to 100 (units:)

Public Transport Provision:

Selection by: Include all surveys

Date Range: 01/01/06 to 23/01/14

This data displays the range of survey dates selected. Only surveys that were conducted within this date range are included in the trip rate calculation.

Selected survey days:

Monday	3 days
Tuesday	4 days
Wednesday	2 days
Thursday	7 days
Friday	3 days

This data displays the number of selected surveys by day of the week.

Selected survey types:

Manual count	19 days
Directional ATC Count	0 days

This data displays the number of manual classified surveys and the number of unclassified ATC surveys, the total adding up to the overall number of surveys in the selected set. Manual surveys are undertaken using staff, whilst ATC surveys are undertaken using machines.

Selected Locations:

Edge of Town	18
Neighbourhood Centre (PPS6 Local Centre)	1

This data displays the number of surveys per main location category within the selected set. The main location categories consist of Free Standing, Edge of Town, Suburban Area, Neighbourhood Centre, Edge of Town Centre, Town Centre and Not Known.

Selected Location Sub Categories:

Residential Zone	14
No Sub Category	5

This data displays the number of surveys per location sub-category within the selected set. The location sub-categories consist of Commercial Zone, Industrial Zone, Development Zone, Residential Zone, Retail Zone, Built-Up Zone, Village, Out of Town, High Street and No Sub Category.

Filtering Stage 3 selection:

Use Class:

C3	19 days
----	---------

This data displays the number of surveys per Use Class classification within the selected set. The Use Classes Order 2005 has been used for this purpose, which can be found within the Library module of TRICS®.

Population within 1 mile:

1,001 to 5,000	3 days
5,001 to 10,000	7 days
10,001 to 15,000	5 days
15,001 to 20,000	2 days
20,001 to 25,000	1 days
25,001 to 50,000	1 days

This data displays the number of selected surveys within stated 1-mile radii of population.

Population within 5 miles:

5,001 to 25,000	2 days
25,001 to 50,000	4 days
75,001 to 100,000	6 days
100,001 to 125,000	4 days
250,001 to 500,000	2 days
500,001 or More	1 days

This data displays the number of selected surveys within stated 5-mile radii of population.

Car ownership within 5 miles:

0.6 to 1.0	4 days
1.1 to 1.5	15 days

This data displays the number of selected surveys within stated ranges of average cars owned per residential dwelling, within a radius of 5-miles of selected survey sites.

Travel Plan:

Yes	1 days
No	18 days

This data displays the number of surveys within the selected set that were undertaken at sites with Travel Plans in place, and the number of surveys that were undertaken at sites without Travel Plans.

LIST OF SITES relevant to selection parameters

1	CB-03-A-03 SEMI DETACHED HAWKSHEAD AVENUE WORKINGTON Edge of Town Residential Zone Total Number of dwellings: 40 Survey date: THURSDAY 20/11/08	CUMBRIA	Survey Type: MANUAL
2	CB-03-A-04 SEMI DETACHED MOORCLOSE ROAD SALTERBACK WORKINGTON Edge of Town No Sub Category Total Number of dwellings: 82 Survey date: FRIDAY 24/04/09	CUMBRIA	Survey Type: MANUAL
3	CH-03-A-05 DETACHED SYDNEY ROAD SYDNEY CREWE Edge of Town Residential Zone Total Number of dwellings: 17 Survey date: TUESDAY 14/10/08	CHESHIRE	Survey Type: MANUAL
4	DS-03-A-01 SEMI D./TERRACED THE AVENUE HOLMESDALE DRONFIELD Neighbourhood Centre (PPS6 Local Centre) Residential Zone Total Number of dwellings: 20 Survey date: THURSDAY 22/06/06	DERBYSHIRE	Survey Type: MANUAL
5	EA-03-A-01 DETACHED TALISKER AVENUE KILMARNOCK Edge of Town Residential Zone Total Number of dwellings: 39 Survey date: THURSDAY 05/06/08	EAST AYRSHIRE	Survey Type: MANUAL
6	ES-03-A-02 PRIVATE HOUSING SOUTH COAST ROAD PEACEHAVEN Edge of Town Residential Zone Total Number of dwellings: 37 Survey date: FRIDAY 18/11/11	EAST SUSSEX	Survey Type: MANUAL
7	GM-03-A-10 DETACHED/SEMI BUTT HILL DRIVE PRESTWICH MANCHESTER Edge of Town Residential Zone Total Number of dwellings: 29 Survey date: WEDNESDAY 12/10/11	GREATER MANCHESTER	Survey Type: MANUAL

LIST OF SITES relevant to selection parameters (Cont.)

8	HI -03-A-11	BUNGALOWS		HIGHLAND
	STEVENSON ROAD			
	INSHES			
	INVERNESS			
	Edge of Town			
	Residential Zone			
	Total Number of dwellings:	85		
	Survey date: MONDAY	05/06/06		Survey Type: MANUAL
9	HI -03-A-13	HOUSING		HIGHLAND
	KINGSMILLS ROAD			
	INVERNESS			
	Edge of Town			
	Residential Zone			
	Total Number of dwellings:	9		
	Survey date: THURSDAY	21/05/09		Survey Type: MANUAL
10	NY-03-A-07	DETACHED & SEMI DET.		NORTH YORKSHIRE
	CRAVEN WAY			
	BOROUGHBRIDGE			
	Edge of Town			
	No Sub Category			
	Total Number of dwellings:	23		
	Survey date: TUESDAY	18/10/11		Survey Type: MANUAL
11	NY-03-A-10	HOUSES AND FLATS		NORTH YORKSHIRE
	BOROUGHBRIDGE ROAD			
	RIPON			
	Edge of Town			
	No Sub Category			
	Total Number of dwellings:	71		
	Survey date: TUESDAY	17/09/13		Survey Type: MANUAL
12	NY-03-A-11	PRIVATE HOUSING		NORTH YORKSHIRE
	HORSEFAIR			
	BOROUGHBRIDGE			
	Edge of Town			
	Residential Zone			
	Total Number of dwellings:	23		
	Survey date: WEDNESDAY	18/09/13		Survey Type: MANUAL
13	SC-03-A-04	DETACHED & TERRACED		SURREY
	HIGH ROAD			
	BYFLEET			
	Edge of Town			
	Residential Zone			
	Total Number of dwellings:	71		
	Survey date: THURSDAY	23/01/14		Survey Type: MANUAL
14	SH-03-A-03	DETACHED		SHROPSHIRE
	SOMERBY DRIVE			
	BICTON HEATH			
	SHREWSBURY			
	Edge of Town			
	No Sub Category			
	Total Number of dwellings:	10		
	Survey date: FRIDAY	26/06/09		Survey Type: MANUAL

LIST OF SITES relevant to selection parameters (Cont.)

15	SH-03-A-05 SANDCROFT SUTTON HILL TELFORD Edge of Town Residential Zone Total Number of dwellings: Survey date: THURSDAY	SEMI -DETACHED/TERRACED 54 24/10/13	SHROPSHIRE Survey Type: MANUAL
16	WK-03-A-02 NARBERTH WAY POTTERS GREEN COVENTRY Edge of Town Residential Zone Total Number of dwellings: Survey date: THURSDAY	BUNGALOWS 17 17/10/13	WARWICKSHIRE Survey Type: MANUAL
17	WL-03-A-01 MAPLE DRIVE WOOTTON BASSETT Edge of Town Residential Zone Total Number of dwellings: Survey date: MONDAY	SEMI D./TERRACED W. BASSETT 99 02/10/06	WILTSHIRE Survey Type: MANUAL
18	WM-03-A-03 BASELEY WAY ROWLEYS GREEN COVENTRY Edge of Town Residential Zone Total Number of dwellings: Survey date: MONDAY	MIXED HOUSING 84 24/09/07	WEST MIDLANDS Survey Type: MANUAL
19	WO-03-A-02 MEADOWHILL ROAD REDDITCH Edge of Town No Sub Category Total Number of dwellings: Survey date: TUESDAY	SEMI DETACHED 48 02/05/06	WORCESTERSHIRE Survey Type: MANUAL

This section provides a list of all survey sites and days in the selected set. For each individual survey site, it displays a unique site reference code and site address, the selected trip rate calculation parameter and its value, the day of the week and date of each survey, and whether the survey was a manual classified count or an ATC count.

Bancroft Consulting Mercury House, New Basford Nottingham

Licence No: 539501

RANK ORDER for Land Use 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED
VEHICLES

Ranking Type: TOTALS

Time Range: 08:00-09:00

WARNING: Using 85th and 15th percentile highlighted trip rates in data sets of under 20 surveys is not recommended by TRICS and may be misleading.

15th Percentile = No. 16 NY-03-A-07 Tot: 0.478

85th Percentile = No. 4 CH-03-A-05 Tot: 0.823

Median Values

Arrivals: 0.000

Departures: 0.565

Totals: 0.565

Mean Values

Arrivals: 0.200

Departures: 0.422

Totals: 0.622

Rank	Site-Ref	Description	Town/City	Area	DWELLS	Day	Date	Trip Rate (Sorted by Totals)			Travel Plan
								Arrivals	Departures	Totals	
1	HI-03-A-13	HOUSING	INVERNESS	HIGHLAND	9	Thu	21/05/09	0.556	0.444	1.000	
2	WK-03-A-02	BUNGALOWS	COVENTRY	WARWICKSHIRE	17	Thu	17/10/13	0.588	0.353	0.941	
3	GM-03-A-10	DETACHED/SEMI	MANCHESTER	GREATER MANCHESTER	29	Wed	12/10/11	0.138	0.759	0.897	
4	CH-03-A-05	DETACHED	CREWE	CHESHIRE	17	Tue	14/10/08	0.235	0.588	0.823	
5	WM-03-A-03	MIXED HOUSING	COVENTRY	WEST MIDLANDS	84	Mon	24/09/07	0.321	0.405	0.726	
6	NY-03-A-10	HOUSES AND FLA	RIPON	NORTH YORKSHIRE	71	Tue	17/09/13	0.183	0.521	0.704	
7	SH-03-A-03	DETACHED	SHREWSBURY	SHROPSHIRE	10	Fri	26/06/09	0.200	0.500	0.700	
8	CB-03-A-03	SEMI DETACHED	WORKINGTON	CUMBRIA	40	Thu	20/11/08	0.225	0.450	0.675	
9	EA-03-A-01	DETACHED	KILMARNOCK	EAST AYRSHIRE	39	Thu	05/06/08	0.231	0.359	0.590	
10	NY-03-A-11	PRIVATE HOUSIN	BOROUGHBRIDGE	NORTH YORKSHIRE	23	Wed	18/09/13	0.000	0.565	0.565	
11	HI-03-A-11	BUNGALOWS	INVERNESS	HIGHLAND	85	Mon	05/06/06	0.129	0.424	0.553	
12	CB-03-A-04	SEMI DETACHED	WORKINGTON	CUMBRIA	82	Fri	24/04/09	0.183	0.366	0.549	
13	SH-03-A-05	SEMI-DETACHED/	TELFORD	SHROPSHIRE	54	Thu	24/10/13	0.130	0.370	0.500	
14	SC-03-A-04	DETACHED & TER	BYFLEET	SURREY	71	Thu	23/01/14	0.141	0.352	0.493	
15	ES-03-A-02	PRIVATE HOUSIN	PEACEHAVEN	EAST SUSSEX	37	Fri	18/11/11	0.081	0.405	0.486	Yes
16	NY-03-A-07	DETACHED & SEM	BOROUGHBRIDGE	NORTH YORKSHIRE	23	Tue	18/10/11	0.087	0.391	0.478	
17	WO-03-A-02	SEMI DETACHED	REDDITCH	WORCESTERSHIRE	48	Tue	02/05/06	0.104	0.333	0.437	
18	WL-03-A-01	SEMI D./TERRAC	WOOTTON BASSETT	WILTSHIRE	99	Mon	02/10/06	0.071	0.333	0.404	
19	DS-03-A-01	SEMI D./TERRAC	DRONFIELD	DERBYSHIRE	20	Thu	22/06/06	0.200	0.100	0.300	

This section displays actual (not average) trip rates for each of the survey days in the selected set, and ranks them in order of relative trip rate intensity, for a given time period (or peak period irrespective of time) selected by the user. The count type and direction are both displayed just above the table, along with the rows within the table representing the 85th and 15th percentile trip rate figures (highlighted in bold within the table itself).

The table itself displays details of each individual survey, alongside arrivals, departures and totals trip rates, sorted by whichever of the three directional options has been chosen by the user. As with the preceding trip rate calculation results table, the trip rates shown are per the calculation factor (e.g. per 100m2 GFA, per employee, per hectare, etc). Note that if the peak period option has been selected (as opposed to a specific chosen time period), the peak period for each individual survey day in the table is also displayed.

Site Reference: CH-03-A-05 Multi-Modal Site
 Created: Version: 2009(a)v6.3.1 27/01/09
 Latitude/Longitude: 53.1017, -2.4188
 Land Use Type: 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED
 Region/Area: NORTH WESTCHESHIRE
 Version/Creation Date: 2009(a)v6.3.1 27/01/09

Description: DETACHED
 Street: SYDNEY ROAD
 District: SYDNEY
 Town: CREWE
 Post Code: CW1 5FR

Location: Edge of Town
 Location Sub Category: Residential Zone
 Use Class: C3

Population within 500m: 1200
 Population within 1 Mile: 5,001 to 10,000
 Population within 5 Miles: 100,001 to 125,000
 Car ownership within 5 Miles: 1.1 to 1.5

Public Transport Provision Summary

Day	Period	Total buses/trams within 400m	Total Trains within 1000m	Total Services
Monday-Friday	0700-1900	9		9
Monday-Friday	0700-1000	3		3
Monday-Friday	1600-1900	1		1
Saturday	0700-1900	9		9
Sunday	0700-1900	8		8

Is site associated with a travel plan: No
 If not, are there any plans to implement a Travel Plan in the future? No
 Is survey data available before the implementation of the Travel Plan?
 Is the location of the site hilly or flat: Flat
 Urban Regeneration: No

Site area: 0.84 hect
 Number of dwellings: 17
 Housing Density: 23.29

No. of developments for this Site: 1
 No. of survey Days for this Site: 1

Comments

This site is located at the eastern edge of Crewe, near the A534, which heads north-east out of town and south-west via Crewe railway station. Other local roads head towards various parts of town.
 The site has a single access for all modes, off Sydney Road.
 There is open land to the east, with mainly residential streets in other directions. A pub is also nearby.

Bus (or tram) site accessibility

3. Is there at least 1 bus (or tram) stop within the site frontage or within 400m of the site frontage? : Yes

11. Please enter general comments/views about the relevance, quality and importance of public transport services relating to this development.

There are 9 buses per day locally, which run a one-way circular route around Crewe, via Elm Drive.

Design features encouraging non-car modes

12. Pedestrians

None

13. Pedal cycles

None

14. Public transport

The site is in proximity to a local bus route.

Design features encouraging non-car modes

Road Network Distance to Local Developments	
Year of Analysis	2009
Nearest Primary School	0.6 kilometres
Nearest Secondary School	1.1 kilometres
Nearest Local Shop/Corner Shop	0.8 kilometres
Nearest Main Supermarket	1.4 kilometres
Nearest Doctors Surgery	0.8 kilometres
Nearest Hospital with Minor Injuries/A & E	4.2 kilometres
Nearest Sports/Leisure Centre	1.1 kilometres

Census Data	
Year of Census	2001
Census Output Area/Data Zone	
Number of people employed within Census Output Area	214
Number of households within Census Output Area	126
Number of people living within Census Output Area	367
Area of Census Output Area (hectares)	11.00
Population density within Census Output Area (per hectare)	34.11

Site reference: CH-03-A-05 Multi-Modal survey site
Trade name: BENTLEY DRIVE

Site area (h/a): 0.84
Site area excluding public open spaces (h/a): 0.73

Open since 1987

Occupied dwellings 17
Unoccupied dwellings
Total dwellings

Housing Density 23.29
Privately owned units 17
Non-Privately owned units 0
Name of nearest site STANIER CLOSE
Distance to nearest similar site 0 Km

Average Bedrooms Per Unit 3.94
No of units with 1 bedroom 0
No of units with 2 bedrooms 2
No of units with 3 bedrooms 14
No of units with 4+ bedrooms 1
Total bedrooms 67
Unit Density 20.2

Residential unit types

	Private	Non-Private	Total
Detached houses	17	0	17
Semi-detached houses	0	0	0
Terraced houses	0	0	0
Bungalows	0	0	0
Flats (in houses)	0	0	0
Flats (in blocks)	0	0	0
Other (specify below)			

Other:

Comments

The nearest similar site is located less than 1 kilometre away.

Multi-Modal survey site

On-Site parking

Total no. of parking spaces	63
Spaces Per Hectare	75.000
Spaces Per dwelling	3.706

Number of spaces

On-Street	5
Driveway	40
Garages	18
Communal parking spaces	0

Off-Site parking details

Is there off-site parking available

Yes

Off-Site parking included in the counts

Yes

Free On-Street parking available nearby

Yes

If yes, considered easy to find a space

Yes

If prepared to pay, easy to find somewhere to park off-site all day

No

Parking restrictions

Area subject to parking restrictions (controlled parking zone - CPZ)

No

Off-Street parking

Off-Street parking available NO

Park & Ride

Park & Ride Type Facility providing relevant means of accessing the site

No

Site reference: CH-03-A-05

Survey date: 14/10/08

Day of week: Tuesday

Multi-Modal survey site

Vehicles surveyed: Total vehicles

Survey type: Manual Count

AM weather: Mild and Light Rain

PM weather: Mild and Heavy Rain

Initial car park occupancy:

Final car park occupancy:

BRACKETED ACCUMULATION FIGURES ARE NOT ABSOLUTE

Parking Capacity

Data proportions in %

Motor cars	88	Motor cycles	0	Public service	0
Light goods	10	OGV (1)	0	OGV (2)	0
				Taxis	2

Time	Arr 46	Dep 56	Totals	Parking Accum
00:00-01:00				
01:00-02:00				
02:00-03:00				
03:00-04:00				
04:00-05:00				
05:00-06:00				
06:00-07:00				
07:00-08:00	1	5	6	(-4)
08:00-09:00	4	10	14	(-10)
09:00-10:00	3	5	8	(-12)
10:00-11:00	1	4	5	(-15)
11:00-12:00	1	4	5	(-18)
12:00-13:00	3	2	5	(-17)
13:00-14:00	4	3	7	(-16)
14:00-15:00	5	1	6	(-12)
15:00-16:00	6	7	13	(-13)
16:00-17:00	7	3	10	(-9)
17:00-18:00	6	7	13	(-10)
18:00-19:00	5	5	10	(-10)
19:00-20:00				
20:00-21:00				
21:00-22:00				
22:00-23:00				
23:00-24:00				

Comments

No OGV's or PSV's visited the site during this survey.

No public transport users visited the site during this survey.

The difference between total people arrivals and departures can be explained by the fact that this site is 24-hour in nature.

Site reference: CH-03-A-05

Survey date: 14/10/08

Day of week: Tuesday

Multi-Modal survey site

Vehicles surveyed: Taxis

Time	Arr 1	Dep 1	Totals	Accumulation
00:00-01:00				
01:00-02:00				
02:00-03:00				
03:00-04:00				
04:00-05:00				
05:00-06:00				
06:00-07:00				
07:00-08:00	0	0	0	(0)
08:00-09:00	0	0	0	(0)
09:00-10:00	0	0	0	(0)
10:00-11:00	0	0	0	(0)
11:00-12:00	0	0	0	(0)
12:00-13:00	0	0	0	(0)
13:00-14:00	0	0	0	(0)
14:00-15:00	0	0	0	(0)
15:00-16:00	0	0	0	(0)
16:00-17:00	0	0	0	(0)
17:00-18:00	1	1	2	(0)
18:00-19:00	0	0	0	(0)
19:00-20:00				
20:00-21:00				
21:00-22:00				
22:00-23:00				
23:00-24:00				

Site reference: CH-03-A-05

Survey date: 14/10/08

Day of week: Tuesday

Multi-Modal survey site

Vehicles surveyed: Cycles

Time	Arr 3	Dep 3	Totals	Accumulation
00:00-01:00				
01:00-02:00				
02:00-03:00				
03:00-04:00				
04:00-05:00				
05:00-06:00				
06:00-07:00				
07:00-08:00	0	1	1	(-1)
08:00-09:00	0	1	1	(-2)
09:00-10:00	0	0	0	(-2)
10:00-11:00	1	1	2	(-2)
11:00-12:00	0	0	0	(-2)
12:00-13:00	0	0	0	(-2)
13:00-14:00	1	0	1	(-1)
14:00-15:00	0	0	0	(-1)
15:00-16:00	0	0	0	(-1)
16:00-17:00	0	0	0	(-1)
17:00-18:00	1	0	1	(0)
18:00-19:00	0	0	0	(0)
19:00-20:00				
20:00-21:00				
21:00-22:00				
22:00-23:00				
23:00-24:00				

Site reference: CH-03-A-05

Survey date: 14/10/08

Day of week: Tuesday

Multi-Modal survey site

People Surveyed: Pedestrians

Time	Arr 3	Dep 10	Totals	Accumulation
00:00-01:00				
01:00-02:00				
02:00-03:00				
03:00-04:00				
04:00-05:00				
05:00-06:00				
06:00-07:00				
07:00-08:00	0	0	0	(0)
08:00-09:00	0	4	4	(-4)
09:00-10:00	0	1	1	(-5)
10:00-11:00	0	0	0	(-5)
11:00-12:00	1	3	4	(-7)
12:00-13:00	0	0	0	(-7)
13:00-14:00	0	0	0	(-7)
14:00-15:00	0	1	1	(-8)
15:00-16:00	1	0	1	(-7)
16:00-17:00	0	1	1	(-8)
17:00-18:00	1	0	1	(-7)
18:00-19:00	0	0	0	(-7)
19:00-20:00				
20:00-21:00				
21:00-22:00				
22:00-23:00				
23:00-24:00				

Site reference: CH-03-A-05

Survey date: 14/10/08

Day of week: Tuesday

Multi-Modal survey site

People Surveyed: Total people

Time	Arr 62	Dep 85	Totals	Accumulation
00:00-01:00				
01:00-02:00				
02:00-03:00				
03:00-04:00				
04:00-05:00				
05:00-06:00				
06:00-07:00				
07:00-08:00	1	7	8	(-6)
08:00-09:00	4	25	29	(-27)
09:00-10:00	3	6	9	(-30)
10:00-11:00	2	5	7	(-33)
11:00-12:00	3	9	12	(-39)
12:00-13:00	4	3	7	(-38)
13:00-14:00	5	3	8	(-36)
14:00-15:00	6	2	8	(-32)
15:00-16:00	11	8	19	(-29)
16:00-17:00	10	6	16	(-25)
17:00-18:00	8	6	14	(-23)
18:00-19:00	5	5	10	(-23)
19:00-20:00				
20:00-21:00				
21:00-22:00				
22:00-23:00				
23:00-24:00				

TRIP RATE CALCULATION SELECTION PARAMETERS:

Land Use : 03 - RESIDENTIAL
Category : A - HOUSES PRIVATELY OWNED
VEHICLES

Selected regions and areas:

08 NORTH WEST
CH CHESHIRE 1 days

This section displays the number of survey days per TRICS® sub-region in the selected set

Filtering Stage 2 selection:

This data displays the chosen trip rate parameter and its selected range. Only sites that fall within the parameter range are included in the trip rate calculation.

Parameter: Number of dwellings
Actual Range: 17 to 17 (units:)
Range Selected by User: 5 to 100 (units:)

Public Transport Provision:

Selection by: Include all surveys

Date Range: 01/01/06 to 23/01/14

This data displays the range of survey dates selected. Only surveys that were conducted within this date range are included in the trip rate calculation.

Selected survey days:

Tuesday 1 days

This data displays the number of selected surveys by day of the week.

Selected survey types:

Manual count 1 days
Directional ATC Count 0 days

This data displays the number of manual classified surveys and the number of unclassified ATC surveys, the total adding up to the overall number of surveys in the selected set. Manual surveys are undertaken using staff, whilst ATC surveys are undertaken using machines.

Selected Locations:

Edge of Town 1

This data displays the number of surveys per main location category within the selected set. The main location categories consist of Free Standing, Edge of Town, Suburban Area, Neighbourhood Centre, Edge of Town Centre, Town Centre and Not Known.

Selected Location Sub Categories:

Residential Zone 1

This data displays the number of surveys per location sub-category within the selected set. The location sub-categories consist of Commercial Zone, Industrial Zone, Development Zone, Residential Zone, Retail Zone, Built-Up Zone, Village, Out of Town, High Street and No Sub Category.

Filtering Stage 3 selection:

Use Class:

C3 1 days

This data displays the number of surveys per Use Class classification within the selected set. The Use Classes Order 2005 has been used for this purpose, which can be found within the Library module of TRICS®.

Filtering Stage 3 selection (Cont.):

Population within 1 mile:

5,001 to 10,000

1 days

This data displays the number of selected surveys within stated 1-mile radii of population.

Population within 5 miles:

100,001 to 125,000

1 days

This data displays the number of selected surveys within stated 5-mile radii of population.

Car ownership within 5 miles:

1.1 to 1.5

1 days

This data displays the number of selected surveys within stated ranges of average cars owned per residential dwelling, within a radius of 5-miles of selected survey sites.

Travel Plan:

No

1 days

This data displays the number of surveys within the selected set that were undertaken at sites with Travel Plans in place, and the number of surveys that were undertaken at sites without Travel Plans.

LIST OF SITES relevant to selection parameters

1 CH-03-A-05 DETACHED CHESHIRE
 SYDNEY ROAD
 SYDNEY
 CREWE
 Edge of Town
 Residential Zone
 Total Number of dwellings: 17
 Survey date: TUESDAY 14/10/08 Survey Type: MANUAL

This section provides a list of all survey sites and days in the selected set. For each individual survey site, it displays a unique site reference code and site address, the selected trip rate calculation parameter and its value, the day of the week and date of each survey, and whether the survey was a manual classified count or an ATC count.

MANUALLY DESELECTED SITES

Site Ref	Reason for Deselection
CB-03-A-03	not comparable to site
CB-03-A-04	not comparable to site
DS-03-A-01	not comparable to site
EA-03-A-01	not comparable to site
ES-03-A-02	not comparable to site
GM-03-A-10	not comparable to site
HI-03-A-11	not comparable to site
HI-03-A-13	not comparable to site
NY-03-A-05	not comparable to site
NY-03-A-07	not comparable to site
NY-03-A-10	not comparable to site
NY-03-A-11	not comparable to site
SC-03-A-04	not comparable to site
SH-03-A-03	not comparable to site
SH-03-A-05	not comparable to site
WK-03-A-02	not comparable to site
WL-03-A-01	not comparable to site
WM-03-A-03	not comparable to site
WO-03-A-02	not comparable to site

TRIP RATE for Land Use 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED
VEHICLES

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	1	17	0.059	1	17	0.294	1	17	0.353
08:00 - 09:00	1	17	0.235	1	17	0.588	1	17	0.823
09:00 - 10:00	1	17	0.176	1	17	0.294	1	17	0.470
10:00 - 11:00	1	17	0.059	1	17	0.235	1	17	0.294
11:00 - 12:00	1	17	0.059	1	17	0.235	1	17	0.294
12:00 - 13:00	1	17	0.176	1	17	0.118	1	17	0.294
13:00 - 14:00	1	17	0.235	1	17	0.176	1	17	0.411
14:00 - 15:00	1	17	0.294	1	17	0.059	1	17	0.353
15:00 - 16:00	1	17	0.353	1	17	0.412	1	17	0.765
16:00 - 17:00	1	17	0.412	1	17	0.176	1	17	0.588
17:00 - 18:00	1	17	0.353	1	17	0.412	1	17	0.765
18:00 - 19:00	1	17	0.294	1	17	0.294	1	17	0.588
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:		2.705			3.293			5.998	

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

Parameter summary

Trip rate parameter range selected: 17 - 17 (units:)
 Survey date range: 01/01/06 - 23/01/14
 Number of weekdays (Monday-Friday): 1
 Number of Saturdays: 0
 Number of Sundays: 0
 Surveys manually removed from selection: 19

This section displays a quick summary of some of the data filtering selections made by the TRICS® user. The trip rate calculation parameter range of all selected surveys is displayed first, followed by the range of minimum and maximum survey dates selected by the user. Then, the total number of selected weekdays and weekend days in the selected set of surveys are shown. Finally, the number of survey days that have been manually removed from the selected set outside of the standard filtering procedure are displayed.

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

Appendix E- Landscape and Visual Impact Assessment for Land at Coton House Estate by Pegasus Design.

LAND AT COTON HOUSE ESTATE, CHURCHOVER, RUGBY

LANDSCAPE AND VISUAL IMPACT ASSESSMENT

ON BEHALF OF CALA HOMES (MIDLANDS) LTD.

Pegasus Group

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PLANNING | **DESIGN** | **ENVIRONMENT** | **ECONOMICS**

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1. INTRODUCTION

Terms of reference

- 1.1. Pegasus Design, part of the Pegasus Group, has been instructed by Cala Homes, to undertake a landscape and visual impact assessment (LVIA) in relation to the proposed development of land at Coton House Estate, Churchover, Rugby (referred to as 'the site').
- 1.2. This LVIA will consider existing landscape and visual receptors in the study area, these include:
- Landscape character, including physical landscape resources; and
 - Views and visual amenity experienced by residents, recreational users (including visitors and tourists) and road users.
- 1.3. Principles and good practice for undertaking landscape and visual impact assessment are set out in the Landscape Institute (LI) and the Institute of Environmental Management (IEMA) Guidelines for Landscape and Visual Impact Assessment, Third Edition (2013)¹. The detailed methodology used is included at **Appendix A**.

Site overview

- 1.4. The site comprises 10.5 hectares (ha) of land to the north of Rugby, and south-east of Churchover, Warwickshire (refer to **Figure 1, Site Location and Planning Designations**). The site comprises the northern and western extents of the wider Coton House Estate and is formed by two parcels of land, separated by an access driveway that leads to the core of the estate which has been subject to recent redevelopment by Cala Homes. The Estate itself comprises an area of parkland associated with the grade II* listed Coton House, mature woodland belts defining the northern and western extents of the estate, and an area of new residential development built on land previously occupied by a Royal Mail training centre.
- 1.5. In the wider context, to the south is the corridor of the M6 motorway and junction 1, beyond which are several areas of industrial development at the edge of the settlement of Rugby, including 'Central Park' and 'Rugby Gateway', which both comprise large scale industrial buildings that feature in many views across the landscape in this location.

¹ Landscape Institute and Institute of Environmental Management and Assessment, Guidelines for Landscape and Visual Impact Assessment 3rd Edition (April, 2013)

Currently, there is a live planning application for a new Motorway Service Area (MSA) on land at the north-western quadrant of junction 1.

- 1.6. To the north, east and west the agricultural context is characterised by a generally medium scale field pattern, the majority of which is in arable production. The small settlement of Churchover is located to the north-west. There are a limited number of public rights of way (PROW) in the local area, although there is one public footpath that passes through the southern parcel of the site and connects with the northern edge of Rugby.
- 1.7. The scheme (referred to as 'the proposed development') will include 100 (market and affordable) residential units and associated infrastructure, including public open space. The application is for full planning permission.
- 1.8. Additional information and a more detailed description of the physical components, landscape character and visual amenity of the site and study area are set out in later sections of this LVIA.

2. ASSESSMENT METHODOLOGY

2.1. The approach and methodology used for this LVIA has been developed using best practice guidance, as set out in the following documents:

- Landscape Institute and Institute of Environmental Management and Assessment (2013) Guidelines for Landscape and Visual Impact Assessment, 3rd Edition;
- Natural England (2014) An Approach to Landscape Character Assessment; and
- Landscape Institute Advice Note 1/11 Photography and Photomontages Guidance.

2.2. Reference has also been made to several additional sources of data and information; these are referred to in the relevant sections of the baseline information. A number of drawings have also been produced as part of this LVIA and are included as **Figures 1 to 7**.

Level of assessment

2.3. The third edition of the Guidelines for Visual Impact Assessment (GLVIA3) was published in April 2013. This guidance acknowledges that LVIA can be carried out either as a standalone assessment or as part of a broader EIA. The GLVIA3 note that the overall principles and core steps in the process are the same but that there are specific procedures in EIA with which an LVIA must comply.

2.4. This report has been prepared as a detailed LVIA and addresses matters of individual resources, character areas and representative viewpoints. The LVIA includes analysis of sensitivity of receptors (both landscape and visual) and magnitude of impact and also professional judgements on the consequential likely effects.

2.5. The proposed development incorporates a landscape mitigation strategy which will avoid, reduce or remedy adverse impacts.

Approach

2.6. The overall approach to the identification, evaluation and assessment of landscape and visual effects is summarised as follows:

- Determine the scope of the assessment;
- Collate baseline information for landscape and visual receptors, including completing desk study research and undertaking field based survey work;

- Review the type of development proposed and identify and describe the likely impacts (enabling specific judgments to be made on sensitivity of landscape and visual receptors);
- Establish the sensitivity of landscape and visual receptors (balancing judgments on value and susceptibility);
- Determine the magnitude of impacts (balancing judgments on size / scale, duration and reversibility);
- The assessment of the significance of likely landscape and visual effects through a balanced approach and clear description of professional judgments on sensitivity and magnitude; and
- The identification of measures to avoid or remedy impacts and the subsequent re-assessment of likely effects.

Scope of assessment

- 2.7. The spatial scope for the LVIA is initially determined by reference to the area of landscape that may be affected and from which the proposed development may be visible².
- 2.8. The preliminary study area for the LVIA has been set at an approximate radius of 2km from the site. This is considered sufficient to account for the likely impacts that will be generated by the proposed development. In some specific instances it has been necessary to vary this distance in order to consider the potential for impacts on specific landscape resources or from specific long distance viewpoints.
- 2.9. The professional judgements in this LVIA consider landscape and visual effects in the short term, at completion, but also in the longer term after fifteen years when mitigation measures (such as planting) will have matured and the mitigation measures are likely to perform the intended function (for example, screening or enhancement of landscape infrastructure).
- 2.10. Supporting photographs used in this LVIA have been taken during winter. In winter, the density of vegetation is less and therefore represents the worst-case scenario. This is considered in the balance of impacts through professional judgement.
- 2.11. Landscape features and elements provide the physical environment for flora and fauna and the associated importance of biodiversity assets. This LVIA does not consider the

² Para 3.15, Landscape Institute and Institute of Environmental Management and Assessment, Guidelines for Landscape and Visual Impact Assessment 3rd Edition (April, 2013)

value, susceptibility or importance on ecology and biodiversity, nor does it consider impacts from an ecological stance.

- 2.12. Heritage assets such as Scheduled Monuments, Listed Buildings and Conservation Areas all contribute to the overall present day landscape character, context and setting of an area. These aspects have been given consideration in the LVIA in terms of physical landscape resources (for example trees and hedgerows) and also landscape character. However, this LVIA does not address the historic significance, importance or potential impacts on heritage assets and designations; these assets are assessed in the context of landscape and visual matters only.

Collating baseline information

- 2.13. Information has been collated using a process of desk study and field survey in order to capture a comprehensive description of the baseline position for landscape and visual receptors. The desk study includes reference to published landscape character studies.
- 2.14. Field survey work was completed during February 2017. A series of illustrative and representative photographs were taken during the field work. These photographs were taken with a digital camera with a 50mm lens (equivalent focal length) at approximately 1.6 metres in height. These are presented as a series of viewpoints and have been used to inform both the landscape and, separately, visual assessment (included as **Figure 5, Viewpoint Photographs**).

Assessment of effects

- 2.15. Having established the relevant baseline position, the assessment process then completes the following specific stages:
- Evaluate the sensitivity of the landscape receptors and visual receptors, specifically in response to the nature of the proposed development (sensitivity is not standard and depends on the nature and type of development proposed and the value and susceptibility of the receptor to that type of development);
 - Identify the potential magnitude of impact on the physical landscape, on landscape character and on visual receptors; and
 - Combine judgments on the nature of the receptor (sensitivity) and the nature of the impact (magnitude) to arrive at clear, professional judgments of significance.

- 2.16. For both landscape effects and visual effects, the final conclusions on significance are based on professional judgements combining the specific analysis of the sensitivity of receptors and detailed predictions on the magnitude of change (or impact). GLVIA3 advocates a balanced justification of these issues using professional judgement rather than formulaic matrices. The rationale for the overall judgement on significance is based on the application of professional analysis and judgement and the subsequent combination of each of the criteria in order to reach a conclusion.
- 2.17. The detailed thresholds and criteria for each of the stages of analysis and assessment of landscape and visual impacts are included in the detailed methodology (**Appendix A**).

3. LANDSCAPE POLICY BACKGROUND

- 3.1. This section sets out a review of national and local policy relevant to landscape and visual matters.
- 3.2. In the context of the relevant planning framework, this section also sets out a summary of those policies specific to the landscape and visual issues pertaining to the proposed development and which will have implications for the landscape strategy presented as part of this LVIA (refer to **Figure 1, Site Location and Planning Designations**).

European Landscape Convention

- 3.3. The European Landscape Convention (ELC) promotes the protection, management and planning of European landscapes. The convention was adopted on 20 October 2000 and came into force on 1 March 2004. The ELC is designed to achieve improved approaches to the planning, management and protection of landscapes and organises cooperation on landscape issues. The convention defines landscape as:
- 3.4. *"...an area, as perceived by people, whose character is the result of the action and interaction of natural and/or human factors)"*
- 3.5. The importance of this definition is that it focuses on landscape as a resource in its own right and moves beyond the idea that landscapes are only a matter of aesthetics and visual amenity.

National Planning Policy Framework

- 3.6. The National Planning Policy Framework (NPPF) sets out the Government's planning policies for England and provides a framework within which the appropriate local council can produce local and neighbourhood plans; the NPPF is a material consideration in planning decisions.
- 3.7. The NPPF sets out three dimensions to achieving sustainable development that include economic, social and environmental considerations. It places an onus on the planning system to perform a role in relation to the environment that 'contributes to the protection and enhancement of our natural, built and historic environment...' and this underpins the strategic guidance set out in the NPPF in relation to landscape and visual matters.

- 3.8. High quality design and local character are repeating themes through the core planning principles and more specific guidance on delivering sustainable development. Specific issues addressed by the core principles of the NPPF include:
- that decision making should secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings;
 - that decision making should take account of the different roles and character of different areas and recognise the intrinsic character and beauty of the countryside and supporting thriving rural communities within it (this also reflects the aims of the ELC); and
 - that development should contribute to conserving and enhancing the natural environment.
- 3.9. Under delivering sustainable development the guidance sets out a series of considerations relating to 'good design'³. This includes reference to sense of place, responding to local character and materials and aiming for visually attractive solutions as a result of appropriate landscaping. It also notes issues relating to local distinctiveness and taking opportunities to enhance the character and quality of an area.
- 3.10. The NPPF notes the importance that designs 'evolve' in response to local issues and to the views of local communities⁴ and also sets out principles in relation to conservation and enhancement of the natural environment⁵.

Planning Practice Guidance Documents

- 3.11. In March 2014 the Department for Communities and Local Government (DCLG) launched a web based resource of planning practice guidance documents (PPG); these effectively supersede a series of previous advice and guidance documents. The website notes that the PPG will be updated as required.
- 3.12. Matters pertaining to 'landscape' are covered under the guidance for the Natural Environment. Para 001 addresses how the character of landscapes can be assessed to inform plan-making and planning decisions. It states that:
- 3.13. *"One of the core principles in the National Planning Policy Framework is that planning should recognise the intrinsic character and beauty of the countryside. Local plans should*

³ Para 58-66, Department for Communities and Local Government, NPPF (March 2012)

⁴ Para 66, Department for Communities and Local Government, NPPF (March 2012)

⁵ Section 11, Department for Communities and Local Government, NPPF (March 2012)

include strategic policies for the conservation and enhancement of the natural environment, including landscape. This includes designated landscapes but also the wider countryside.

- 3.14. *Where appropriate, landscape character assessments should be prepared to complement Natural England's National Character Area profiles. Landscape Character Assessment is a tool to help understand the character and local distinctiveness of the landscape and identify the features that give it a sense of place. It can help to inform, plan and manage change and may be undertaken at a scale appropriate to local and neighbourhood plan-making."*
- 3.15. This LVIA includes reference to landscape character assessment prepared at a national, regional, county and district level and also addresses local character by reference to the key characteristics of the site and its immediate context. Therefore, this LVIA responds fully to the requirement of the PPG.
- 3.16. The PPG also include guidance on light pollution. The guidance notes that artificial light has the potential to become what is termed 'light pollution' or 'obtrusive light' and not all modern lighting is suitable in all locations. It also notes that some proposals for new development, but not all, may have implications for light pollution and it considers issues such as changes to the baseline, suitability of location, protected sites and designated dark skies when determining whether light pollution might arise.

Local planning policy

- 3.17. The following section sets out the local planning policy background relevant to the site.

Adopted Policy

- 3.18. The site is located within the administrative area of Rugby Borough Council. The current adopted planning policy comprises the Core Strategy⁶ and the saved policies of the Rugby Borough Local Plan⁷. Those policies relevant to a study of landscape and visual issues are set out in the table as follows.

⁶ Core Strategy (June 2011), Rugby Borough Council.

⁷ Rugby Borough Local Plan (2006), Rugby Borough Council.

Table 1: Adopted policies relevant to landscape and visual matters

Policy reference	Summary
Core Strategy	
Policy CS14: Enhancing the Strategic Green Infrastructure Network	This policy states that the Council will work with partners towards the creation of a comprehensive Borough wide strategic GI Network which will be achieved through the protection, restoration and enhancement of existing GI assets within the network as shown on the Proposals Map; and the introduction of appropriate multi-functional linkages between existing GI assets. Where appropriate new developments must provide suitable GI linkages throughout the development and link into adjacent strategic and local GI networks or assets, where present.
Rugby Borough Local Plan – Saved Policies	
Policy GP2: Landscaping	<p>This policy states that the landscape aspects of a development proposal will be required to form an integral part of the overall design. A high standard of appropriate hard and soft landscaping will be required. All proposals should ensure that:</p> <ol style="list-style-type: none"> 1. Important site features have been identified for retention through a detailed site survey; 2. The landscape character of the area is retained and, where possible, enhanced; 3. Features of ecological, geological and archaeological significance are retained and protected and opportunities for enhancing these features are utilised; 4. Opportunities for utilising sustainable drainage methods are incorporated; 5. New planting comprises native species which are of ecological value appropriate to the area; 6. In appropriate cases; there is sufficient provision for planting within and around the perimeter of the site to minimise visual intrusion on neighbouring uses or the countryside; and 7. Detailed arrangements are incorporated for the long-term management and maintenance of landscape features.

Emerging Policy

- 3.19. The emerging Rugby Borough Local Plan publication draft also contains policies relevant to a study of landscape and visual issues which are summarised in the following table.

Table 2: Emerging policies relevant to landscape and visual matters

Policy reference	Summary
Policy NE3: Blue and Green Infrastructure Policy	This policy states that the Council will work with partners towards the creation of a comprehensive Borough wide Strategic Green Infrastructure Network as shown indicatively on the Green Infrastructure Proposal Map. This will be achieved through the following:

	<ul style="list-style-type: none"> - the protection, restoration and enhancement of existing Green Infrastructure assets within the network as shown on the proposals map; and - the introduction of appropriate multi-functional linkages between existing Green Infrastructure assets <p>Where appropriate new developments must provide suitable Green Infrastructure linkages throughout the development and link into adjacent strategic and local GI networks or assets where present.</p> <p>Where such provision is made a management plan should be produced as part of the planning application demonstrating the contribution to the overall achievements of the multi-functional strategic Green Infrastructure network.</p>
Policy NE4: Landscape Protection and Enhancement	<p>This policy states that new development which positively contributes to landscape character will be permitted and that development proposals will be required to demonstrate that they:</p> <ul style="list-style-type: none"> - integrate landscape planning into the design of development at an early stage; - consider its landscape context, including the local distinctiveness of the different natural and historic landscapes and character, including tranquillity; - relate well to local topography and built form and enhance key landscape features, ensuring their long term management and maintenance; - identify likely visual impacts on the local landscape and townscape and its immediate setting and undertakes appropriate landscaping to reduce these impacts; - aim to either conserve, enhance or restore important landscape features in accordance with the latest local and national guidance; - avoid detrimental effects on features which make a significant contribution to the character, history and setting of an asset, settlement or area; - address the importance of habitat biodiversity features, including aged and veteran trees, woodland and hedges and their contribution to landscape character, where possible enhancing and expanding these features through means such as buffering and reconnecting fragmented areas; and - are sensitive to an area's capacity to change, acknowledge cumulative effects and guard against the potential for coalescence between existing settlements.
Policy SDC2: Landscaping	<p>This policy states that the landscape aspects of a development proposal will be required to form an integral part of the overall design. A high standard of appropriate hard and soft landscaping will be required. All proposals should ensure that:</p> <ul style="list-style-type: none"> - Important site features have been identified for retention through a detailed site survey; - The landscape character of the area is retained and, where possible, enhanced;

	<ul style="list-style-type: none"> - Features of ecological, geological and archaeological significance are retained and protected and opportunities for enhancing these features are utilised; - Opportunities for utilising sustainable drainage methods are incorporated; - New planting comprises native species which are of ecological value appropriate to the area; - In appropriate cases; there is sufficient provision for planting within and around the perimeter of the site to minimise visual intrusion on neighbouring uses or the countryside; and - Detailed arrangements are incorporated for the long-term management and maintenance of landscape features.
--	--

3.20. These policy considerations have been taken forward to inform the design of the proposed development.

Designations

3.21. The site is not subject to any specific landscape related designations. Coton House grade II* and grade II listed stable block are located to the east. Any potential impacts on these buildings are considered in the Heritage Assessment prepared by Heritage Collective. Reference is made in Section 5 of this report (Development Proposals and Landscape Strategy) in terms of the influence of heritage matters on the scale and extent of the proposed development.

3.22. In the wider study area, Churchover Conservation Area is located ca. 1km to the north-west. A scheduled monument (Bowl barrow 470m south west of Coton House) is located ca. 350m to the south-west of the site. Tripontium Roman station scheduled monument is located ca. 1.67 km to the east. Given the distance of the site from these designations, the small-scale nature of the site itself in relation to the wider landscape, and the enclosure provided by existing mature vegetation, any potential impacts and consequent effects are not likely to be significant in landscape and visual terms.

4. LANDSCAPE AND VISUAL BASELINE

- 4.1. The following section describes the individual components of the physical landscape that are present in the study area. These have been described to establish an understanding of the specific landscape baseline, including individual elements and more distinctive features which together contribute to landscape character. This is preceded by a more detailed site description, with further reference to the Arboricultural Assessment prepared by Ruskin's. is provided.

Site Description

- 4.2. The site itself extends to 10.5 hectares and forms the northern and western flanks of the Coton House Estate. It is sub-divided into two overall parcels: a northern one and a southern one.

Northern Parcel

- 4.3. The northern parcel extends from the mature Lime Avenue that encloses a site access point from the A426, northwards and then eastwards to the driveway to Coton House itself, that also extends from the A426. The northern boundary is defined by a mature woodland belt; the southern boundary is defined by the mature Lime avenue; the eastern boundary is defined by a mixed mature landscape infrastructure surrounding a moat (see Heritage Assessment, and thereafter extends up to the Coton House Driveway; and the western boundary is also defined by a mature woodland belt.
- 4.4. Within this mature planting infrastructure, there is rough grassland together with a number of large parkland trees. Species are typically Lime, Poplar and Oak and these are generally located towards the eastern end of this parcel of the site adjacent to the Coton House Driveway, and thereafter continue more plentifully east of the driveway. There are two large Poplars (BS Category B2) in the centre of the parcel, but between them and the Lime avenue (A1), the land is without tree cover.
- 4.5. The woodland belt to the west between the site and the A426 frontage includes Sycamore, Ash, Holly, Beech, Scots Pine, Oak and Norway Maple, and their BS Category generally ranges from C1-B2. The mixed vegetation on the eastern site boundary, adjacent to the moat, includes Oak, Goat Willow, Hawthorn and Beech, and their BS Category generally ranges from C1-B2. Further detailed information can be found in the Arboricultural Report prepared by Ruskin's Tree Consultancy.

- 4.6. Topography on the northern parcel is generally level at c. +131 metres AOD.

Southern Parcel

- 4.7. The southern parcel extends from the mature Lime Avenue that encloses the site access point from the A426, and extends southwards to a field boundary that demarcates the southern edge of the Estate, and only a single field enclosure away from the M6 motorway corridor. The northern boundary is common with the southern boundary of the northern parcel i.e. the Lime avenue; the southern boundary is defined by a post and rail fence and 5 no. trees; the eastern boundary is defined by the 'domestic' thresholds of existing and emerging residential development; and the western boundary is defined by a mature woodland belt.
- 4.8. Within this mature planting infrastructure, there is rough grassland but no trees. The woodland belt to the west between the site and the A426 frontage includes sycamore, Ash, Oak, Scots Pine Beech and Holly, and the BS Category of many of the larger trees is B2. The 5 no. trees on the southern boundary comprise an Oak; a Lime; and three Hawthorns, all of which are Category B2. Again, further detailed information can be found in the arboricultural report prepared by Ruskin's Tree Consultancy.
- 4.9. Topography on the southern parcel is also generally level at c. +131 metres AOD, and there is a public footpath linking the A426 Lutterworth Road at the site entrance beneath the Lime avenue, directly across the site to its southern tip before thereafter heading towards the motorway corridor and following an underpass to get to the other side.

Context: Physical landscape resources

Topography and landform

- 4.10. As described, the landform of the site is relatively flat, lying at ca. +131m AOD (Above Ordnance datum) and sits on a topographical plateau. In the wider landscape the topography is generally undulating and is characterised by a number of steep escarpments, for example the A426 to the south sits on a localised ridgeline, which slopes eastwards towards the valley of the River Swift. A disused canal also occupies the lower ground in this location. To the south-east beyond the village of Newton the landform also forms a localised ridgeline reaching ca. +115m. Here it falls steeply to Clifton Lakes, within the flood basin of the River Avon. Other local high points include that east of Newton where the topography rises to ca. +130m AOD again. This is illustrated on

Figure 2: Topography.

Hydrology and water features

- 4.11. There are no water bodies on the site itself, although a historic moat is located south-east of the southern parcel (see also Heritage Assessment). In the wider landscape there is an extensive drainage network, generally associated with the corridor of the River Avon which passes from the east, into the centre of Rugby and out to the west. The River Swift passes from beneath the M6 motorway corridor to the north and also passes through the centre of Rugby, connecting with the Oxford canal. There are a number of larger water bodies in the area, including Clifton Lakes to the south-east and those associated with sand and gravel workings to the north-east.

Land use

- 4.12. As described, land use on site comprises two separate parcels of rough pasture, associated with the wider Coton House Estate. Recent residential development and the ongoing construction of several new properties is taking place directly to the east of the site. Beyond the site itself, land uses include large scale industrial and distribution units, and motorway infrastructure to the south; and land fill and sand and gravel workings to the north-east. Otherwise land use generally comprises agriculture. A number of lines of electricity pylons also cross the landscape. There are several individual farmsteads and a small settlement at Churchover; the main residential edge of Rugby is located slightly further south.

Vegetation patterns

- 4.13. As described, vegetation on site comprises an avenue of mature Lime trees that line the access road into the new Cala Homes development; individual 'parkland' trees (towards the east) and the woodland belts along the A426 frontage. The wider Coton Park Estate also comprises woodland belts that define the northern extent of the estate, further mature parkland trees and a tree belt to the south.
- 4.14. In the wider landscape hedgerows with hedgerow trees generally line roadsides and field boundaries and are characteristic as such. There are also a number of individual planting belts and blocks, typically in regular geometric patterns. In contrast the River Swift corridor supports riparian vegetation along its banks.

Public access

- 4.15. As described, there is a PROW (public footpath) that crosses the southern parcel of the site and connects with Newton to the south (via an underpass below the M6 motorway). There is a relatively limited network of PROW to the north of the site, but a number of public footpaths and bridleways connect the western edge of Churchover with the wider landscape and there is also a greater concentration of PROW around Cosford.

Development and transport patterns

- 4.16. The M6 motorway forms a major transport route which passes through the landscape in this location. The motorway junction (no. 1) is located to the south-west of the site and a number of industrial land uses are associated with the junction, these include the Rugby Gateway development. There is currently a live planning application for an MSA (Motorway Service Area) at the north-western quadrant of junction 1.
- 4.17. The A426 connects the motorway with Rugby to the south, other key routes include the A5 which passes through the landscape to the east. A number of other smaller roads and lanes connect smaller settlements such as Churchover and Newton with the urban area of Rugby and the wider landscape.

Landscape character

- 4.18. Reference has been made to published guidance on landscape character for the area. The site is located in the following landscape character types/areas (refer to **Figure 3, Landscape Character**):
- National level – NCA 94: Leicestershire Vales;
 - County Level – Warwickshire Landscape Guidelines; High Cross Plateau and Open Plateau; and
 - Local level – Landscape Assessment of the Borough of Rugby; High Cross Plateau Landscape Character Area (LCA) and Open Plateau Landscape Character Type (LCT).

See Plate 1 below.

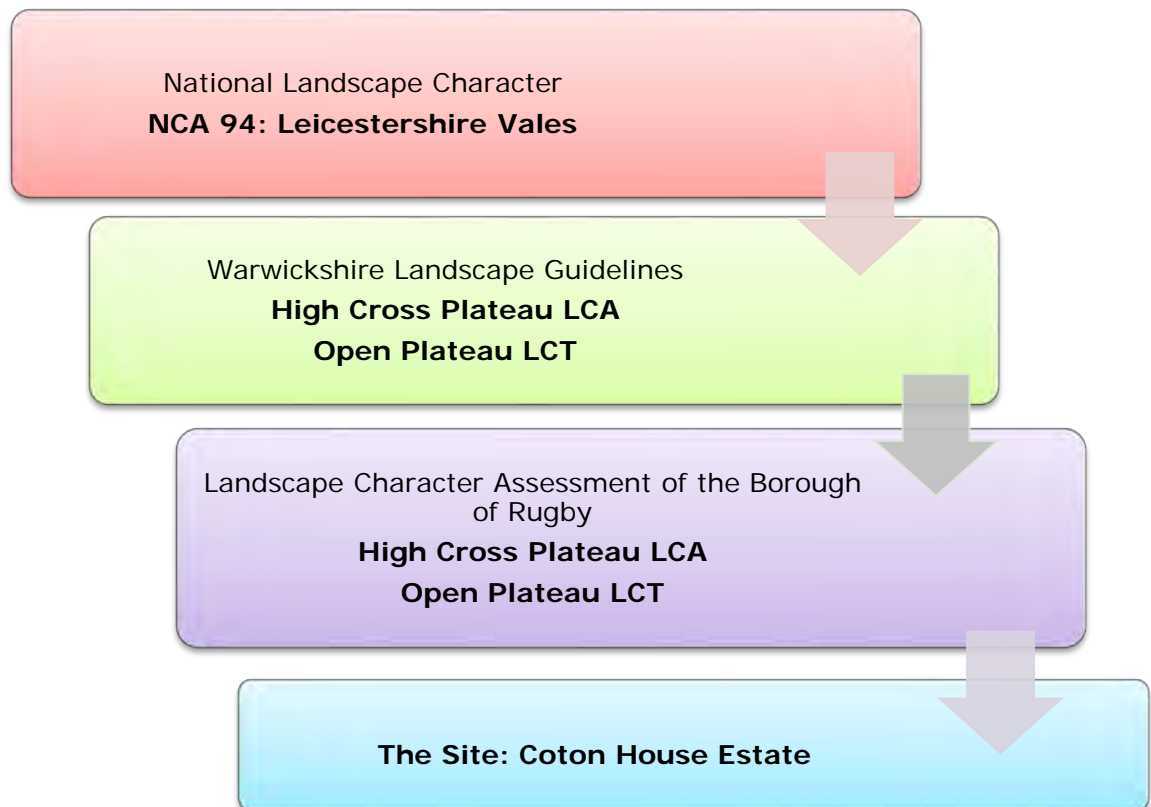


Plate 1: Summary of landscape character hierarchy

4.19. The following sections set out a summary of the characteristics contained in published guidance, relevant to the site and study area.

National landscape character

4.20. At a national level, the site is situated within the National Character Area (NCA) 94: Leicestershire Vales⁸. Where relevant to the site and its landscape context, the key characteristics of NCA 94 are summarised as follows:

- An overall visual uniformity to the landscape and settlement pattern;
- Land use characterised by a mixture of pasture and arable agriculture that has developed on the neutral clay soils;
- The NCA's woodland character is derived largely from spinneys and copses on the ridges and more undulating land and hedgerow trees and hedgerows;

⁸ NE532: NCA Profile: 94 Leicestershire Vales, Natural England (February 2014).

Riverside meadows and waterside trees and shrubs are common, along with waterbodies resulting from gravel extraction;

- Diverse levels of tranquility associated with contrasts between busy urban areas and some deeply rural parts. Large settlements dominate the open character of the landscape;
- Frequent small towns and large villages often characterised by red brick buildings and attractive stone buildings in older village centres and eastern towns and villages. Frequent, imposing spired churches are also characteristic, together with fine examples of individual historic buildings; and
- Rich and varied historic landscape, prominent historic parklands and country houses.

4.21. Given the scale of the NCA and the diversity of the key characteristics, it is evident that the landscape components which define the character at this regional level are represented across the wider NCA. This includes the reference to historic parkland.

4.22. In order to complete a more detailed appraisal of potential landscape and visual issues, reference has been made to the published landscape character assessment prepared at a finer grain and more local scale.

County Level landscape character

Warwickshire Landscape Guidelines

4.23. At the County level, the site and its context are considered within the Warwickshire Landscape Guidelines, and fall within the “High Cross Plateau”. This is sub-divided into two: “Open Plateau”; and the “Village Farmlands”. The site and its context fall within the Open Farmlands.

4.24. On page 18 of the Guidelines it describes the Open Plateau as a remote rural landscape lying along the north-eastern boundary of the county between Rugby and Hinckley, and for the most part is a large scale rolling landscape characterised by wide views and a strong impression of ‘emptiness’ and space. It goes on to note that perhaps the most notable feature is the *“remote rural character of the landscape.....with few roads or settlements, and in places there are extensive areas of largely inaccessible ‘empty’ countryside”*.

4.25. Key characteristics include:

- A rolling plateau dissected by broad valleys;

- A medium to large scale, often poorly defined field pattern;
- A sparsely populated landscape of hamlets and isolated manor farmsteads;
- Deserted medieval village sites surrounded by extensive areas of 'empty' countryside;
- Pockets of permanent pasture often with ridge and furrow; and
- Prominent belts of woodland.

Borough level landscape character

Landscape Assessment of the Borough of Rugby

- 4.26. The Landscape Assessment of the Borough of Rugby⁹ sets out a description of the different landscape character areas (LCA) which are present across the borough, and within these, on a finer scale, the local landscape is further divided into landscape character types (LCT). The LCA and LCT relevant to the site are discussed in the sections below. This study is co-ordinated with the 1993 Warwickshire Landscape Guidelines, described above. At the introduction on page 1 it notes that the Study looks in more detail at the countryside around the urban fringe of Rugby itself. The aim was to examine the character of the landscape around the Town, its sensitivity to change; its condition; and how these factors can assist as a decision tool.
- 4.27. The site is located within the High Cross Plateau Landscape Character Area (refer to **Figure 3, Landscape Character**). The High Cross Plateau is a sparsely populated area characterised by wide rolling ridges and valleys. The published assessment states that:
- 4.28. *"It actually belongs to the south western section of the Leicestershire Wolds with only part of the area extending into Warwickshire - here it forms the region of high ground between Rugby and Hinckley in the north-eastern corner of the county. This rolling plateau is dissected by a series of streams, forming deep but poorly defined valleys separated by broad, round ridges."*
- 4.29. The site and its context is located within the Open Plateau Landscape Character Type (LCT). The key characteristics are summarised by the published character assessment as follows:

⁹ The Landscape Assessment of the Borough of Rugby (Sensitivity and Condition Study), April 2006.

- 4.30. *"This is a remote, large-scale, open, rolling plateau dissected by broad valleys, characterised, for the most part, by wide views and a strong impression of "emptiness" and space. This is reinforced by an absence of roads and settlements, with sparsely populated hamlets and isolated manor farmsteads prevailing. In places there are extensive areas of largely inaccessible countryside, which relates closely to deserted medieval village sites. Field pattern is generally medium to large in scale but is often poorly defined and tends to be a relatively minor element in this landscape, as the eye is naturally drawn to distant skylines rather than to foreground views. In places, however, smaller fields may occur, often associated with pockets of permanent pasture, and ridge and furrow. Shelterbelts may also form prominent features in an otherwise open and featureless landscape."*
- 4.31. The published character assessment then describes the sensitivity and condition analysis of each LCT. For the 'Open Plateau' the published assessment states:
- 4.32. *"Sensitivity – Fragility: Whilst the Open Plateau is a planned landscape, the cultural sensitivity varies with the central plateau summit having a more consistent pattern than the rest. Ecological fragility is low across the whole LCT."*
- 4.33. *Sensitivity – Visibility: The rolling topography gives rise to a moderate sensitivity, but on the plateau summit the more gently rolling landform, together with the presence of very distinctive shelterbelts, results in reduced visibility. The area between Harborough Magna, and Cosford, north of Newbold on Avon, has a high visual sensitivity due to lack of tree cover."*
- 4.34. *Overall sensitivity: Due to a combination of cultural coherence and rolling topography, the **overall sensitivity is rated as moderate**. This becomes high to the north of Newbold on Avon where the landscape is rolling but unwooded."*
- 4.35. In terms of condition, the published assessment states:
- 4.36. *"Condition: There is an extensive area to the north of Rugby whose condition is considered to be weak. **To the north and west of Newbold on Avon, as too with land around Coton House, condition is strong**. Elsewhere it is in decline."*

Landscape character summary

- 4.37. From the site and contextual based evaluation undertaken for this LVIA, it is noted that the NCA study identifies the presence of "prominent historic parklands and country

houses", however this is not a characteristic that is identified in the Warwickshire Landscape Guidelines. The site is an example of a country house and associated parkland, but the parkland itself is not necessarily prominent, nor is it statutorily recognised.

- 4.38. Whilst the site context does retain and represent several the characteristics set out for the "Open Plateau" LCT, the overriding characteristics are ones that are heavily influenced by the urban fringe. These are generated by the presence of the M6 motorway infrastructure; commercial development in close proximity on the northern edge of Rugby; and the noise and other infrastructure elements associated with this. If the MSA proposal were to come forward at junction 1, then this will serve to reinforce these characteristics.
- 4.39. Regarding the site itself, it is more heavily enclosed by mature vegetation and is consequently more intimate in terms of character than the wider landscape that surrounds it. Whilst characteristics of parkland are still evident, in particular to the north and north-east of Coton House itself, the Estate has been subject to, albeit sensitive, modern residential development that has further influenced site specific character. As such it retains elements of the modern day as opposed to only those from past years.
- 4.40. Taking all the above into account, the characteristics of an 'empty countryside' that is sparsely populated are difficult to comprehend on what is an urban fringe site and context. In contrast, characteristics such as prominent belts of woodland do prevail, not least of all those surrounding the western and northern flanks of the site and Coton Park itself, and hence there is a degree of consistency and overlap between the published description of the landscape character of the Open Plateau LCT and the character of the local area around the site.
- 4.41. Further analysis of the local landscape character draws on this baseline information and is presented as part of the detailed landscape and visual analysis, set out in Section 5 of this LVIA, and with reference to **Figure 6: Landscape and Visual Analysis**.

Visual baseline

- 4.42. This section provides a summary description of the nature and extent of the existing views from, towards and between the site and the surrounding area. It also includes reference to specific locations that will potentially be subject to impacts as a result of the proposed development.

- 4.43. Establishing the specific nature of these views provides an understanding of the context and setting of representative viewpoints and also the nature of views in terms of distance, angle of view, and seasonal constraints associated with specific visual receptors. The identification of key sensitive receptors and links to the representative viewpoint are carried forward to the assessment process (refer to **Figure 5, Viewpoint Photographs 1 to 12**).

Overview

- 4.44. The visual envelope is the area of landscape from which a site or proposed development will potentially be visible. It accounts for general judgements on the theoretical visibility of a site or proposed development and sets a broad context for the study area within which to address landscape and visual impacts. The extent of a visual envelope will be influenced by the physical landscape components of an area, such as hedgerows, woodlands or buildings and can also be influenced by distance from a site. The visual envelope for the site is defined as follows

- From the north, views of the site are limited by existing mature vegetation that follows the line of a spring passing from Coton Road and through Coton Spinney to the north-east and the mature tree belts that characterise the northern extent of Coton House Estate (although some glimpsed views into the site are possible in winter from along the A426);
- From the east, views of the site are limited by the mature tree cover associated with the parkland to the east, which encloses the Coton House Estate and serve to generally screen views of the site - and from the south-east by a more recently planted woodland belt including numerous conifers;
- From the west by a combination of mature and dense woodland belt planting along the A426 and road infrastructure associated with junction 1 of the M6 motorway; and
- From the south by the M6 motorway corridor and large scale industrial development to the south.

- 4.45. Within this overall visual envelope there are variations in the degree of inter-visibility between areas and in the nature and extent of views. Overall these are tested further through the detailed field work and the subsequent assessment of visual impacts.

Representative viewpoints and visual receptors

- 4.46. The visual assessment references a series of viewpoints that are representative of visual receptors in the area. These illustrate views towards the site in the context of the

surrounding landscape and are used to inform judgements on impacts for specific receptors (refer to **Figure 4, Viewpoint Locations and PROW, Figure 5, Viewpoint Photographs 1 to 12**).

- 4.47. A detailed description for each of the locations identified as receptors for this LVIA, including judgements on overall sensitivity of visual receptors, is included in later sections of this report under the assessment of visual effects.

5. DEVELOPMENT PROPOSALS AND LANDSCAPE STRATEGY

- 5.1. This section considers the type of development proposed and the nature of the impacts that are likely to occur; thereafter it draws the landscape and visual baseline information together and summarises the key constraints and opportunities in the existing landscape (refer to **Figure 6: Landscape and Visual Analysis**; and **Figure 7: Illustrative Landscape masterplan**).

Landscape and Visual Analysis

- 5.2. Based on the description of the landscape and visual baseline, the landscape and visual analysis is illustrated on **Figure 6**. It can be summarised as follows as set out below, considering wider contextual matters and thereafter local and site specific matters.

Contextual Analysis

- 5.3. This can be summarised as follows:
- The wider site context is generally consistent with the characteristics identified in the relevant published landscape character studies, with areas of remoteness, inaccessibility, 'emptiness', a rolling plateau; prominent belts of woodland and a medium to large scale field pattern;
 - The field patterns and woodland belts are prevalent near and around the site;
 - In contrast, because of the location of the site and its local context, close to the M6 and northern urban edge of Rugby, the sense of 'emptiness' and 'inaccessibility' do not prevail, and any inherent sense of tranquillity is eroded accordingly;
 - The immediate site context, and site itself, are subject to the influence or urban fringe characteristics that are likely be exacerbated should the junction 1 MSA development proceed.

Site Analysis and Local Landscape Character

- 5.4. This can be summarised as follows:
- The site, especially the northern parcel, is generally very well visually contained within the wider landscape, by a combination of woodland belts to the north and west; by a mixture of old and new built form to the east; and by the Lime Avenue to the south;

- In contrast, the southern parcel of the site is open directly to the south, however the visual receptors from this aspect are most likely to be motorists using the A426 at speed and/or users of the roadside lay by;
- Consequently, the primary mitigation in terms of new development is already in place in the form of mature perimeter woodland planting belts;
- The land that comprises the site is part of the existing parkland, however its character has been eroded primarily by the removal of individual parkland trees, such that it contrasts with those parts of the parkland to the north and north-east/east of Coton House where many more parkland trees still characterise the landscape;
- The site has a recent commercial history, by virtue of its former role as a Royal Mail Training and Management Centre, and more recently the (Cala Homes) residential development. As such, it has recognised and accommodated modern change as opposed to retain a more static baseline dynamic – and it is perceived as such;
- The Cala Homes development has established a residential characteristic to the site in a sensitive and response manner, and with a high amenity value;
- On the northern parcel of the site, there are two large parkland trees, both Poplar (BS Category B2) – such species are not considered to be typical parkland tree species, however, there are more of them elsewhere within this parkland and as such they can be considered to be typical of this particular parkland;
- The Lime Avenue that extends along the site access drive from the A426 is a very recognisable and prominent parkland feature, in good condition (ref. Arboricultural Survey), and an element that should be considered as a key driver in any design and development process;
- Coton House (Grade II*) itself lies to the east of the site, and is generally visually separated from the majority of the site by existing vegetation and associated infrastructure, however its presence and visual relationship with the park and wider countryside are likewise a driver in the design process (see Heritage Assessment); and
- The woodland belts adjacent to the A426, are an important features not only in terms of mitigation but also in terms of the character of the parkland itself and

simultaneously, the wider landscape context. As such, they also remain a key driver for this reason and for ecological/habitat reasons.

- 5.5. In summary, the recent change in the character of built form on site has played a material role in characterising the existing landscape and visual baseline of the site and its local character. There are clearly some key landscape and visual drivers that must be recognised and play a role in the preparation of a masterplan design framework, and inherently matters of heritage and ecology as well. Key landscape and visual issues include recognition of the Lime avenue; retention and enhancement of the perimeter woodland belts; additional mitigation where required; the interface with the setting of the House and wider parkland; and clear acknowledgement and management of the transition between the two.

Development proposals

- 5.6. In summary the proposed development comprises:
- Residential development of 100 no. dwellings;
 - Associated highways infrastructure; and
 - Public open space and green infrastructure (including landscaping).
- 5.7. A full description of the proposed development is provided in the Design and Access Statement (DAS), prepared by Malcolm Payne Associates, that accompanies the planning application. As a general principle of the masterplanning and urban design approach, all residential units' face 'outwards' onto areas of different public open space. No units are proposed to 'back' on to any existing elements of existing landscape or other built infrastructure.
- 5.8. As part of the acoustic design, inherent in the masterplan layout are 4 no. locations on the western edge of the proposed development envelope, which together include 6 no. short lengths of solid boundary treatment rising to 3.2 metres AGL (Above Ground Level). These locations are illustrated in the Environmental Noise Assessment prepared by Cundall. Otherwise, all boundary treatments are set at 1.8 metres AGL where they define rear garden spaces, or lower where they occur in the public realm e.g. estate railings.

Likely causes of impact

- 5.9. Although a landscape has some intrinsic sensitivity, different landscapes contain a range of components which will respond differently to change, subject to the type of the

development that is proposed. Therefore, in order to inform the analysis of impacts, judgements should be made with reference to the specific changes which arise from the type of development being considered.

- 5.10. The following section sets out the likely causes of impacts which would occur in relation to the specific type of development proposed (i.e. residential led development).

Causes of temporary impact during construction

- 5.11. The temporary construction works which may give rise to impacts on landscape and visual receptors are listed as follows:

- site clearance and accommodation works (including vegetation clearance where required);
- movement and presence of associated construction vehicles and plant;
- presence of construction compounds, site offices and welfare facilities;
- earthworks and construction of internal road infrastructure and practical development platforms;
- phased implementation and emerging built form of residential units; and
- alterations to the existing road network for access improvements.

Causes of impacts at completion

- 5.12. The permanent components of the proposed development which may give rise to impacts on landscape and visual receptors are listed as follows:

- the built form of residential development (incorporating highways infrastructure); and
- mitigation integrated into the proposed development (i.e. green infrastructure and strategic landscaping), including retained trees, hedgerows, open space provision, and any new planting.

Constraints and opportunities

- 5.13. In the context of the likely impacts the following key constraints and opportunities have been identified during the landscape and visual analysis (including reference to field work and also to landscape character guidance).

Constraints

- 5.14. Constraints for the site are considered to be:

- The potential impact on the existing framework of vegetation (woodland belts; Lime Avenue; individual trees etc.) that are key to the legibility of the site and its parkland setting;
- The 'parkland' setting generally to the site and the presence of Grade II* listed Coton House to the east; and
- The alignment of the public right of way which passes through the southern parcel of the site.

Opportunities

5.15. Opportunities for the site are considered to be:

- Notwithstanding some localised views, the overall visibility of the site from the local and wider landscape is very limited due to the visual containment provided by the mature woodland belts and other tree cover;
- The opportunity to maintain and enhance the formal Lime avenue entrance to the wider site, and make it a key feature of the design framework;
- The precedent for new residential land use, set by the partial redevelopment of the built element of the Coton House Estate;
- In visual terms at a very local level, the proposed development will be seen in the context of existing and emerging new residential development on the wider Coton House estate to the south and east; and
- The opportunity to integrate new development within the existing landscape elements, creating an advanced landscape framework from the outset and working to retain and enhance the vegetation structure.

Landscape Strategy and Design

Overview

5.16. The landscape and visual strategy for the site is outlined below. It was initially formulated following an on-site design workshop (August 2016) that involved: Client; Architect; Heritage Consultant; and Landscape Architect. The reason for this was to gain an integrated and collaborative understanding of what the key (multi-disciplinary) drivers were to formulate a development proposal that would be acceptable across all disciplines, not least of all landscape planning and design. It included, for example, setting out a series of "set-backs" from the Lime avenue to understand what space was required to enhance its setting and ensure it played a focal role in the proposals; and consideration

of potential sight lines from Coton House itself. The overall approach, is founded on the following principles:

- Identification of a suitable 'development envelope', the location of which pays particular attention to the views to and across the site; site features and resources;
- Recognition and enhancement of the parkland character of the site, to ensure the development proposal both respects it and remains subservient to it;
- Wherever possible, retention and enhancement of the existing vegetation on the site, including the woodland belts and Lime avenue; and
- Creation of additional green infrastructure and open space on site, taking into account landscape character and visual containment of the site in order to propose landscape mitigation which is both consistent with and complementary to, the existing local landscape character in terms of scale, disposition and species mix.

Strategy components

5.17. The key elements of the landscape strategy have been considered as separate but integrated components; these are described in more detail in the following sections, and illustrated on **Figure 7: Illustrative Landscape Masterplan**.

Development Envelope and Layout

5.18. The development envelope is the defined area within which the residential built form will be contained; this is effectively a 'horizontal' parameter set for the scale of the proposed development. The development envelope has been defined by the site analysis and pays attention to a number of landscape related constraints and opportunities. This includes, following a series of iterations:

Northern Parcel:

- Restricting the northern edge of the envelope to a point south of the 2 no. large Poplar trees for two reasons: firstly, to retain a characteristic feature of this parkland i.e. the mature trees; and secondly to avoid and respect matters of setting and sight lines as discussed in the Heritage Assessment (ref para. 4.13 thereof);
- Retaining a set-back from the A426 perimeter woodland belt to ensure all Root Protection Areas (RPA's) are not compromised, and to ensure ecological aspects of the woodland belt are respected (with reference to light spillage and bat foraging);

- Retaining a large set back from the moat to the east, and its associated vegetation infrastructure to provide a significant “buffer” between the proposed development and the existing built form and the historic core of the site and associated features, including the moat;
- Retaining a deep (c. 15 metres between tree trunks and private drive, and c. 25 metres between trunks and front elevations) set back from the Lime avenue for several reasons: to again respect the RPA's; to ensure the scale of the space was commensurate with the scale of the landscape feature, not only in itself, but also to ensure it remains a key feature of the overall development proposal; and to ensure that the scale of the avenue would be appreciated from close quarters, and not just from a distance;

Southern Parcel:

- Lime Avenue: as above;
- A426 woodland Belt: as above;
- Retaining a set-back adjacent to the southern site boundary, where the aspect is currently open, to allow planting a native woodland belt to read consistently with those that are existing; and
- Retaining a set-back to the east, in respect of the amenity value of existing properties, and to be consistent with the set-back along the eastern edge of the northern parcel.

5.19. Regarding the ‘vertical’ parameter of the development envelope, this has been restricted to two storey units with a small number (12 in total) of two and a half storey units. This aligns with the scale and character of the recently constructed existing residential built form on site; and the landscape and visual analysis when considering views from the south.

Strategy for Existing Vegetation

5.20. From the discussion on the development envelope, it has been demonstrated that a lot of consideration has been given to the existing vegetation infrastructure. This has been further informed by the Arboricultural Survey. Virtually all the existing vegetation will be retained.

Site Access

- 5.21. In terms of the site access, this will be from the A426 via a new roundabout. There will be provision of a new pavement and lay-by along the eastern edge of the A426 that will connect through the woodland belt into the southern parcel. Any loss of vegetation in this instance will be minimised and localised.
- 5.22. Access into the development envelope will be along the existing drive, to a small 'cross-roads' towards its eastern end. This will necessitate the removal of 4 no. of the Category A1 Lime trees. It is considered preferable to make this 'break' towards one end of the avenue such that the major part of it remains intact. Whilst this 'gap' in the avenue will be appreciated from perpendicular views at close quarters, when travelling along the avenue, by foot, bicycle or car, the coalescence of the tree trunks and canopies will consequently ensure the overall integrity, legibility and functionality of the avenue will be retained.

Green Infrastructure (GI), Open Space, Landscape Character and Design

- 5.23. The inset diagram on **Figure 7: Illustrative Landscape Masterplan** sets out several landscape character areas across the development proposals, that work together as part the overall GI. These are effectively landscape design character areas, and their composition and function are discussed in turn below.

Retained Parkland

- 5.24. That section of the parkland right at the north-eastern end of the site, adjacent to the driveway to Coton House, will be retained in-situ. This area has a direct relationship with the House and its current composition will remain unchanged. Any access to it will be very informal, via mown paths extending from the other open spaces associated with the development.

Retained Woodland

- 5.25. As described above the mature flanks of woodland that extend around the western and northern boundaries of the site are integral to its character, and provide significant visual mitigation from day one. As such their retention and enhanced arboricultural and ecological management is important. They are an important part of the landscape design strategy, and access into them will be at a single point only, from the new pavement

along the A426 into the southern part of the site. Such a path can be aligned so as not to interfere with any large trees either by removal or by damage to RPA's.

Parkland Transition

- 5.26. This area extends westwards and south-westwards from the retained parkland area, along the interface between the development envelope and the existing built form on site, including new private residencies. In the area between the northern development envelope and the retained parkland, addition specimen trees will be planted in an area that is currently lacking them. This will create a 'transition' space between the wider retained parkland and the proposed development.
- 5.27. This approach is considered more appropriate, softening the interface between the parkland and built form, such that the development is perceived as being both within and subservient to the landscape infrastructure of the parkland. This is infinitely preferable than a more defensive approach such as planting a blanket screen of vegetation that is likely to 'sub-divide' the parkland.
- 5.28. To the east of the northern development envelope, the parkland transition provides a 'buffer' between the proposed and existing built form including the historic core and the moat and associated infrastructure. Access through these areas will be via a series of mown paths as opposed to hard landscape paths, which can be re-aligned on a seasonal basis.
- 5.29. To the east of the southern development envelope, the parkland transition provides an opportunity to accommodate a Locally Equipped Area for Play (LEAP) that is likely to take the form of a more 'natural' play area using very localised earth mounding, logs, boulders etc. as opposed to a more traditional, multi-coloured and fenced arrangement.
- 5.30. Tree planting in this parkland transition will include the following species, planted at heavy, extra heavy and semi-mature sizes:

<i>Quercus robur:</i>	Oak
<i>Quercus rubra:</i>	Red Oak
<i>Tilia cordata:</i>	Small Leaf Lime
<i>Pinus sylvestris:</i>	Scots Pine

Lime Avenue

- 5.31. The Lime Avenue is intended to be the central, key feature of the design proposals, and celebrated accordingly. Its function as an access avenue will be retained, and its setting enhanced. Adjacent proposed built form will be set back c. 25 metres. The space between the avenue and built form will be designed as a simple composition, including mown grass and a cast iron estate railing demarcating the boundary between the public and private realms.
- 5.32. The hard landscape design associated with the proposed units that front on to the avenue will retain a consistency with this approach and be in the form of a bound gravel, or similar. It is also important to note that proposed planting in the adjacent parkland transition areas will take on an informal layout so as not to compete with the formal strength of the existing Lime avenue.
- 5.33. The scale of the open space that will flank the avenue is very important. It needs to ensure the protection of the RPA's and allow for a setting of the trees that is commensurate with their scale and presence on site, such that it is enhanced accordingly. It also needs however to be of sufficient depth such that when standing in the adjacent space, the height, scale and habit of the trees can be appreciated and from close quarters and not simply from a distance.

Residential Green

- 5.34. The residential green comprises two areas, one on the northern envelope and one within the southern envelope. These areas will provide smaller pockets of open space within the residential enclave, and again be simple in their composition: mown grassed areas; bound gravel pathways with occasional seating areas; and groups of more formal tree planting defining and enclosing the spaces, in locations where they will not compete with the Lime avenue. Tree planting will include the following species, planted at heavy and extra heavy sizes:

<i>Liquidambar styraciflua</i> :	Sweet Gum
<i>Pyrus calleryana</i> 'chanticleer':	Ornamental Pear
<i>Acer campestre</i> 'Elsrijk':	Field Maple
<i>Prunus avium</i> 'Plena':	Scots Pine

Amenity Landscape

- 5.35. This comprises two linear open spaces, set between the (north) western edge of the development envelope and the retained woodland along the A426 boundary. They will provide a setting for this edge of the proposed development, that is fully accessible to offer a 'looped' walk around the development. They could also include areas of woodland edge planting, and long lengths of hedging as part of the ecological mitigation and design. Hedge species could comprise a single species of either:

Fagus sylvatica; Beech

Carpinus betulus; Hornbeam

Or alternatively take on a more 'natural' mix to include:

Crataegus monogyna; Hawthorn

Prunus spinosa; Blackthorn

Ilex aquifolium; Holly

Woodland Infrastructure Belt

- 5.36. At the southern end of the southern parcel, there is an open aspect southwards, from where there are views into the site. It is proposed to provide a narrower, but just as effective a visual screen (following establishment) along this boundary, that effectively demarcates the edge of development, and the edge of the parkland.
- 5.37. On this basis, it will form a similar boundary treatment, in time, as that already along the A426 frontage. The species mix will be consistent with that in the other woodland belts except for Sycamore.

Public Access

- 5.38. The single public footpath across the southern parcel of the site will be diverted. As it enters the site, it will follow the pavement, cross the residential green, and then cross the parkland transition buffer to the east at which point it will re-join its original alignment. So, whilst there will be a diversion, it will be more of an adjustment to its alignment as opposed to a more convoluted change.

Summary

- 5.39. The measures described above are considered to be integral to the aim of ensuring that this is perceived as a high quality, responsive design solution based on an iterative and collaborative multi-disciplinary approach. The elements of parkland landscape infrastructure will be central to the design response, and the built form will remain subservient to them, such that their cohesive legibility will be retained.
- 5.40. The measures are also considered integral to the landscape and visual mitigation process, which are considered in the balance of judgements when determining the magnitude of impacts and significance of effect, as set out in sections 6 and 7 of this report.

6. ASSESSMENT OF LANDSCAPE EFFECTS

Overview of Landscape Effects

- 6.1. Landscape sensitivity is a term applied to specific receptors, combining judgements on the value related to a landscape (i.e. the receptor) with the susceptibility of the landscape to the specific type of change proposed. Receptors can include specific landscape elements or features or may be judged at a wider scale and include landscape character parcels, types or areas.
- 6.2. As advocated in the GLVIA3, professional judgement is used to balance analysis of value and susceptibility in order to determine sensitivity. Each of these aspects of the analysis will vary subject to the scale and detail of the assessment.
- 6.3. The landscape character of the study area is documented at national, regional and at a more local level. The findings of these studies represent a thorough and generally consistent analysis of landscape character and this has been supported by an analysis of the local landscape character of the site in its context.
- 6.4. This assessment of landscape effects focuses on the areas of landscape character which is defined at a borough level which, as previously noted in the baseline section of this LVIA, presents guidance at an appropriate scale and detail for the area. The assessment is then also applied at a more refined level to consider additional judgements based on the local landscape character.

Landscape Sensitivity

- 6.5. In order to inform judgements on value and susceptibility the following section refers to the baseline information (Section 3) and additional consideration of the local character in relation to the site and its immediate context. These judgements are then carried through to the analysis of landscape sensitivity.

Landscape Value

- 6.6. In LVIA, landscape value is the value attached to a potentially affected landscape. It is relative in relation to the different stakeholders and different parts of society that use or experience a landscape. Although factors such as formal designations are an important component when determining value, other aspects are also considered as part of the judgement process. These include issues related to condition (of features and elements), seclusion and presence of detracting influences, rarity and the degree of

representativeness. Landscape value will vary in response to the specific landscape that is being considered, even where a landscape is included in the boundaries of a formal designation.

Value of Open Plateau LCT

- 6.7. This section determines the value of the defined LCT relevant to the site and study area. The site is located within the Open Plateau LCT. The considerations and professional judgements used in determining value are summarised in the following table.

Table 3: Determining Landscape Value

Considerations	Open Plateau LCT
Landscape quality (condition), scenic quality and perceptual aspects	The published assessment states that there is an extensive area to the north of Rugby where the condition is weak. To the north and west of Newbold on Avon, as too with land around Coton House, condition is strong. Elsewhere it is in decline. Tranquillity is relatively high away from urbanising influences such as major transport corridors and built form, but much reduced when closer to the urban fringe and transport corridors.
Rarity and representativeness	The open plateau landscape extends across the Coton House Estate and the northern edge of Rugby and covers an extensive area of land to the north of the M6 motorway. It is not considered that this landscape type is rare, nor does it contain any elements / features that are important examples.
Conservation interests	There are several heritage designations within the open plateau in this location, these include a limited number of scheduled monuments, the conservation area at Churchover and occasional listed buildings, in Coton House.
Recreation value	The PROW network across the open plateau in this location is relatively limited, in particular to the east. To the west the network is slightly more extensive.
Associations	There are no known associations with particular people or events in history in relation to the open plateau landscape type in this location.

- 6.8. On balance the wider landscape context of the Open Plateau LCT is considered to be of **medium value** in landscape terms.

Value of the Local Landscape and Site Character

- 6.9. The site and its immediate context are not covered by any statutory or non-statutory designations. Coton House Estate is an example of a parkland in the wider landscape and whilst the House is Grade II* listed, the landscape has no formal protection. In addition,

the parkland and specifically the built core has a recent history of change of use and redevelopment for residential purposes. The site is therefore set within the context of a new and emerging residential development and is simultaneously visually contained by the mature vegetation along boundaries and internally.

- 6.10. In addition, its location near the existing urban edge of Rugby which includes at this point motorway infrastructure and large scale commercial development, serves to dilute any prospect of remoteness, inaccessibility and tranquillity – the influence of the wider ‘open plateau’ landscape to the north is much reduced. Whilst the site and its context retain some value from the agricultural setting and views across the open farmland, this is very limited and in many cases views also include the large scale commercial built form. The influence of the urban fringe is very much present, physically, visually and auditorily. Approval of the adjacent MSA planning application will serve to reinforce this.
- 6.11. Overall the condition of the landscape elements associated with the site is good, with woodland belt boundaries intact and a strong Lime avenue along the access to the built core. The published character assessment states that condition of the landscape in the context of Coton House is “strong”, and that is evident on site.
- 6.12 In overall terms, the enclosure of the Coton House Estate together with its strong and legible landscape features need to be considered in the context of the recognisable changes that have taken place in recent years on site, together with its urban fringe influence. The local landscape character of the site and its immediate context is considered, on balance, to be of **medium value** in landscape terms.

Landscape Susceptibility

- 6.13 In LVIA, landscape susceptibility is the ability of a landscape to accommodate change without undue consequences for the maintenance of the baseline situation. Different types of development can affect landscapes in different ways, therefore landscape susceptibility is specific to the type of development proposed (i.e. residential).

Susceptibility of the Open Plateau LCT

- 6.14 This section determines the susceptibility of the defined LCA relevant to the site and the local landscape character. The considerations and professional judgements used in determining susceptibility are summarised in the following table.

Table 4: Determining landscape susceptibility

Considerations	Open Plateau LCT
Scale of enclosure	Generally open and large-scale landscape with sometimes limited along roadsides.
Nature of land use	Some existing reference to residential development at areas of settlement (for example Churchover) otherwise residential built form is generally limited to more isolated properties and farmsteads. Area of large scale built form at Rugby Gateway.
Nature of existing elements	Arable practices leading to some intensification over time and loss of hedgerows. PROW network across the area. Limited woodland cover.
Nature of existing features	Mix of features that reflect both positively and negatively on the landscape, including smaller village edges amongst a medium to large scale field pattern but also major road infrastructure to the south.

- 6.15 On balance the wider landscape context of *the Open Plateau LCT* is considered to be of **medium susceptibility** in landscape terms.

Susceptibility of the Local Landscape Character

- 6.16 In terms of susceptibility, the local landscape character of the site itself benefits from a greater sense of enclosure by existing mature vegetation and more recently, residential development (both existing and emerging) than the wider LCT. This contributes substantially to the capacity of the site to accommodate residential development. The current land use on site is rough pasture with parkland trees; this land use in itself is typically susceptible to the development of any new residential scheme, but can also be incorporated into areas proposed for green infrastructure, as described.
- 6.17 The influence of both existing and emerging housing within the boundaries of the Coton House Estate creates a strong reference and context to the type of development proposed. This is also influenced by the large scale built form south of the M6 corridor, seen in some local views towards and from the site.
- 6.18 Landscape features that form the parkland such as the perimeter woodland belts; mature parkland trees; and the Lime avenue, can be readily accommodated into a residential layout and therefore opportunities are available to retain the very vast majority of these landscape elements as part of a scheme, reducing susceptibility. Effectively, the majority of the visual mitigation is in place from day one.
- 6.19 There are also opportunities for creation and enhancement of new green infrastructure and landscape planting which would be beneficial to landscape elements and reduces

susceptibility. The proposed woodland infrastructure planting along the south-western edge of the site is a good example of this.

- 6.20 In relation to the matters described above, the site and its local landscape character is, on balance, also considered to be of **medium susceptibility** in landscape terms.

Landscape Sensitivity

- 6.21 The following conclusions on sensitivity are based on the detailed description and justification presented in the previous sections, balancing the professional judgements on value and susceptibility. Following a review of the types of impact on physical landscape resources, the conclusions on landscape sensitivity are then taken forward to address the impact and effect on landscape character.
- 6.22 Overall the landscape analysis has determined the *Open Plateau LCT* to be of *medium value* and *medium susceptibility*. Therefore, the LCT within the study area is considered to be of **medium sensitivity** in landscape terms.
- 6.23 At the local level, the landscape analysis has determined the character of the site and its immediate context to be of *medium value* and *low to medium susceptibility*. Therefore, the site and its local landscape context is also considered to be of **medium sensitivity**.

Landscape impacts

Impacts on Physical Landscape Resources

- 6.24 The following section describes the predicted changes to the physical landscape elements and features on the site that will give rise to the subsequent perceived changes in landscape character.
- 6.25 Construction impacts will include initial ground clearance, earthworks, demolition and limited clearance of existing vegetation. This process will also include the implementation of temporary measures such as site hoardings, temporary fencing and vegetation/tree protection measures. These impacts will be temporary.
- 6.26 Impacts at completion are concerned with the long term alteration in the landscape from the current undeveloped context of the site to the future scenario of the proposed development. The built form of the proposed development will be complete and will be a permanent component in the landscape.

- 6.27 In the long term, impacts will be associated with the influence of mitigation measures on landscape character. This establishes the changes to landscape character as a result of built development but with proposed mitigation measures fully established and performing their intended function. The impacts are considered to be long term and not reversible.
- 6.28 In terms of physical landscape resources, the direct changes will be restricted to parts of the site. These will include the very minimal changes to landform required for the creation of practical development platforms and impacts generated by the change in land use from the current field enclosures to residentially led development; areas of existing green infrastructure will be retained and enhanced and new areas of open space created (including proposed landscape planting). The exception to this is the four mature Lime trees that are likely to be removed in order to accommodate highways access. Whilst this comprises a permanent loss of a physical landscape component, it will be balanced by a significant amount of new planting as part of the landscape design strategy.
- 6.29 Overall the physical landscape impacts are considered to be direct and will be limited to the certain parts of the site only. There will be no additional direct impacts on the wider landscape context of the *Open Plateau LCT*.
- 6.30 In the context of the impacts considered above, the following sections set out an assessment of the likely landscape effects on the relevant LCT and local landscape character.

Open Plateau LCT

- 6.31 The Open Plateau LCT is an extensive area of character that extends across the Coton House Estate and the northern edge of Rugby. It includes areas of more open agricultural landscape and excludes more urban areas, including the industrial area south of Cosford. The M6 corridor passes through the LCT to the south but overall it covers the predominantly open, rural landscape to the north of the motorway.
- 6.32 The site forms only a very discrete parcel of land within the wider LCT and is located within a more intimate piece of landscape characterised by the setting of Coton House grade II* listed building and its associated parkland and mature vegetation. This vegetation serves to enclose the site and its immediate setting from the wider landscape. The main impacts are therefore likely to arise on the site itself. Therefore, the degree of change to the Open Plateau LCT is likely to be very limited. Overall the magnitude of

impact to the LCT will be **neutral**; assessed alongside the **medium** sensitivity, this will result in a **neutral** effect.

Local Landscape Character

- 6.33 Effects on the local landscape character are defined as those occurring on site and in the immediate context of the site.
- 6.34 In the context of the site and local landscape character the proposed development will result in the direct, partial loss of two parcels of grassland which will be replaced with residential development. Four mature Lime trees will be removed to accommodate access; however new tree planting will be introduced to account for the loss of these trees. Balanced with this is also the alignment of the development envelope to allow a large area of open space and green infrastructure as part of the 'parkland transition' character area which will complement the existing parkland to be retained. The character of this open space will vary slightly to the existing parkland as it will be set in the context of new built form to the south-west, however grassland and parkland trees will remain part of the landscape and green infrastructure.
- 6.35 Retained and proposed woodland belts along the boundaries of the site will help to screen and filter views of new built form, from day one in many aspects. Notwithstanding some minimal losses for access, trees and woodland are generally to be retained around the site and will be enhanced through additional planting and management. This includes new woodland infrastructure planting along the south-western boundary of the site.
- 6.36 In terms of settlement pattern, the proposed development will form an extension to the existing, recently built residential development within the wider Coton House estate. The development envelope for the residential built form has been restricted based on the landscape and visual constraints and opportunities, as described in section 5 of this Report, such that the legibility of the parkland will be retained and remain dominant to the scale, form and character of the proposed development.
- 6.37 Vegetation and green infrastructure of the site will generally be improved through retention and enhancement, and notwithstanding the potential loss of some mature Lime trees along the access road, the impact on vegetation as a whole is considered to be beneficial in terms of landscape character as the programme of planting, maintenance and management will positively influence both the existing green infrastructure network and also the existing landscape components.

- 6.38 The public footpath which passes through the southern parcel of the site will be realigned but enter and exit the site at its current entry and exit points. Across the site additional access will be created through formation of open spaces (including residential greens and the transitional parkland).
- 6.39 Visual impacts of the proposed development are addressed separately in this LVIA; this considers the impact on specific visual receptors. However, in landscape character terms, the principle that the proposed development will have a limited influence due to the restricted inter-visibility with the wider landscape, reduces the overall level of impact.
- 6.40 Overall, regarding the local landscape character, the magnitude of impact is **low**. Assessed alongside the **medium** sensitivity, this will result in a **minor adverse** effect.
- 6.41 Regarding the site-specific character, the likely magnitude of impact will inevitably increase, as would the likely significance of effects. This however is the case with the vast majority of developments of similar scale on green field sites, and is largely due to the change in land use form, in this instance, from rough pasture and parkland to residential built form. This magnitude of effect however must be considered in terms of the retention of the landscape features that give the parkland its legibility, and incorporating them as design features; allied to the sensitive and responsive design approach to the architecture of the proposed built form, as evidenced in that which has recently been built in the central part of the Estate.

7 ASSESSMENT OF VISUAL EFFECTS

Visual sensitivity

- 7.1 The sensitivity of a visual receptor is a function of the value attached to a particular view balanced with the susceptibility of the visual receptor to changes in a view and/or visual amenity. The criteria for the sensitivity of visual receptors are set out in the detailed methodology (**Appendix A**).

Visual impacts

- 7.2 Visual impacts are considered separately to landscape impacts. For landscape impacts it is necessary to understand the combination of direct and indirect impacts on the landscape resources potentially affected by a proposed development and therefore it is possible to provide a description and overview of the key impacts that are likely to affect the study area.
- 7.3 However, for visual receptors it is necessary to understand the specific, direct impacts on each view. Therefore, the causes of impact are considered on the basis of individual receptors and are set out in the following sections as an integral part of the assessment of visual effects.

Visual effects

- 7.4 The following section summarises the main visual impacts which are likely to be generated by the proposed development. This includes reference to the likely significance of effects on specific visual receptors. A number of representative viewpoints were captured during the field work and these are presented as a series of panoramic photos and included as **Figure 5, Viewpoint Photographs 1 to 12**.
- 7.5 A detailed analysis has been completed for each of the representative viewpoints. This includes reference to the sensitivity of the visual receptors and the nature and degree of the likely changes to the view.
- 7.6 The following table sets out the detailed visual impact assessment and is followed by a summary description of visual effects.

Table 3: Assessment of visual effects

Viewpoint	Sensitivity	Description of change and nature of impact	Magnitude and significance of effects Completion	Magnitude and significance of effects Year 15
Viewpoint 1: View from junction between Coton Road and Lutterworth Road, looking south-east.	Medium	<p>From this location there will be very limited views of the rooflines of new residential properties in the middle distance, filtered by the woodland belt in the middle distance. Changes to the view would be barely perceptible.</p> <p>This is viewed in the context of the industrial units at Rugby Gateway which are visible in the background of the view.</p>	<p>Mag. Negligible</p> <p><i>Sig. Neutral to negligible adverse</i></p>	<p>Mag. Negligible</p> <p><i>Sig. Negligible adverse</i></p>
Viewpoint 2: View from A426, looking south-west.	Medium	<p>From this location there will be glimpsed, limited filtered views of the proposed development, including new residential properties beyond the existing woodland belt along the northern extent of the site.</p>	<p>Mag. Negligible to low</p> <p><i>Sig. Negligible adverse</i></p>	<p>Mag. Negligible to low</p> <p><i>Sig. Negligible adverse</i></p>

Viewpoint	Sensitivity	Description of change and nature of impact	Magnitude and significance of effects Completion	Magnitude and significance of effects Year 15
Viewpoint 3: View from public right of way (bridleway) northeast of Coton Estate, looking south-west.	High	<p>From this location there will be extremely (if any) limited filtered views of the rooflines of new residential properties at distance. Coton Spinney will screen the very vast majority of any new development to the centre-right of the view. Mature trees along the eastern boundary of Coton House estate also form a screening function. This is in the context of large scale industrial development at 'Central Park' to the south of the M6 corridor.</p> <p>In the longer term, mitigation, including additional tree planting throughout the proposed 'parkland transition' area will be established and performing its intended function. This will further enclose and screen views (if any) of built form and help integrate and provide a transition between the site and adjacent countryside.</p>	<p>Mag. Negligible <i>Sig. Neutral</i></p>	<p>Mag. Negligible <i>Sig. Neutral.</i></p>
Viewpoint 4: View from Newton Lane north of M6, looking north-west.	Medium	<p>From this location there are not likely to be any views of the proposed development due to a combination of topography and intervening vegetation, in particular that associated with the south-eastern extent of the Coton House estate.</p>	<p>Mag. Nil <i>Sig. Nil</i></p>	<p>Mag. Nil <i>Sig. Nil</i></p>

Viewpoint	Sensitivity	Description of change and nature of impact	Magnitude and significance of effects Completion	Magnitude and significance of effects Year 15
Viewpoint 5: View from public right of way (footpath) adjoining M6, looking north-west.	High	From this location there are not likely to be any views of the proposed development due to a combination of rising topography in the foreground and intervening vegetation infrastructure, in particular the coniferous vegetation associated with the south-eastern extent of the Coton House estate, as well as the emerging built form directly behind those trees.	Mag. Nil <i>Sig. Nil</i>	Mag. Nil <i>Sig. Nil</i>
Viewpoint 6: View from public right of way (footpath) adjoining M6, looking north-west.	High	<p>From this location there be will direct views of part of the southern parcel of the site, including new residential built form and proposed woodland infrastructure belt along the southern boundary of the site. The proposed development will form a minor extension to the existing recently built residential built form in the view. Views of the northern parcel of the site will be screened by existing, recently built housing development and vegetation.</p> <p>In the longer term, the proposed woodland infrastructure belt will be established sufficient to perform its intended function. This will further help to screen views of built form and help integrate the proposed development into the landscape and provide a transition between the site and adjacent countryside.</p>	Mag. Medium <i>Sig. Minor adverse</i>	Mag. Low <i>Sig. Negligible</i>

Viewpoint	Sensitivity	Description of change and nature of impact	Magnitude and significance of effects Completion	Magnitude and significance of effects Year 15
Viewpoint 7: View from southbound layby on A426, looking north-east.	Medium	<p>From this location there will be short distance, direct views of the southern parcel of the proposed residential development and proposed woodland infrastructure belt along the southern boundary of the site.</p> <p>In the longer term, the proposed woodland infrastructure belt will be established sufficiently to perform its intended function. This will further help to screen views of built form and help integrate the proposed development into the landscape and provide a transition between the site and adjacent countryside.</p>	<p>Mag. High</p> <p><i>Sig. Moderate adverse</i></p>	<p>Mag. Medium</p> <p><i>Sig. Minor adverse</i></p>
Viewpoint 8: View from public right of way (footpath) north of M6, looking north.	High	<p>From this location there will be very close, direct views of the southern parcel of the proposed residential development and proposed woodland infrastructure belt along the southern boundary of the site. Views of the northern parcel of the site will be generally screened by the built form on the western parcel and existing mature vegetation.</p> <p>In the longer term, proposed planting in the area of public open space in the southern corner of the site will be established sufficient to soften the appearance of built form in views across it from this location.</p>	<p>Mag. Very high</p> <p><i>Sig. Major adverse</i></p>	<p>Mag. Very high</p> <p><i>Sig. Major/moderate adverse</i></p>

Viewpoint	Sensitivity	Description of change and nature of impact	Magnitude and significance of effects Completion	Magnitude and significance of effects Year 15
Viewpoint 9: View from public right of way (footpath) adjacent James Arthur Drive, looking south.	High	<p>From this location there will be immediate, direct views of the southern parcel of the proposed residential development, set back from the footpath which will be set in an area of open space. Development will also be set back from the existing avenue of mature lime trees to the left of the view. In this particular instance, views of the existing commercial development in Central park across the open enclosure will effectively be displaced by high quality residential architecture.</p> <p>In the longer term, proposed planting in the area of public open space in the foreground will be established sufficient to soften the appearance of built form in views across it from this location.</p>	<p>Mag. Very high</p> <p><i>Sig. Moderate adverse</i></p>	<p>Mag. Very high</p> <p><i>Sig. Moderate adverse</i></p>

Viewpoint	Sensitivity	Description of change and nature of impact	Magnitude and significance of effects Completion	Magnitude and significance of effects Year 15
Viewpoint 10: View from James Arthur Drive, looking west.	Medium/High	<p>From this location, there will be immediate, direct views of the northern parcel of the proposed residential development, set back from a proposed footpath which will pass through an area of open space in the foreground. Development will also be set back from the existing avenue of mature lime trees to the left of the view.</p> <p>In the longer term, proposed planting in the area of public open space in the foreground will be established sufficient to soften the appearance of built form in views across it from this location.</p>	<p>Mag. Very high <i>Sig. Major adverse</i></p>	<p>Mag. Very high <i>Sig. Major adverse</i></p>
Viewpoint 11: View from A426 at entrance to Ashtree Farm, looking north-east.	Medium	<p>From this location, there will be filtered views of the proposed development, including new residential units, set behind the existing woodland belt that defines the western extent of the Coton House Estate. The proposed footway along the A426 will be visible on the edge of the A426 carriageway, and elements of the access roundabout will also be visible in the highway corridor.</p> <p>In the longer term, proposed tree planting associated with the edge of the development envelope in this location will be established sufficient to soften the appearance of built form in views across it from this location.</p>	<p>Mag. Medium <i>Sig. Moderate adverse</i></p>	<p>Mag. Low <i>Sig. Minor adverse</i></p>

Viewpoint	Sensitivity	Description of change and nature of impact	Magnitude and significance of effects Completion	Magnitude and significance of effects Year 15
Viewpoint 12: View from Coton Road, looking south-east.	Medium	<p>From this location, there will be short distance, filtered views of the proposed development, including new residential units, set behind the existing tree belt that defines the western extent of the Coton House estate along the A426. From this location the existing perception of depth across the site will be displaced by elements of built form in part.</p> <p>In the longer term, proposed tree planting associated with the edge of the development envelope in this location will be established sufficient to soften the appearance of built form in views across it from this location.</p>	<p>Mag. Medium</p> <p><i>Sig. Moderate adverse</i></p>	<p>Mag. Low</p> <p><i>Sig. Minor adverse</i></p>

Summary

- 7.7 In summary, the assessment of likely visual effects serves to demonstrate how well visually contained the site is. The vast majority of adverse effects are likely to be at very close proximity adjacent to the site (which is typically the case with all forms of similar development) and where the public footpath currently crosses it. At greater distance, and with the exception of the view from the south (view 7) the visual effects in the wider landscape are likely to be very limited.
- 7.8 It is important to consider that these effects are assessed as mid-winter visual effects, a worse-case scenario. With respect to a summer assessment, all the views from the local and wider landscape outside the site would be influenced by the perimeter woodland belts in full leaf. These belts would effectively visually screen the development from day one.
- 7.9 It is also important to consider that views from the existing public footpath that extends across the southern parcel of the site, will of course change. However, when walking this footpath in a southerly direction, the current view, across the southern parcel, is towards the large scale commercial development across the M6 corridor (view 9), with the majority of the parkland of the Estate behind the viewer and playing a much-reduced role in the view. Whilst the change in view will be of a high magnitude, this needs to be balanced with the change from a motorway corridor and adjacent lit junction, and commercial development beyond; to one of a high quality residential development set within a robust and attractive landscape framework.
- 7.10 In terms of the small and isolated lengths of 3.2. metre high boundaries required as part of the acoustic mitigation, it is considered that these elements will read as part of the overall architectural and urban design of the proposed development, and not as any form of acoustic barrier that is likely to generate any further adverse visual effects.

8 SUMMARY AND CONCLUSIONS

Overview

- 8.1 This landscape and visual impact assessment (LVIA) has been prepared to determine the likely effects of the proposed development of land at Coton House Estate, Churchover, Rugby. The LVIA has addressed the following landscape resources and visual receptors:
- Landscape character, including physical landscape elements and features; and
 - Views and visual amenity experienced by recreational users (including visitors and tourists) and road users.
- 8.2 The LVIA identifies the key constraints and opportunities present on the site and in the surrounding landscape, and the nature of the likely impacts that may arise from the proposed development. The LVIA has analysed the baseline information in the context of the proposed development and has informed the proposals for landscape mitigation, strategy and design.

Landscape Character

- 8.3 In terms of landscape character, the site and its context are considered in detail in a number of published assessments working down from National to Local Level. The site is an example of an historic parkland as part of a wider estate.
- 8.4 Both the site and its context are heavily influenced by existing urban fringe activity, including the presence of large scale commercial built form in many views; major highways infrastructure, and associated noise. There is also a live planning application for a MSA, in close proximity to junction 1, just south-west of the site. These elements serve to cast great influence; more so than the published characteristics for the local Landscape Character Type that are typically more present and represented in areas much further to the north, away from the urban edge.
- 8.5 On the site itself, the parkland structure, whilst evident, is reduced in comparison with areas of the parkland directly north and west, by virtue of reduced tree cover. It does however include a mature Lime avenue. Directly east of the site, modern residential development is now a characteristic of the built core of the Estate.
- 8.6 It is considered that the effects on the wider landscape character are likely to be neutral; and that the effects on the local landscape character are likely to be minor adverse.

Visual Amenity

- 8.7 The site is very well visually contained in the wider landscape by the mature woodland belts that flank its northern and western boundaries; the parkland tree cover to the east; and more modern tree belts to the south-east including several conifers. Open views extend from the A426 from the south across an open boundary; and from the public footpath that crosses the southern parcel of the site.
- 8.8 Therefore, the likely visual effects are limited from the wider landscape, and greater in close proximity and typically from locations very close to the site boundary, or in the site itself. These views are, to a greater or lesser extent, capable of being mitigated, through additional infrastructure planting and well considered, sensitive landscape, urban and architectural design.

Design Proposals

- 8.9 The proposed development comprises 100 no. residential units, means of access, landscaping, SUD's and Green Infrastructure. The proposals have been concluded on the basis of an integral and collaborative design process that has undergone a number of iterations. Landscape and visual matters have played an important role in determining the extent of the development envelope.
- 8.10 The final design solution will ensure that the overall legibility of the parkland landscape that encloses the site will be retained, not least of all the mature Lime avenue that will be a central feature in the design, together with a 'transition' from the built form into the wider parkland to the north and east. As such, the landscape will remain dominant to the scale and character of the proposed built form.
- 8.11 The landscape and visual strategy and design will generate additional Green Infrastructure (GI), with reference to Policy CS14 of the Core Strategy and Policy NE3 of the Emerging Local Plan; and is in integral to the overall design approach and solution, with reference to Saved Policy GP2 of the Local Plan. It is also considered that the design approach positively addresses the criteria set out in Policy NE4 (Landscape Protection and Enhancement) of the Emerging Local Plan, with all important site features (Lime avenue etc.) retained, with reference to Policy SDC2.

Conclusion

- 8.12 In conclusion, it is considered that the likely landscape and visual effects have been identified in accordance with recognised guidance and methodology, with mitigation inherent in the design process and solution. As such the proposals are acceptable in landscape and visual terms.

Appendix A

Detailed LVIA Methodology

FIGURES

A. Appendix A: Landscape and visual effects detailed methodology (GLVIA3)

A.1. INTRODUCTION

A.1.1 This assessment aims to determine the likely effects of the proposed development on the existing landscape and visual receptors in the study area. The following landscape resources and visual receptors have been addressed:

- Physical landscape features and elements;
- Landscape character; and
- Views and visual amenity experienced by residents, recreational users (including visitors and tourists) and road users.

A.1.2 This assessment details the impacts that may result as a consequence of the proposed development and considers the likely significance of effect arising as a result.

A.2. APPROACH

A.2.1 The approach and methodology used for this assessment has been developed in accordance with the guidance in the following documents:

- Landscape Institute and Institute of Environmental Management and Assessment (April 2013) Guidelines for Landscape and Visual Impact Assessment 3rd Edition;
- Natural England (October 2014) An Approach to Landscape Character Assessment; and
- Landscape Institute Advice Note 1/11 Photography and Photomontages Guidance.

A.2.2 The overall approach to the identification and assessment of landscape and visual effects is summarised as follows:

- determining the scope of the assessment;
- collating baseline information for landscape and visual receptors, including completing desk study research and undertaking field based survey work;
- review the proposed development and identify and describe the likely impacts of the proposed development (enabling specific judgments to be made on sensitivity of landscape and visual receptors);
- establish the sensitivity of landscape and visual receptors (balancing judgments on susceptibility and value);

- determine the magnitude of impacts (balancing judgments on size / scale, duration and reversibility);
- the assessment of the likely significance of landscape and visual effects through a balanced approach and clear description of judgments on sensitivity and magnitude; and
- the identification of measures to avoid or remedy impacts and the subsequent re-assessment of likely effects.

A.2.3 The following sections provide further detail on this approach.

Determining the Scope of Assessment

Spatial Scope

- A.2.4 The spatial scope for the assessment has been determined by a two stage approach. Firstly, a 'preliminary study area' is identified. This is based on the wider setting and context of the site and sets the broad parameters for collation of baseline information; this scope also accounts for the potential effects that will be generated by the proposed development.
- A.2.5 In order to focus on the key sensitive receptors and likely effects the spatial scope of the preliminary study area is then refined through the initial stages of the assessment (i.e. desk study and field survey work).
- A.2.6 The visual envelope of the site has been considered through desk top analysis of topographical data combined with field surveys to investigate visual enclosure arising from landform, vegetation and built form.

Collating Baseline Information

- A.2.7 In order to capture a comprehensive description of the baseline position for landscape and visual receptors, information has been collated using desk study and field survey work. These processes include reference to published landscape character studies and a range of views and visual receptor types.

Desk Study

- A.2.8 The desk study has identified potentially sensitive landscape resources by reference to OS maps and existing published landscape character studies, relevant planning policy guidance and/or designated or protected views. This stage has also enabled the identification of potential visual receptors such as public rights of way (PROW), residential properties or designated areas.

Field Survey

- A.2.9 Detailed field survey work for this LVIA has further identified landscape elements and features that contribute to the landscape character of the area and visual receptors that will have potential views of the site.
- A.2.10 A series of representative photographs were taken during the field work, in February 2017. The photographs were taken with a digital camera with a 50mm lens (equivalent focal length). These are presented as a series of panoramic viewpoints included to illustrate landscape character in the area and also as specific representative viewpoints. These have been used to inform the assessment of both landscape and visual impacts.

Assessment of Effects

- A.2.11 Having established the relevant baseline position, the assessment process then completes the following specific stages:
- Evaluate the sensitivity of the landscape receptors and visual receptors, specifically in response to the type of proposed development (sensitivity of landscape resources is not standard and depends on the nature and type of development proposed);
 - Identify the potential magnitude of impact on the physical landscape, on landscape character and on visual receptors; and
 - Combine professional judgments on the nature of the receptor (sensitivity) and the nature of the change or impact (magnitude) to arrive at a clear and transparent judgment of significance.
- A.2.12 For both landscape and visual effects, the final conclusions on significance are based on the combination of sensitivity and magnitude. The overall judgment on significance is based on the combination of each of the criteria. The rationale for the balance and justification for each judgement is expressed in the detailed analysis.
- A.2.13 To draw a distinction between different levels of significance, a scale for the degrees of significance, along with criteria and definitions, have been developed. These provide a structure for making judgements which are clear and objective. However, it is necessary to remember that landscapes and interactions in the landscape are both complex and subtle; as such an element of subjectivity remains. No landscape will fit wholly into any one definition and to try would require extensive and complex criterion.
- A.2.14 Therefore, professional judgements draw together conclusions in respect of sensitivity, magnitude and significance are fully and clearly described by the detailed written

analysis presented in the LVIA, supported by descriptive thresholds and criteria for each of these stages in relation to landscape impacts and, separately, visual impacts are set out in the following sections.

A.3. ASSESSMENT OF LANDSCAPE EFFECTS

Overview of landscape sensitivity

- A.3.1 Although landscape has some intrinsic sensitivity, different landscape receptors have different elements and features that can accommodate a variety of development types.
- A.3.2 To reliably inform detailed assessment of impacts, landscape sensitivity needs to be determined with reference to the changes arising from the specific type of development in question. Therefore, landscape sensitivity is assessed combining judgements on the value attached to a landscape and the susceptibility to the type of change and nature of the development proposed.

Landscape value

- A.3.3 Landscape value is the relative value attached to a potentially affected landscape. Landscape value will vary in relation to the different stakeholders and different parts of society that use or experience a landscape.
- A.3.4 Landscape value is not solely indicated by the presence of formal designations and a range of factors influence landscape value. Factors that have been considered in making judgements on landscape value include designations (both national and local), local planning documents, status of features (e.g. TPO's or Conservation Areas) and local community and interests (for example local green spaces, village greens or allotments). Landscape value will vary in response to the specific landscape that is being considered in relation to its condition, sense of seclusion or isolation, the presence or absence of detracting features and the presence or absence of rare or distinctive elements and features and to what degree these form key characteristics.
- A.3.5 The following table sets out the criteria that have been considered for determining landscape value.

Table A.1: Criteria for landscape value

Value	Criteria
High	<p>Designated areas at an International or National level (including, but not limited to, World Heritage Site, National Parks, AONB's) and also considered an important component of the country's character, experienced by high numbers of tourists.</p> <p>Landscape condition is good and components are generally regularly maintained to a high standard.</p> <p>In terms of seclusion, enclosure by land use, traffic and movement, light pollution and presence / absence of major infrastructure, the landscape has an elevated level of tranquillity.</p> <p>Rare or distinctive elements and features are a key component that contribute to the character of the area.</p>
Medium	<p>Designated areas at a Regional or County level (including, but not limited to, green belt, regional scale parks, designated as open space or a Conservation Area in local planning documents) and also considered a distinctive component or the region/county character experienced by a large proportion of its population.</p> <p>Landscape condition is fair and components are generally relatively well maintained.</p> <p>In terms of seclusion, enclosure by land use, traffic and movement, light pollution and presence / absence of major infrastructure, the landscape has moderate levels of tranquillity.</p> <p>Rare or distinctive elements and features are a notable component that contribute to the character of the area.</p>
Low	<p>No formal designations but a landscape of local relevance (including, but not limited to, public or semi-public open spaces, village greens or allotments) and also green infrastructure and open spaces within residential areas likely to be visited and valued by the local community.</p> <p>Landscape condition is poor and components are generally poorly maintained or damaged.</p> <p>In terms of seclusion, enclosure by land use, traffic and movement, light pollution and presence / absence of major infrastructure, the landscape has limited levels of tranquillity.</p> <p>Rare or distinctive elements and features are not a notable component that contribute to the character of the area.</p>

Landscape susceptibility

A.3.6 The second component of landscape sensitivity relates to susceptibility. Landscape susceptibility to change is the ability of a landscape to accommodate change without undue consequences for the maintenance of the baseline situation. In this context, the term landscape receptors can be expanded to cover character areas, particular landscape character types or an individual landscape element or feature. Landscape

susceptibility will vary in response to the specific landscape that is being considered and to the nature or type of change that may occur.

A.3.7 The following table sets out the criteria that have been considered for determining landscape susceptibility.

Table A.2: Criteria for landscape susceptibility

Susceptibility	Criteria
High	<p>Scale of enclosure – landscapes with a low capacity to accommodate the type of development proposed due to the interactions of topography, vegetation cover and built form.</p> <p>Nature of land use – landscapes with no or very little existing reference or context to the type of proposed development.</p> <p>Nature of existing elements – landscapes with components that are not easily replaced or substituted (e.g. ancient woodland, mature trees, historic parkland).</p> <p>Nature of existing features – landscapes where detracting features or major infrastructure is not present or where these are present but their influence on the landscape is limited.</p>
Medium	<p>Scale of enclosure – landscapes with a medium capacity to accommodate the type of development proposed due to the interactions of topography, vegetation cover and built form.</p> <p>Nature of land use – landscapes with some existing reference or context to the type of proposed development.</p> <p>Nature of existing elements – landscapes with components that are easily replaced or substituted.</p> <p>Nature of existing features – landscapes where detracting features or major infrastructure is present and the influence of these on the landscape is noticeable.</p>
Low	<p>Scale of enclosure – landscapes with a high capacity to accommodate the type of development proposed due to the interactions of topography, vegetation cover and built form.</p> <p>Nature of land use – landscapes with extensive existing reference or context to the type of proposed development.</p> <p>Nature of existing elements – landscapes with components that are easily replaced or substituted, or where there are few/no existing elements present (e.g. cleared brownfield sites).</p> <p>Nature of existing features – landscapes where detracting features or major infrastructure is present and the influence of these on the landscape is dominant.</p>

Landscape sensitivity

A.3.8 Landscape sensitivity is a term applied to specific receptors, combining judgements of the susceptibility of the receptor to the specific type of change or development proposed

and the value related to that receptor. Receptors can include specific elements or features or may be judged at a wider scale and include landscape character parcels, types or areas.

A.3.9 Having considered in detail the contributing factors to landscape value and the susceptibility of the site and surrounding area to the type of the development proposed, conclusions on landscape sensitivity can be drawn by balancing the judgements on value and susceptibility.

A.3.10 As advocated in the GLVIA3, professional judgement is used to balance judgements on value and susceptibility in order to determine sensitivity. Each of these aspects of the analysis will vary subject to the scale and detail of the assessment. Overall judgements on landscape sensitivity are subsequently described as; 'very high', 'high', 'medium', 'low' or 'negligible'.

Magnitude of landscape impacts

A.3.11 The effect on landscape receptors is assessed in relation to the size or scale of impact, the geographical extent of the change and the duration and the reversibility of the impact. The magnitude of landscape effects has been assessed in accordance with the criteria set out in the following table.

Table A.3: Criteria for determining magnitude of landscape impacts

Magnitude	Criteria
Very high	<p>The size and scale of change is considered very large due to the extent and proportion of loss of existing landscape elements or the degree of alteration to aesthetic or perceptual aspects.</p> <p>The nature and scale of change to key characteristics which are critical to character is considered very large.</p> <p>Where the geographical extent would have a very substantial influence on the landscape at a regional scale, i.e. across several landscape character areas/types.</p> <p>Duration of impacts would be considered very long term and where the potential reversal of the impact is not likely and in practical terms would be very difficult to achieve.</p>

Magnitude	Criteria
High	<p>The size and scale of change is considered large due to the extent and proportion of loss of existing landscape elements or the degree of alteration to aesthetic or perceptual aspects.</p> <p>The nature and scale of change to key characteristics which are critical to character is considered large.</p> <p>Where the geographical extent would have a substantial influence on the landscape at a regional scale, i.e. across several landscape character areas/types.</p> <p>Duration of impacts would be considered long term and where the potential reversal of the impact is not likely and in practical terms would be very difficult to achieve.</p>
Medium	<p>The size and scale of change is considered moderate due to the extent and proportion of loss of existing landscape elements or the degree of alteration to aesthetic or perceptual aspects.</p> <p>The nature and scale of change to key characteristics which are critical to character is considered moderate.</p> <p>Where the geographical extent would influence the landscape at a local scale, i.e. a single landscape character area/type (or potentially multiple areas/types where a site is located on the boundary between areas).</p> <p>Duration of impacts would be considered midterm and where the potential reversal of the impact is likely and in practical terms would be difficult to achieve.</p>
Low	<p>The size and scale of change is considered small due to the extent and proportion of loss of existing landscape elements or the degree of alteration to aesthetic or perceptual aspects</p> <p>The nature and scale of change to key characteristics which are critical to character is considered small</p> <p>Where the geographical extent would influence the landscape in the immediate setting of the site, i.e. limited to the influence of part of a single landscape character area/type</p> <p>Duration of impacts would be considered short term and where the potential reversal of the impact is more likely and in practical terms would easily be achieved</p>
Negligible	<p>The size and scale of change is considered very small due to the extent and proportion of loss of existing landscape elements or the degree of alteration to aesthetic or perceptual aspects</p> <p>The nature and scale of change to key characteristics which are critical to character is considered very small</p> <p>Where the geographical extent would substantially influence the landscape of the site only</p> <p>Duration of impacts would be considered very short term and where the potential reversal of the impact is very likely or committed and in practical terms would very easily be achieved</p>

A.3.12 These judgements are then taken forward to an assessment of the significance of landscape effects.

A.4. ASSESSMENT OF VISUAL EFFECTS

- A.4.1 Visual receptors include a particular person or groups of people likely to be affected at a specific viewpoint or series of viewpoints.

Visual sensitivity

- A.4.2 Sensitivity of visual receptors is determined through balancing judgements on the value attached to a particular view against the receptors susceptibility to change in a view or visual amenity. Given the need to address the specific issues of the proposed development these factors in the context of visual sensitivity are considered as part of the assessment of visual effects.
- A.4.3 The value attached to a view includes recognition of value through formal designations (for example planning designations or heritage assets), indicators of value attached to views by visitors (for example inclusion on maps/guide books, provision of facilities, presence of interpretation).
- A.4.4 For example views of higher value are likely to be from designated landscapes where the condition or scenic quality of the view is higher and where distinctive elements or features form a prominent part of a view; views of lower value are likely to be from area of landscapes where the condition and scenic quality of the view is poorer, where there is no reference to distinctive elements or features and where detracting features are prominent in the view.
- A.4.5 The susceptibility of different visual receptors to changes in views and visual amenity is judged based on the activity of people experiencing the view at any given time or location and the extent to which their attention would be focused on the view and visual amenity rather than on the activity being undertaken.
- A.4.6 For example, views more susceptible to change are likely to be permanent views, in unenclosed or elevated positions in the landscape and where the landscape forms a primary focus for the activity of the receptor; views less susceptible to change are likely to be transient or temporary views, located in enclosed areas of the landscape where the landscape is a secondary focus or consideration to the activity of the receptor.
- A.4.7 The following table sets out the definitions of sensitivity for different visual receptors.

Table A.4: Criteria for visual sensitivity

Sensitivity	Definition
Very high	Designated or protected views or views from publically accessible locations in protected landscapes Tourists and visitors to heritage assets, or other attractions, where views of the surroundings are an important contributor to the experience and visit
High	Occupiers of residential properties People who are engaged in outdoor recreation whose attention is likely to be focussed on the landscape People travelling through the landscape on roads, rail or other transport routes where this involves recognised scenic routes and an awareness of views and visual amenity
Medium	People travelling more generally through the landscape on roads, rail or other transport routes People staying in hotels and healthcare institutions People at work and in educational institutions where visual amenity is an important contributor to the setting and quality of working life
Low	People at work and in educational institutions where the visual setting is not important to the quality of working life People engaged in formal sports where the visual setting may play a role but attention is focused on the activity Views from publically accessible locations in degraded landscapes

A.4.8 It should be noted that as professional judgement is applied to the balance of value and susceptibility of visual receptors, there may be some instances where a typical receptor is defined a different degree of sensitivity to the guidance included in the table, above.

Magnitude of visual impacts

A.4.9 The effect on visual receptors is also assessed in relation to the size or scale of change, the geographical extent of the change, the duration of the change and the reversibility of the impact. The magnitude of visual impacts has been assessed in accordance with the criteria set out in the following table.

Table A.5: Criteria for determining magnitude of visual impacts

Magnitude	Criteria
Very High	<p>The size and scale of change is considered very substantial due to the extent of loss, addition or alteration of features, the changes to the composition of the view including the proportion of the view occupied by the proposal, the degree of contrast and the nature of the experience</p> <p>The geographical extent in relation to the angle, distance and proportion of visibility is considered as very extensive</p> <p>Duration of impacts would be considered long term and where the potential reversal of the impact is not likely and in practical terms would not be achievable</p> <p>Alteration in very close proximity</p>
High	<p>The size and scale of change is considered substantial due to the extent of loss, addition or alteration of features, the changes to the composition of the view including the proportion of the view occupied by the proposal, the degree of contrast and the nature of the experience</p> <p>The geographical extent in relation to the angle, distance and proportion of visibility is considered as extensive</p> <p>Duration of impacts would be considered long term and where the potential reversal of the impact is not likely and in practical terms would be very difficult to achieve</p> <p>Alteration in close proximity</p>
Medium	<p>The size and scale of change is considered fair due to the extent of loss, addition or alteration of features, the changes to the composition of the view including the proportion of the view occupied by the proposal, the degree of contrast and the nature of the experience</p> <p>The geographical extent in relation to the angle, distance and proportion of visibility is considered as small or intermediate</p> <p>Duration of impacts would be considered medium term and where the potential reversal of the impact is likely and in practical terms would be difficult to achieve</p>
Low	<p>The size and scale of change is considered small due to the extent of loss, addition or alteration of features, the changes to the composition of the view including the proportion of the view occupied by the proposal, the degree of contrast and the nature of the experience</p> <p>The geographical extent in relation to the angle, distance and proportion of visibility is considered as limited</p> <p>Duration of impacts would be considered short term and where the potential reversal of the impact is very likely and in practical terms would easily be achieved</p>

Magnitude	Criteria
Negligible	<p>The size and scale of change is considered very small due to the extent of loss, addition or alteration of features, the changes to the composition of the view including the proportion of the view occupied by the proposal, the degree of contrast and the nature of the experience</p> <p>The geographical extent in relation to the angle, distance and proportion of visibility is considered as very limited</p> <p>Duration of impacts would be considered very short term and where the potential reversal of the impact is very likely or committed and in practical terms would very easily be achieved</p>
Nil	There is no view of the proposed development in the view

A.4.10 These judgements are then taken forward to an assessment of the significance of visual effects.

A.5. DEFINING SIGNIFICANCE OF EFFECTS

A.5.1 For both landscape and visual effects the final conclusion on the significance of an effect is based on the combination of sensitivity of receptor and magnitude of change (or impact). The rationale for the overall judgement on significance is based on the combination of each of the criteria individually leading to the balance and justification of these.

A.5.2 Detailed assessment is a means of drawing together, in a systematic way, an assessment of the likely significant environmental effects of a proposed development; however not all landscape and visual effects arising will be significant.

A.5.3 Determination of the significance of an effect requires the application of professional judgement to balance the findings in relation to the sensitivity of the receptor and the magnitude of the predicted impacts.

A.5.4 The GLVIA3 advocate a move away from formulaic matrices and tables and encourages an approach using professional judgement. Analysis and consideration of value and susceptibility gives rise to a spectrum of judgements on sensitivity, which along with magnitude inform decision making of the effects and help to determine the acceptability of a proposal in landscape and visual terms.

A.5.5 The criteria for determining the significance of effects for landscape and visual impacts are set out in the following tables, below. These criteria are based on guidance provided by the Landscape Institute.

Table A.6: Criteria for determining significance of landscape effects

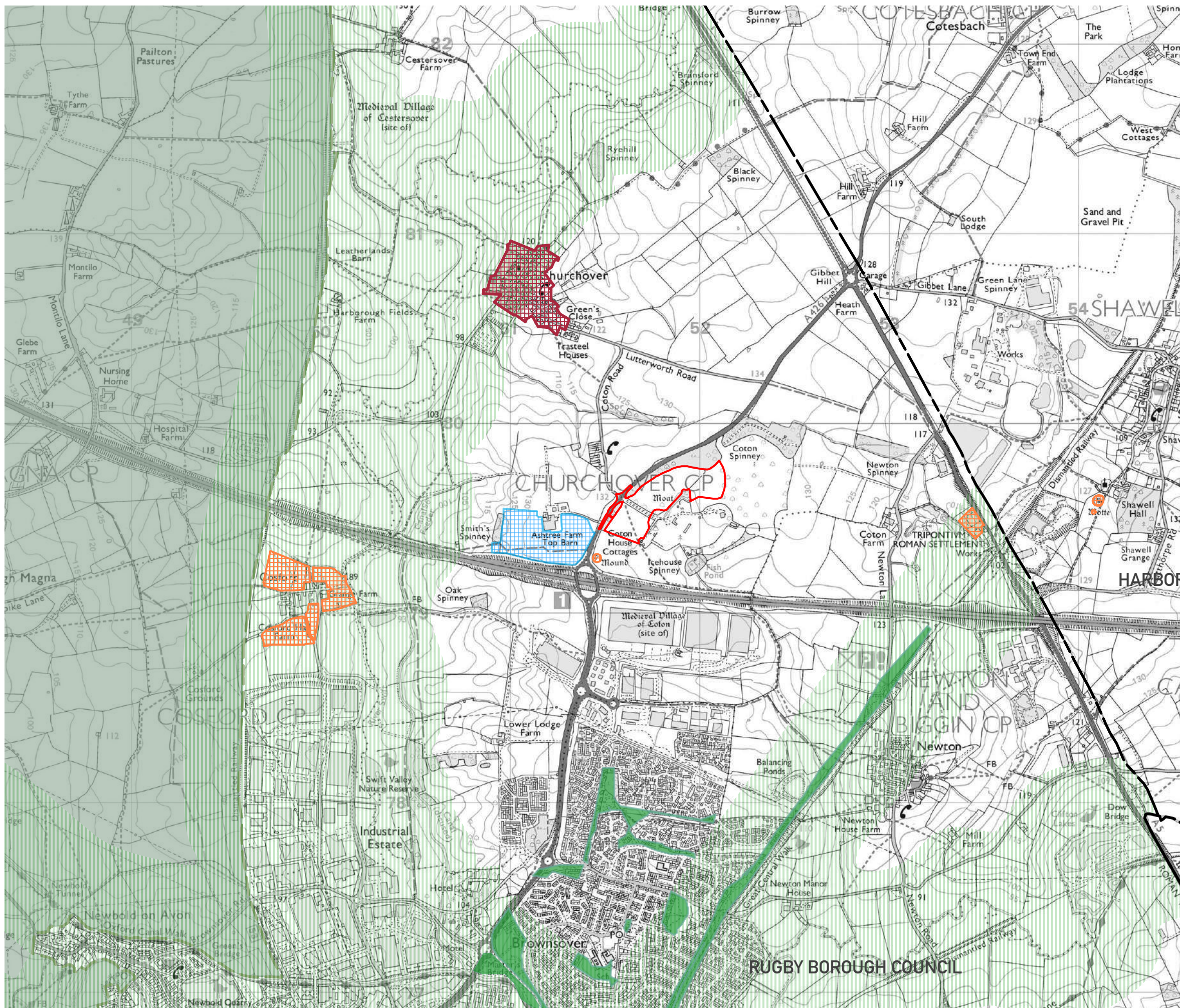
Significance of Effect	Description The proposed development would:
Major Adverse (Negative) Effect	Be at considerable variance with the character of the receiving landscape. Degrade or diminish the integrity of a range of characteristic elements and features. Damage the sense of place.
Moderate Adverse (Negative) Effect	Show some variance or inconsistency with the character of the receiving landscape. Have an adverse impact on characteristic elements and features. Detract from the sense of place.
Minor Adverse (Negative) Effect	Not quite fit the character of the receiving landscape. Be at variance with characteristic elements and features. Have a limited influence on sense of place.
Neutral/Negligible Effect	Maintain the character of the receiving landscape. Blend in with characteristic elements and features. Enable the sense of place to be retained.
Minor Beneficial (Positive) Effect	Complement the character of the receiving landscape. Maintain or enhance characteristic elements and features. Enable some sense of place to be restored.
Moderate Beneficial (Positive) Effect	Improve the character of the receiving landscape. Enable the restoration of characteristic elements and features partially lost or diminished as a result of changes from inappropriate management or development. Enable the sense of place to be restored.
Major Beneficial (Positive) Effect	Enhance the character of the receiving landscape. Enable the restoration of characteristic elements and features lost as a result of changes from inappropriate management or development. Enable the sense of place to be enhanced.

Table A.7: Criteria for determining significance of visual effects

Significance of Effect	Description
Major Adverse	The proposed development project would cause major deterioration to a view from a highly sensitive receptor, and would constitute a major discordant element in the view.

Significance of Effect	Description
Moderate Adverse	The proposed development would cause obvious deterioration to a view from a moderately sensitive receptor, perceptible damage to a view from a receptor of lower sensitivity or limited damage to views to receptors of higher sensitivity.
Minor Adverse	The proposed development would cause limited deterioration to a view from a moderately sensitive receptor, or cause greater deterioration to a view from a receptor of lower sensitivity.
Negligible Adverse	The proposed development and associated changes would be barely perceptible in a view. Changes will be negative (adverse) however this degree of change is not likely to be material and therefore no distinction is made.
Neutral	The change in the view would be barely perceptible or perceptible but would not be apparent as either a positive or negative change.
Nil	There would be no view of the proposed development.
Negligible Beneficial	The proposed development and associated changes would be barely perceptible in a view. Changes will be positive (beneficial) however this degree of change is not likely to be material and therefore no distinction is made.
Minor Beneficial	The proposed development would cause limited improvement to a view from a moderately sensitive receptor, or would cause greater improvement to a view from a receptor of lower sensitivity.
Moderate Beneficial	The proposed development would cause obvious improvement to a view from a moderately sensitive receptor, perceptible improvement to a view from a receptor of lower sensitivity or limited improvements to views to receptors of higher sensitivity.
Major Beneficial	The proposed development would lead to a major improvement in a view from a highly sensitive receptor.

A.5.6 For both landscape and visual effects, interim categories of 'negligible to minor', 'minor to moderate' and 'moderate to major' are used where the judgements on an effect are determined to fit across the descriptive criteria for significance banding.



KEY

- Site boundary
- Proposed MSA Development
- LPA boundary
- Green Belt (CS1*)
- Strategic Green Infrastructure (CS14*)
- Open Space (LR4*8)
- Conservation Area
- Scheduled Monument

Sources:

- * Core Strategy (June 2011), Rugby Borough Council
- ** Saved Policies of the Local Plan
- ** www.magic.gov.uk

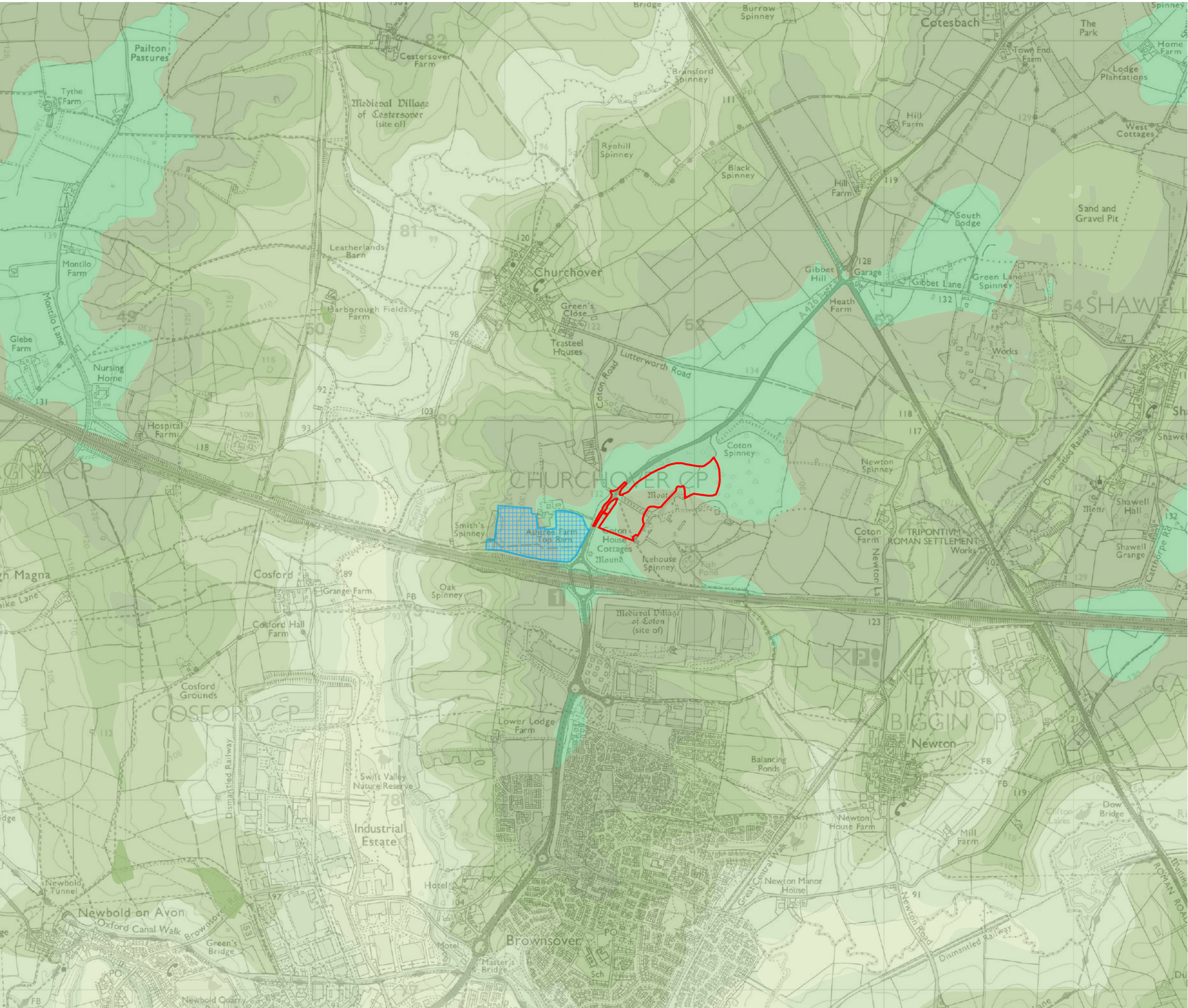
Coton House & Estate, Rugby

Client: CALA Homes

Fig. 1: Site Location and Planning Designations

Drawing no. : BIR4821_08
Date : 13/03/2017
Drawn by : LG/JT
Checked by : KM
Scale : 1 : 20000 @ A3





KEY

Site boundary

Proposed MSA Development

140 - 150m AOD

130 - 140m AOD

120 - 130m AOD

110 - 120m AOD

100 - 110m AOD

90 - 100m AOD

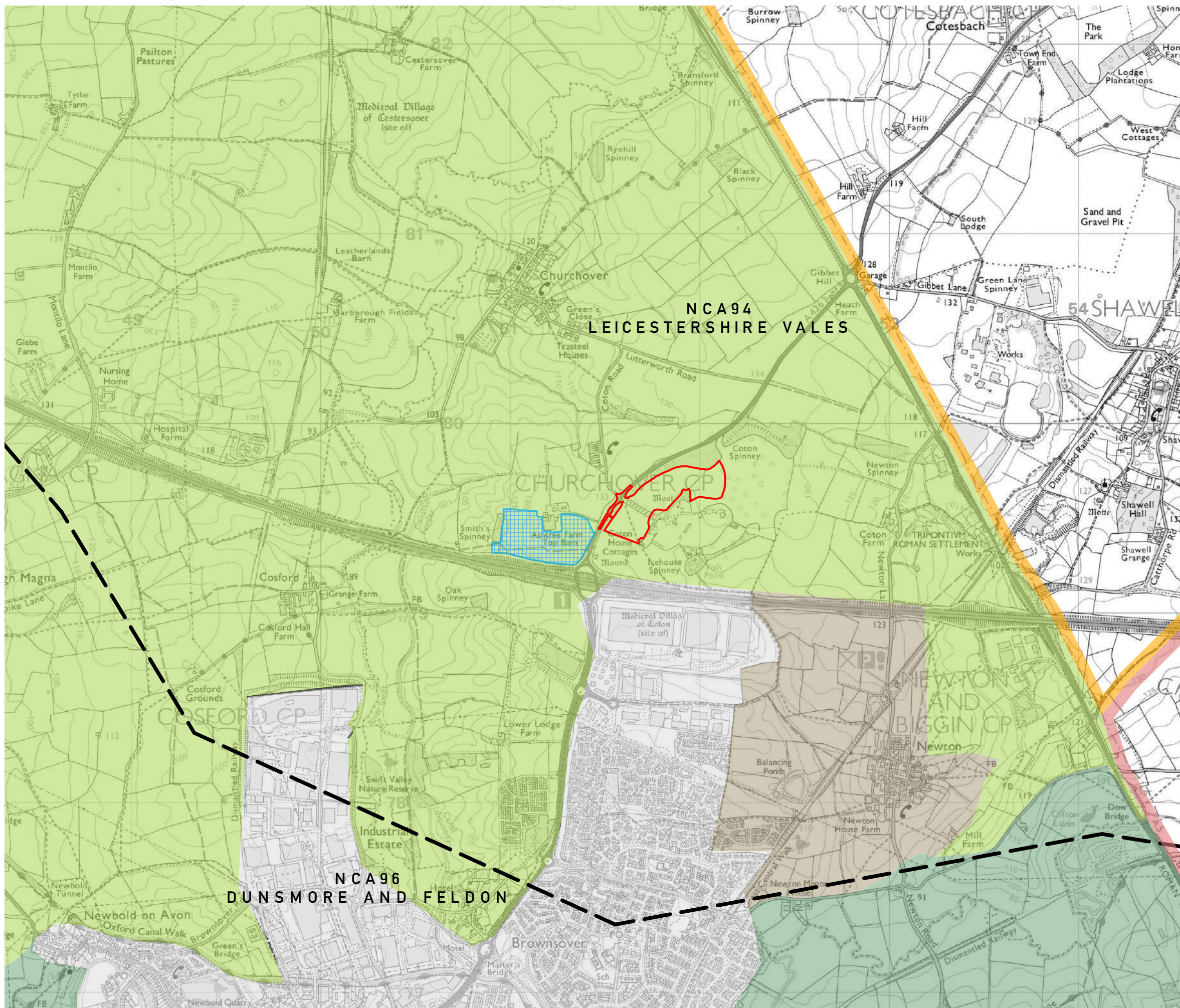
80 - 90m AOD

70 - 80m AOD

Coton House & Estate,
Rugby
Client: CALA Homes
Fig. 2: Topography

Drawing no. : BIR4821_09
Date : 13/03/2017
Drawn by : LG/JT
Checked by : KM
Scale : 1 : 20000 @ A3





KEY



Site boundary



National Character Area boundary



Proposed MSA Development

Landscape Assessment of the Borough of Rugby (2006)



Dunsmore:
Plateau fringe



High Cross Plateau:
Village farmlands



High Cross Plateau:
Open Plateau



Urban/urban edge

Landscape Assessment of the Harborough District (2007)



Lutterworth:
Lowlands



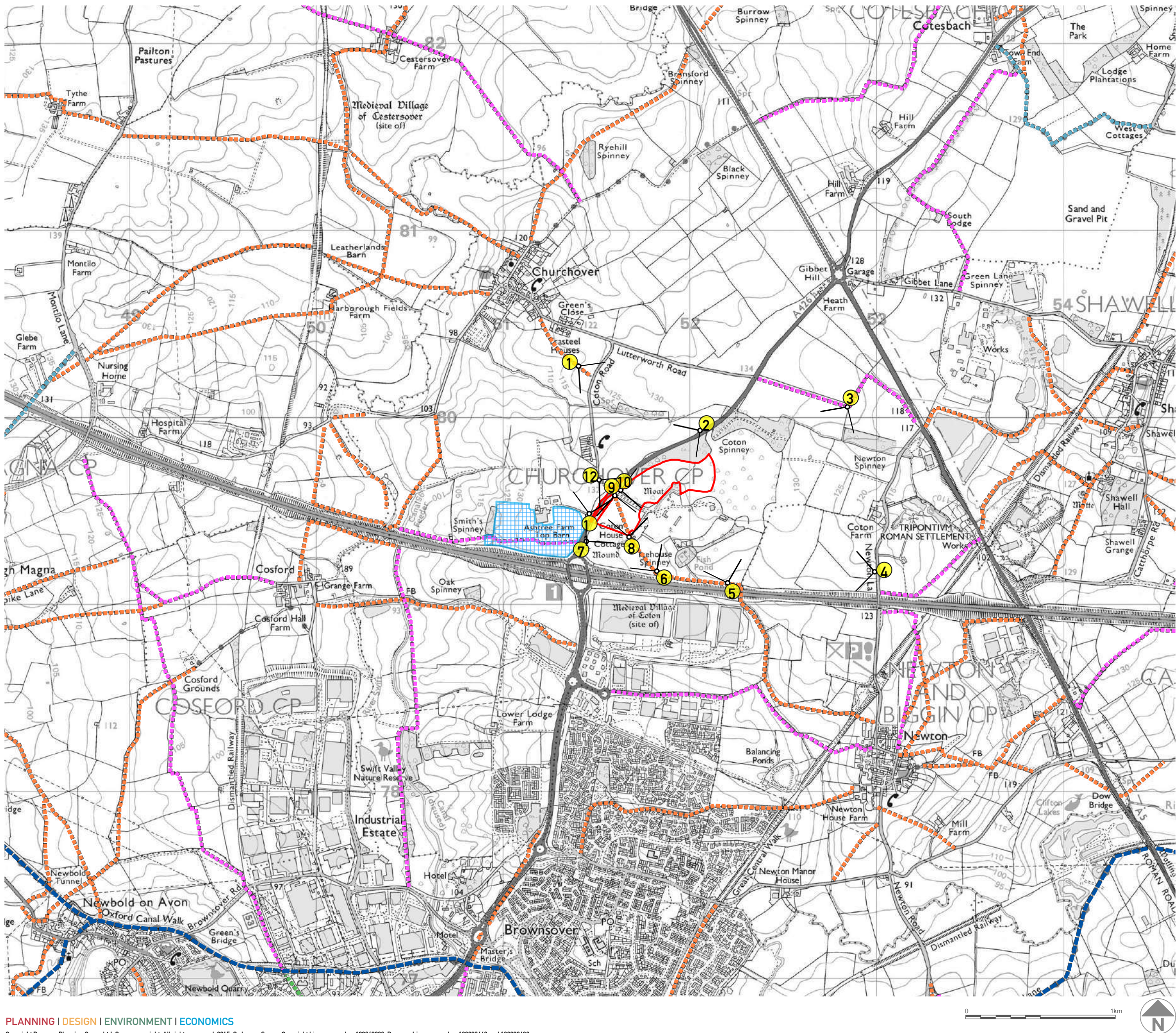
Laughton Hills

Coton House & Estate, Rugby

Client: CALA Homes

Fig. 3: Landscape Character

Drawing no. : BIR.4821_10
Date : 13/03/2017
Drawn by : LG/JT
Checked by : KM
Scale : 1 : 20000 @ A3



KEY

- Site boundary
- Proposed MSA Development
- Public footpath
- Public bridleway
- Recreational route
- Cycle route
- Other routes with public access
- Viewpoint location

**Cotons House & Estate,
Rugby**
Client: CALA Homes

Fig. 4: Viewpoint Locations and Public Rights of Way

Drawing no. : BIR4821_11
Date : 13/03/2017
Drawn by : LG/JT
Checked by : KM
Scale : 1 : 20000 @ A3





Viewpoint 1	View from junction between Coton Road and Lutterworth Road, looking south-east
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451600 280450
Approx elevation	+125m AOD
Distance to site	c. 700m



Viewpoint 2	View from A426, looking south-west
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	452050 279900
Approx elevation	+133m AOD
Distance to site	c. 100m

Coton House and Estate,
Rugby
Cala Homes

Fig. 5 Viewpoint Photographs



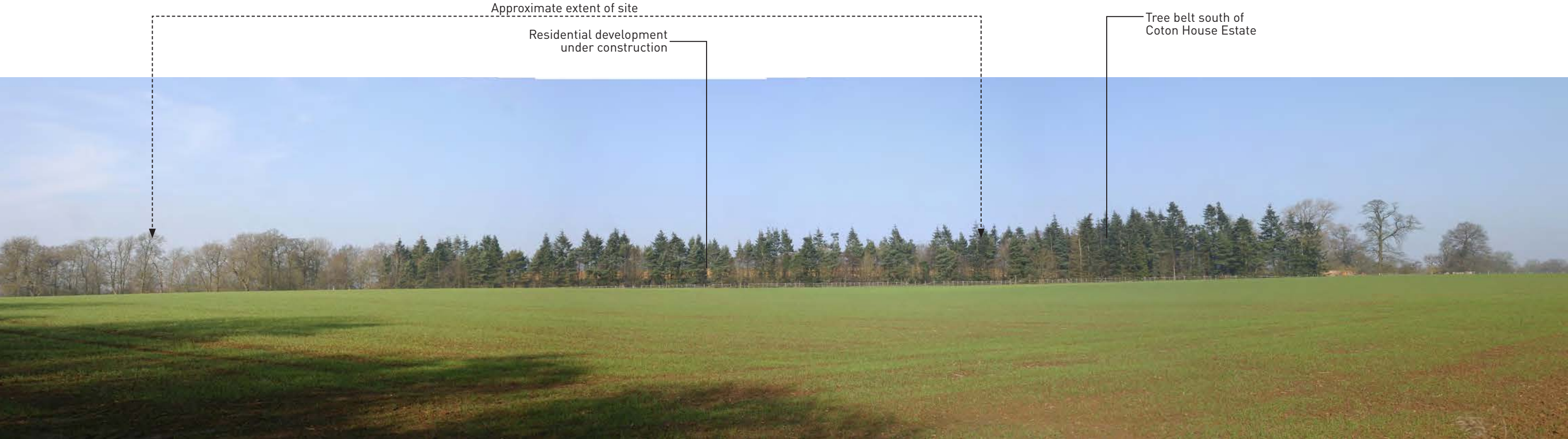
Viewpoint 3	View from public right of way (bridleway) north-east of Coton Estate, looking south-west
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	452850 280050
Approx elevation	+127m AOD
Distance to site	c. 750m



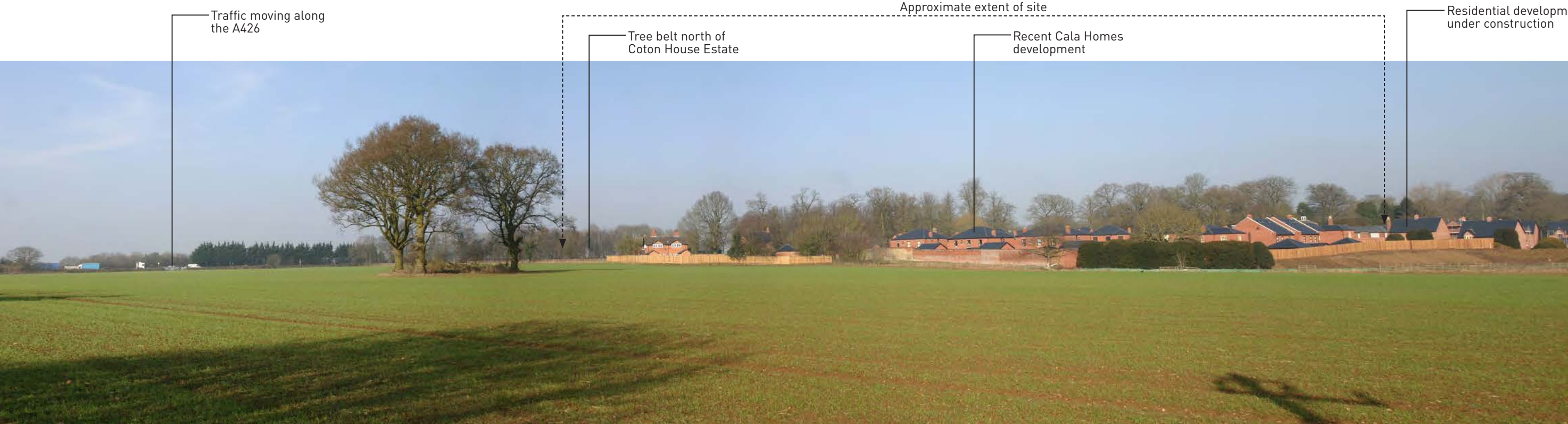
Viewpoint 4	View from Newton Lane north of M6, looking north-west
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	453000 279150
Approx elevation	+125m AOD
Distance to site	c. 1000m

Coton House and Estate,
Rugby
Cala Homes

Fig. 5 Viewpoint Photographs



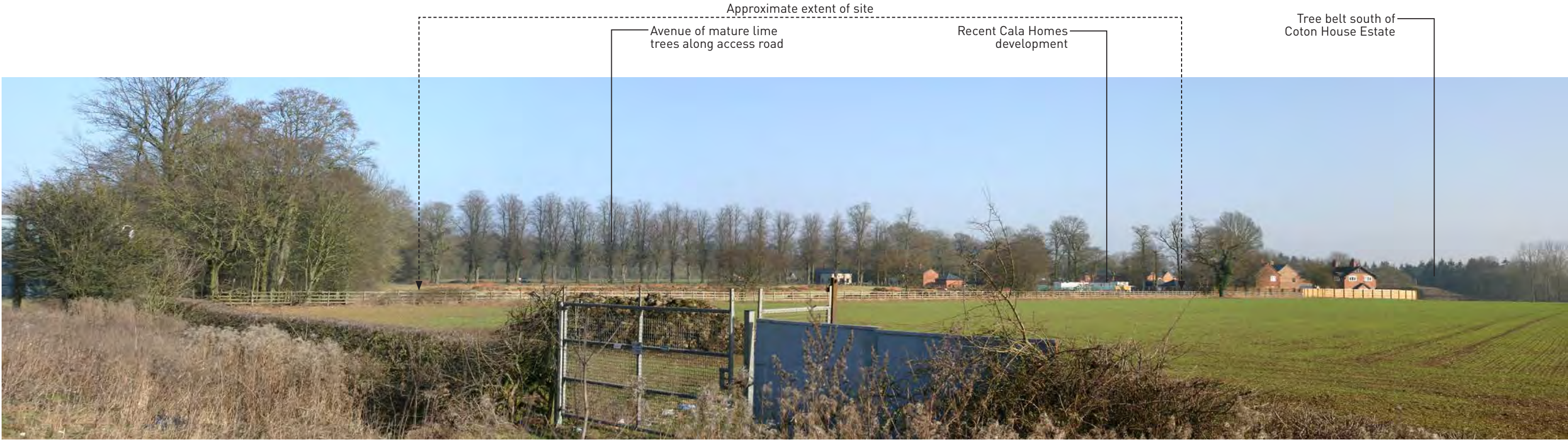
Viewpoint 5	View from public right of way (footpath) adjoining M6, looking north-west
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	452300 279100
Approx elevation	+118m AOD
Distance to site	c. 550m



Viewpoint 6	View from public right of way (footpath) adjoining M6, looking north-west
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451850 279150
Approx elevation	+127m AOD
Distance to site	c. 250m

Coton House and Estate,
Rugby
Cala Homes

Fig. 5 Viewpoint Photographs



Viewpoint 7	View from southbound layby on A426, looking north-east
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451450 279350
Approx elevation	+130m AOD
Distance to site	c.100m



Viewpoint 8	View from public right of way (footpath) north of M6, looking north
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451700 279375
Approx elevation	+130m AOD
Distance to site	On site boundary

Coton House and Estate,
Rugby
Cala Homes

Fig. 5 Viewpoint Photographs



Viewpoint 9	View from public right of way (footpath) adjacent James Arthur Drive, looking south
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451625 279600
Approx elevation	+132m AOD
Distance to site	Within site



Viewpoint 10	View from James Arthur Drive, looking west
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451650 279625
Approx elevation	+132m AOD
Distance to site	Within site

Coton House and Estate,
Rugby

Cala Homes

Fig. 5 Viewpoint Photographs



Viewpoint 11	View from A426 at entrance to Ashtree Farm, looking north-east
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451500 279450
Approx elevation	+132m AOD
Distance to site	c. 50m



Viewpoint 12	View fromCoton Road, looking south-east
Camera type	Canon EOS 300D with 18-55mm zoom lens set to 31-32mm focal length
Date	13th February 2017
Approx grid ref	451550 279650
Approx elevation	+130m AOD
Distance to site	c. 125m

Coton House and Estate,
Rugby
Cala Homes

Fig. 5 Viewpoint Photographs



Illustrative Landscape Masterplan
Scale 1:750@A0

LEGEND

- Site boundary
- Existing woodland to be retained
- Existing trees to be retained
- Existing trees to be removed
- Existing hedgerow to be retained
- Diverted Public Rights of Way
- Proposed Locally Equipped Area for Play (LEAP)
- Proposed infrastructure buffer to western site boundary
- Proposed trees - To Residential Green (refer to POS Landscape Character Areas to the top right corner of the sheet)
- Proposed trees - To plot frontages
- Proposed trees - To rear gardens
- Proposed trees - To Parkland Transition (refer to POS Landscape Character Areas to the top right corner of the sheet)
- Proposed feature trees
- Proposed native hedgerow planting to be managed to 1.5-2m high
- Proposed ornamental hedgerow planting
- Proposed ornamental shrub planting
- Proposed native shrub planting
- Proposed woodland edge planting to existing woodland
- Proposed wildflower/wetland meadow grassland
- Proposed grass area
- Proposed surfaced footpath
- Proposed mown footpath
- Proposed bulb planting
- Proposed sculptural mounding
- Focal/meeting point and seating area
- Proposed estate fencing

Note:
This plan is based on Malcolmpaynegroup's Proposed Site Plan (drawing ref: 189B108-01, Rev: P)



Public Open Space Landscape Character Areas
Scale 1:2000@A0

LEGEND

- Retained parkland
- Landscape Strategy:
 - Retain existing parkland setting.
- Parkland transition
- Landscape Strategy:
 - Retain and enhance existing parkland space with informal groups of parkland tree planting;
 - Integration of equipped natural play space adjacent to existing footpath network;
 - Inclusion of sustainable urban drainage systems at an appropriate location within the site;
 - Inclusion of biodiversity measures primarily through habitat creation and species selection with inclusion of other habitat where appropriate.

Planting Species

TREES	SPECIES	SUPPLY	SIZE(cm)	GIRTH(cm)
Acer campestre	Sid	250/300	8/10	
Betula pendula	Sid	250/300	8/10	
Tilia cordata	Sid	250/300	8/10	
Fagus sylvatica	Sid	250/300	8/10	
Quercus robur	Sid	250/300	8/10	
Quercus rubra	Sid	250/300	8/10	
Prunus avium	Sid	250/300	8/10	
Acer campestre	EHsid	350/425	14/16	
Betula pendula	EHsid	350/425	14/16	
Tilia cordata	EHsid	350/425	14/16	
Pinus sylvestris	EHsid	350/425	14/16	

SHRUB MIX - To play space:

SPECIES	SUPPLY	SIZE(cm)	HABIT	CTRS(cm)
Aucuba japonica 'Rozanne'	3L	40/60	Bushy	0.80
Buddleia davidii 'White Profusion'	3L	40/60	Branching	0.80
Cornus alba 'Sibirica'	3L	40/60	Branching	0.80
Cornus sanguinea 'Midwinter Fire'	3L	40/60	Branching	0.80
Corylus avellana	3L	40/60	Bushy	0.80
Prostris x fraseri 'Red Robin'	3L	40/60	Bushy	0.80

NATIVE SHRUB

SPECIES	MAX(N)	SUPPLY	SIZE(cm)	BREAKS	CTRS(cm)
Acer campestre	15	T 1+1	60/80	3	1.50
Corylus avellana	10	T 1+1	60/80	3	1.50
Crataegus monogyna	30	T 1+1	60/80	3	1.50
Ilex aquifolium	10	2L	40/60	3	1.50
Prunus spinosa	15	T 1+1	60/80	3	1.50
Rosa canina	5	T 1+1	60/80	3	1.50
Viburnum opulus	10	T 1+1	60/80	3	1.50

GENERAL PURPOSE MEADOW MIXTURE (EM1)

Wild flowers	(%)	Grasses	(%)
Achillea millefolium	Yarrow	Agrostis capillaris	Common Bent
Centaurea nigra	Common Knapsweed	Cirsium cristatum	Crested Dogtail
Daucus carota	Wild Carrot	Festuca rubra ssp.	Slender-creeping
Galium verum	Lady's Bedstraw	Phleum bertolonii	Red-festue
Leucanthemum vulgare	Daye Daisy		Smaller Cat's-tail
Plantago media	Hoary Plantain		
Prunella vulgaris	Sethrill		
Ranunculus acris	Meadow Buttercup		
Rumex acetosa	Common Sorrel		
Sanguisorba minor	Salad Burnet		
Stemella	Red campion		

BULBS

SPECIES	NO. p/m2
Crocus Dutch Collection (50% of area)	10
'Queen of the Blues'	10
'Golden Yellow'	10
'Jeanne D' Arc'	10
Dwarf Daffodils (50% of area)	10
'February Gold'	10
'Jet Fire'	10
'Candidatus'	10

Crocus and Daffodil bulbs to alternate every 2 linear metres.

- 1: Locally Equipped Area for Play (LEAP)
- 2: SW pumping station and FW treatment works

Retained woodland belt

- Landscape Strategy:
- Retain and manage of existing woodland belt.

Lime avenue

- Landscape Strategy:
- Retain existing lime avenue trees;
 - Retain mown grass between residential edge and access road;
 - Inclusion of estate railing and bound gravel paving to maintain and enhance parkland character of the site.

Residential green

- Landscape Strategy:
- Surfaced access for pedestrians with grass verge on both sides;
 - Formal trees to be planted along the road to define the green space;
 - Inclusion of bulb planting under trees;
 - Amenity grass with opportunity of mown footpath.

Planting Species

TREES

SPECIES	SUPPLY	SIZE(cm)	GIRTH(cm)
Acer campestre	EHsid	450/650	18/20
Liquidambar styraciflua	EHsid	450/650	18/20
Pyrus calleryana 'Chanticleer'	EHsid	450/650	18/20
Prunus 'Sunset Boulevard'	EHsid	450/650	18/20

HEDGEROWS

SPECIES	SUPPLY	SIZE(cm)	HABIT	CTRS(cm)
Buxus sempervirens	10L	40/60	Bushy	0.35

Woodland infrastructure belt

- Landscape Strategy:
- Retention and enhancement of existing landscape features;
 - 10 metres woodland belt planting with native scrub understorey;
 - Screening value for proposed development.

Planting Species

TREES (20%)

SPECIES	SUPPLY	SIZE(cm)	MIX (%)	CTRS(cm)
Acer campestre	T1+1	40/60	4	2.00
Betula pendula	T1+1	40/60	4	2.00
Fagus sylvatica	T1+1	40/60	4	2.00
Quercus robur	T1+1	40/60	4	2.00
Tilia cordata	T1+1	40/60	4	2.00

SHRUBS (20%)

SPECIES	SUPPLY	SIZE(cm)	MIX (%)	CTRS(cm)
Corylus avellana	T1u1	40/60	43	2.00
Crataegus monogyna	T1+1	40/60	17	2.00
Ilex aquifolium	2L	40/60	8	2.00
Prunus spinosa	T1+1	40/60	5	2.00
Cornus sanguinea	T1+1	40/60	3	2.00
Eurostyum europaeus	T1+1	40/60	2	2.00
Taxus baccata	T1+1	40/60	2	2.00

Amenity Landscape

- Landscape Strategy:
- Linear open spaces for public access and building setting;
 - Hedgerow alignments for ecological mitigation;
 - Woodland edge planting.

Planting Species

NATIVE Hedgerow

SPECIES	MAX(N)	SUPPLY	SIZE(cm)	CTRS(cm)
Carpinus betulus	10	T 1+1	40/60	0.3
Crataegus monogyna	60	2x1hd	125/150	0.3
Fagus sylvatica	10	T 1+1	40/60	0.3
Prunus spinosa	10	T 1+1	40/60	0.3
Ilex aquifolium	10	2L	40/60	0.3

WOODLAND EDGE PLANTING

SPECIES	MAX(N)	SUPPLY	SIZE(cm)	CTRS(cm)
Corylus avellana	T1u1	40/60	43	2.00
Crataegus monogyna	T1+1	40/60	17	2.00
Ilex aquifolium	2L	40/60	8	2.00
Prunus spinosa	T1+1	40/60	5	2.00
Cornus sanguinea	T1+1	40/60	3	2.00
Eurostyum europaeus	T1+1	40/60	2	2.00
Taxus baccata	T1+1	40/60	2	2.00

Revisions:

Land at Coton House
Estate, Churchover, Rugby

Fig. 7: Illustrative
Landscape Masterplan

Drawing Ref: BIR.4821_070

Client: Cala Homes

Date: 13/12/2016

Drawn by: WM

Checked by: JP

Scale: as shown@ A0

Pegasus
Design

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton
House (Policies DS3 and DS6)

**Appendix F- Site Layout Plan and Decision Details for new Motorway Service Area at
New Ash Tree Farm.**



Site Layout Details

Planning Application Decision Details:

APPLICATION NUMBER

R17/0011

DATE VALID

05/01/2017

ADDRESS OF DEVELOPMENT

NEW ASH TREE FARM
LEICESTER ROAD
CHURCHOVER
RUGBY
CV23 0EZ

APPLICANT/AGENT

Mr Tony Collins
Collins & Coward Ltd
The Courtyard
9a East Street
Coggeshall
CO6 1SH
On behalf of Mr M Franks, Moto Hospitality Limited

APPLICATION DESCRIPTION

Demolition of existing farm building. Construction of new Motorway Service Area ("MSA") to comprise: Amenity Building, Lodge (use class C1), Drive Thru Coffee Unit, associated car, coach, motorcycle, caravan, HGV and abnormal load parking and a Fuel Filling Station with retail shop, together with alterations to the adjacent roundabout on the M6 to form an access point and works to the local highway network. Provision of landscaping, signage, infrastructure and ancillary works including diversion of bridleway.

CONDITIONS, REASONS & RELEVANT DEVELOPMENT PLAN POLICIES

CONDITION: 1

The development to which this permission relates must not be begun later than the expiration of three years from the date of this permission.

REASON:

To comply with Section 51 of the Planning and Compulsory Purchase Act 2004.

CONDITION: 2

Unless otherwise agreed in writing by the Local Planning Authority the development shall be carried out in accordance with the plans and documents detailed below:

Amended Plans/Documents:

- Proposed Incoming Water and Fire Hydrant Plan - Drawing No. 8299-PE-ZO-XX-DR-ME-0802 Rev P01 - Received on the 13/03/2017
- Water Supplies for Fire Fighting Vehicles - ref: JTH/NJF/160135/17-4/ N104 Issue CP05-24 Rev B - received on the 13/03/2017
- Amenity Building – Elevations – Drawing No. 8299/PL030 Rev. C
- Proposed Biomass Boiler Flue Drawing No. 8299-PE-ZO-XX-DR-M-0803 Rev P01 dated 24.03.17
- Lodge Elevations - Drawing No. 8299/PL046 Rev B
- Fuel Filling Stations Elevations – 8299/PL066 Rev C
- Ancillary Buildings - Biomass and Energy Centre – 8299/PL070 Rev C dated 29/03/17
- Foul Drainage Route to Cosford Drawing. No. 4620-SK-013 Rev A - received on the 24/05/2017
- Foul drainage strategy for Moto Rugby (Motorway Service Area) - REVISION B dated 18th May 2017
- Amended Low and Zero Carbon Technologies Design Statement issue 02
- Supplementary Air Quality Note dated 19th April 2017
- Ancient Monument Scope of Works received on the 18/07/2017
- Proposed Signage- Drawing PL-015 Rev. B received 06/09/2017
- Landscape Masterplan Drawing No. 21605/01 Rev G dated 28.3.2017

- Boundary Treatment 21605/14 Rev C
- Proposed Site Plan Drawing No. 21605/03 Rev H
- Proposed External Lighting Layout Drawing No. 8299-PE-Z0-XX-DR-E-0102 Rev P09 dated 24.03.17

Original Plans/Document

Existing Site Plan - Drawing No. PL-002 Rev A
 Existing Building to be demolished – Drawing No. PL-005 Rev A
 Section Drawing No. 21605/08 Rev B

Amenity Building - Ground Floor Plan - Drawing No. PL-020 Rev A
 Amenity Building - First Floor Plan – Drawing No. PL-021 Rev A
 Amenity Building - Roof Plan - Drawing No. PL-022 Rev B
 Extent of Retail Area Drawing No. PL-099 Rev A
 Amenity and Lodge LPG Compounds Drawing No. PL-072 Rev A
 Chiller, Water tank and Substation Drawing No. PL-071 Rev A
 Plaza Detail Drawing No. 21605/06 Rev B

Lodge - Ground Floor Plan - Drawing No. PL-040 Rev A
 Lodge - First, Second Floor and Roof Plan - Drawing No. PL-041 Rev A

Costa - Ground Floor, Roof Plan and Sections Drawing No. PL-050 Rev A
 Costa – Elevations Drawing No. PL-055 Rev B

Fuel Filling Station - Ground Floor Plan Drawing No. PL-060 Rev A
 Fuel Filling Station - Roof Plan Drawing No. PL-061 Rev A

Planting Strategy Drawing No.21605/011 Rev C

SuDs Schematic Drawing No. 21605/010 Rev D
 Storm Drainage Drawing No. 4620-SK-004 Rev P4

Outline of access arrangements Drawing No. M326/31 Rev A
 Preliminary horizontal geometry Drawing No. M326/22 Rev A
 Preliminary longitudinal sections Drawing No. M326/23 Rev A
 Preliminary Proposed Road Markings Drawing No. M326/40
 Vehicle Tracking Cars Drawing No. 4620-SK-007 P5
 Vehicle Tracking Caravans Drawing No. 4620-SK-008 P5
 Vehicle Tracking Coaches Drawing No. 4620-SK-009 P4
 Vehicle Tracking HGV Drawing No. 4620-SK-010 P5
 Vehicle Tracking Abnormal Load Drawing No. 4620-SK-011 P4
 Vehicle Tracking Fire Engine Drawing No. 4620-SK-012 P3
 Proposed Incoming Services Plan Drawing No. 8299-PE-ZZ-00-DR-ME-0801
 Parking numbers Drawing No. 21605/02 Rev F

Travel Plan ref: RD05

Design and Access Statement RD11 December 2016

Environmental Impact Assessment Vol. I, II and III (except for documents/parts and plans superseded by amended plans/documents set out above)

REASON:

For the avoidance of doubt and to ensure that the details of the development are acceptable to the Local Planning Authority.

CONDITION: 3

The development hereby permitted shall not exceed the total quantum of development as detailed below:

Amenity building – 3,959sqm (gross internal floorspace) containing

- Retail units of which no individual retail unit contained within the amenity building shall not exceed 359sqm (gross internal floorspace)
- Administration and staff facilities at first floor level shall not exceed 598sqm

Up to a 100 bed hotel

Drive-thru coffee unit (205sqm gross internal area)

Fuel Filling Station (261sqm gross internal area in respect to a kiosk unit)

REASON

To ensure the development does not adversely impact on the vitality and viability of Rugby Town Centre.

CONDITION: 4

The landscaping scheme, as detailed on the approved plans, shall be implemented no later than the first planting season following first occupation of the development. If within a period of 5 years from the date of planting, any tree/shrub/hedgerow is removed, uprooted, destroyed or dies, (or becomes in the opinion of the LPA seriously damaged or defective), another tree/shrub/hedgerow of the same species and size originally planted shall be planted at the same place, unless the LPA gives its written consent to any variations.

REASON:

To ensure the proper development of the site and in the interest of visual amenity.

CONDITION: 5

Notwithstanding any indication on the approved plans, full details of the proposed landscaping to enclose the proposed diverted public right of way, shall be submitted to and approved by the LPA prior to commencement of any development. The approved landscaping shall be implemented no later than the first planting season following first occupation of the development. If within a period of 5 years from the date of planting, any tree/shrub/hedgerow is removed, uprooted, destroyed or dies, (or becomes in the opinion of the LPA seriously damaged or defective), another tree/shrub/hedgerow of the same species and size originally planted shall be planted at the same place, unless the LPA gives its written consent to any variations.

REASON:

To ensure the proper development of the site and in the interest of visual amenity.

CONDITION: 6

No works or development shall take place until a final arboricultural method statement/tree protection plan for the protection of the retained trees (section 5.5 & 6.1, BS5837:2012) has been agreed in writing with the LPA. This scheme must include details and positioning of tree protection fencing, ground protection measures, root pruning/access facilitation pruning specification, project phasing and an auditable monitoring schedule.

REASON:

To ensure all retained trees are not damaged during the development phase and are successfully integrated into the scheme

CONDITION: 7

No part of the development shall be occupied until a specification of all proposed tree planting shall be submitted to and approved in writing by the LPA. This specification will include details of the quantity, size, species, position and the proposed time of planting of all trees to be planted, together with an indication of how they integrate with the proposal in the long term with regard to their mature size and anticipated routine maintenance. In addition all shrubs and hedges to be planted that are intended to achieve a significant size and presence in the landscape should be similarly specified.

REASON:

To maintain enhance continuity of tree cover within the site for the purposes of screening, amenity value and biodiversity enhancement.

CONDITION: 8

No construction will be undertaken until a Construction Management Plan, which must contain a Construction Phasing Plan, details to prevent mud, debris and obstacles on the Highway and HGV Routing Plan, is submitted and approved by both the Planning and Highway Authorities.

REASON

In the interest of highway safety

CONDITION: 9

No part of the development hereby permitted shall be occupied until the detailed design of the site access arrangements (as indicatively shown on Drawing M326/22), and have first been submitted to and approved in writing by the District Planning Authority. Thereafter the approved highway access works shall be implemented in accordance with the approved plans and permanently retained thereafter.

REASON

In the interest of highway safety

CONDITION: 10

No part of the site shall be occupied on any part of the site until the approved highway access works have been completed, as evidenced by the issuing of a Certificate of Substantial Completion by the Highway Authority.

Minor alterations maybe required during the detailed design process.

REASON

In the interest of highway safety

CONDITION: 11

No part of the development hereby permitted shall be occupied until the detailed design of the M6 Junction 1 Improvement Scheme (as indicatively shown on Drawing M326/31), and have first been submitted to and approved in writing by the District Planning Authority and address the following matters;

- a. Provision of an engineering layout demonstrating the geometry of the junction layout and lane widths;
- b. Identification of the detection system and cabling routing through the junction;
- c. Identification of the method of control and back-up system for the operation of M6 Junction 1;
- d. Identification of the location for a maintenance vehicle bay near the traffic signal controllers;
- e. Identification of the locations for two PTZ CCTV cameras for traffic management of M6 Junction 1;
- f. Identification of the locations for street lighting of M6 Junction 1;
- g. Provision of Stage 2 Road Safety Audits based on the detailed drawings.

Thereafter the approved highway access works shall be implemented in accordance with the approved plans and permanently retained thereafter.

No part of the site shall be occupied on any part of the site until the approved highway access works have been completed, as evidenced by the issuing of a Certificate of Substantial Completion by the Highway Authority.

Minor alterations maybe required during the detailed design process.

REASON

In the interest of highway safety

CONDITION: 12

No part of the development hereby permitted shall be occupied until the detailed design of lane markings and location of signage for the M6 Junction 1 Improvement Scheme (as indicatively shown on Drawing M326/40), and have first been submitted to and approved in writing by the District Planning Authority.

REASON

In the interest of highway safety

CONDITION: 13

Prior to making any submission to the local planning authority in respect of the discharge of any of the conditions herein, the developer shall first submit a written schedule, setting out in respect of the relevant condition, those matters (if any) that relate to the operation of the Strategic Road Network and shall provide written evidence to the Local Planning Authority of Highways England's approval of such schedule.

REASON

In the interest of certainty of the implications of development for the safe operation of the Strategic Road Network.

CONDITION: 14

No development shall commence until a scheme of works as shown indicatively in the latest drawing numbers, or as amended by Detailed Design and Road Safety Audit, is implemented. This scheme of works shall include (but not be limited to) the following items:

- M326/31 Revision A Outline of Access Arrangements
- M326/22 Revision Preliminary Horizontal Geometry
- M326/23 Revision A Preliminary Longitudinal Section
- M326/38 Northbound Slip Road and Merge Arrangements
- M326/39 Southbound Slip Road and Merge Arrangements
- M326/40 Preliminary Proposals for Road Markings
- M326/42 Diagrammatic Layout of Signs

a) Boundary of the works site

b) Scope of works – including but not limited to the following – alteration of the M6 overbridge, new local highway including new access roads and supporting infrastructure, revised highway drainage system.

c) Detailed Design approvals of the revised and new road infrastructure, and highway structures to be obtained from Highways England and Warwickshire County Council.

d) Detailed Signage Strategy and associated designs approved by Highways England and Warwickshire County Council.

REASON

To ensure the safe and free flow of traffic on the Strategic Road Network and in the interest of certainty and to enable the development to proceed.

CONDITION: 15

No on-site construction of the development hereby permitted shall be carried out prior to the building of a suitable access and shall be in accordance with the details shown on the submitted plans referred to in Condition 14 or as amended by Detail Design and Road Safety Audit.

REASON

To ensure the safe and free flow of traffic on the Strategic Road Network.

CONDITION: 16

No development shall commence until a Construction Environmental Management Plan for the development has been submitted to, and approved in writing by, the local planning authority and written confirmation of approval has been received from Highways England and provided to the local planning authority. The plan shall include details of:

- a. the hours of construction work and deliveries;
- b. area(s) for the parking of vehicles of site operatives and visitors;
- c. area(s) for the loading and unloading of plant and materials;
- d. storage of plant and materials used in constructing the development;

- e. wheel washing facilities;
- f. the responsible person (e.g. site manager / office) who could be contacted in the event of complaint;
- g. mitigation measures in respect of noise and disturbance during the construction phase including vibration and noise limits, monitoring methodology, screening, a detailed specification of plant and equipment to be used and construction traffic routes;
- h. a scheme to minimise dust emissions arising from demolition / construction activities on the site. The scheme shall include details of all dust suppression measures and the methods to monitor emissions of dust arising from the development;
- i. waste management.
- j. routing of construction traffic during the phases of development.
- k. drainage during the construction phase shall identify how surface water run-off will be dealt with so as not to increase the risk of flooding to downstream areas as a result of the construction programme.
- l. protection measures for hedgerows and grasslands. There shall be no burning of materials on site during construction.
- m. Risk Assessments and Method Statements for the works.

REASON:

Development shall take place in accordance with the approved Construction Environmental Management Plan.

CONDITION: 17

No development shall commence until details of connections to utility services and supporting infrastructure, including details of construction and maintenance of the supply infrastructure are submitted for approval by the local planning authority in consultation with Highways England.

REASON:

To comply with paragraph 49 of DfT Circular 02/2013 The Strategic Road Network and Sustainable Development ('The Circular').

CONDITION: 18

No part of the development hereby approved shall be used by the public until all parking spaces, internal access roads, turning and manoeuvring areas, and footpaths have been constructed and laid out in accordance with relevant plans listed in Condition 2 and 3.

REASON:

In the interest of highway safety.

CONDITION: 19

No part of the development hereby approved shall be used by the public until a Management and Maintenance Plan for all roads within the site, including verges has been submitted to the local planning authority and written confirmation of approval of such details has been received from Highways England and provided to the local planning authority. The Management and Maintenance Plans shall incorporate a drawing setting out the maintenance boundaries between the Motorway Service Area and the M6 motorway. Such details shall thereafter be adhered to.

REASON:

In the interest of road safety and to safeguard the character and appearance of the area.

CONDITION: 13

Prior to the development being brought into use the applicant shall undertake the external work along the A426/Highway Boundary as detailed within Ancient Monument Scope of Works, unless otherwise agreed in writing by the LPA.

REASON

To enhance the setting and legibility of this heritage asset.

CONDITION: 14

No development shall take place until the applicant, or their agent or successors in title, has secured the implementation of a programme of archaeological work in accordance with a written scheme of investigation which has been submitted by the applicant and approved in writing by the Local Planning Authority.

REASON:

To ensure the preservation of important archaeological remains and that any archaeological history of the site is recorded.

CONDITION: 15

The proposed Hotel shall not be occupied until a noise survey has been completed and details to be submitted and approved by the LPA. These details shall seek to achieve the internal noise levels as stipulated within the table below.

Activity	Location	07:00 to 23:00	23:00 to 07:00
Resting	Living room	35dBLAeq,16hour	-
Dining	Dining room / area	40dBLAeq,16hour	-
Sleeping	Bedroom	35dBLAeq,16hour	30dBLAeq,8hour

Any mitigation measures identified shall be implemented and retained thereafter in accordance with the approved details.

REASON

In the interest of amenities of users of the hotel

CONDITION: 16

With the exception of works on the highway, the hours of work during the construction phase of the development hereby approved and any traffic movements into and out of the site associated with the construction or maintenance of the authorised development shall be 08:00 to 1800 hours on Monday to Friday and 08:00 to 13:00 hours on Saturday. No work shall take place outside these times, or on public holidays, unless otherwise agreed by the LPA. Outside of these hours, work at the site shall be limited to foundation concrete pouring, site security, testing of plant and equipment and emergency works including any works to prevent or remedy environmental pollution or health and safety risks (provided that the developer retrospectively notifies the LPA of any emergency works within 24 hours).

REASON

In the interest of residential amenities

CONDITION: 17

Any fixed plant(s) to be installed shall not be operated until a scheme to control noise emitted from it has been approved in writing by the LPA and installed as approved. The scheme shall limit noise from fixed plant(s) to a rating level of as least 5dBA below existing background noise level from existing receptors as set out within Table 12.13 with measurements and assessment made in accordance with BD4142:2014. The approved scheme shall be retained thereafter in accordance with the approved details.

REASON

To protect the amenities of nearby residential properties.

CONDITION: 18

Prior to installation of the biomass boiler full manufacturing details of the boiler appliance shall be submitted to and approved by the LPA. Any mitigation measures shall be fully implemented prior to it being brought into use and thereafter retained in accordance with the approved details.

REASON

In the interest of residential amenities

CONDITION: 19

Prior to the installation of on-site back-up generators, full details shall be submitted and approved by the LPA detailing their predicted associated emission concentrations in respect to receptors in proximity likely located at the Amenity Building or where relevant. Any mitigation measures shall be fully implemented prior to it being brought into use and thereafter retained in accordance with the approved details.

REASON

In the interest of residential amenities

CONDITION: 20

The development hereby permitted shall not commence until an Air Quality Defra Cost Assessment shall be undertaken and submitted to and approved in writing by the LPA. Any mitigation measures shall be fully implemented prior to the development being brought into use and thereafter retained in accordance with the approved details.

REASON

In the interest of residential amenities

CONDITION: 21

Prior to the first occupation of the development, details of Electric Vehicle Charging Points (active and passive) to be provided shall be submitted to and approved in writing by the LPA. The works shall be carried out in accordance with the approved details prior to first occupation of the development and thereafter retained in accordance with the approved details.

REASON

In the interest of promoting sustainable travel opportunities and to form part of air quality mitigation measures.

CONDITION: 22

If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, an amendment to the remediation strategy detailing how this unsuspected contamination shall be dealt with.

A validation/verification report shall be submitted to and approved in writing by the Local Planning Authority to demonstrate that successful remediation has been carried out.

REASON:

To ensure the protection of controlled waters.

CONDITION: 23

Full detail of any Fuel tanks/tank farm shall be submitted to and approved in writing by the LPA prior to its installation. The fuel tanks/tank farm shall be installed in accordance with the approved details.

REASON

To prevent contamination to ground and controlled waters.

CONDITION: 25

Prior to the demolition of any existing buildings a full Asbestos Survey shall be submitted to and approved in writing by the Local Planning Authority. Development shall not be carried out other than in accordance with the approved details.

REASON:

In the interest of health and safety.

CONDITION: 26

The use shall not be brought into operation until a grease trap has been provided on the drainage outlet(s) from the food preparation area(s). The grease trap shall be retained at all times thereafter.

REASON

To ensure the removal of grease from waste effluent in the interest of general amenity

CONDITION: 27

No part of the development hereby permitted, including site clearance, shall be commenced until an updated badger survey, including timetabled mitigation measures where appropriate, has been carried out by a suitably qualified badger consultant and has been submitted to and approved in writing by the Local Planning Authority. This should show an appropriate buffer zone around the vicinity of the identified sett during construction and details of whether a development licence is required, including the location of a badger tunnel under the proposed access roads to the services and appropriate fencing within the design of the proposed development. The approved mitigation plan shall thereafter be implemented in full.

REASON:

To ensure appropriate measures are taken in relation to protected species.

CONDITION: 28

No development shall commence unless and until a scheme ("the scheme") to ensure that there is no net biodiversity loss as a result of the development has been submitted to and agreed in writing by the Local Planning Authority. The net biodiversity impact of the development shall be measured in accordance with the DEFRA biodiversity offsetting metric as applied in the area in which the site is situated at the relevant time and the scheme shall include:

1. Proposals for on-site mitigation and/or for off-site offsetting;
2. A methodology for the identification of any receptor site(s) for offsetting measures;
3. The identification of any such receptor site(s);
4. The provision of arrangements to secure the delivery of any offsetting measures (including a timetable for their delivery); and
5. A management and monitoring plan (to include for the provision and maintenance of any offsetting measures in perpetuity).

The written approval of the Local Planning Authority shall not be issued before the arrangements necessary to secure the delivery of any offsetting measures have been executed. The scheme shall be implemented in full accordance with the requirements of the scheme or any variation so approved.

REASON

To ensure a no net loss of biodiversity

CONDITION: 29

No development shall take place until a detailed surface water drainage scheme for the site, based on sustainable drainage principles and an assessment of the hydrological and hydrogeological context of the development, has been submitted to and approved in writing by the Local Planning Authority in consultation with the LLFA. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed. The scheme to be submitted shall:

- Limit the discharge rate generated by all rainfall events up to and including the 100 year plus 40% (allowance for climate change) critical rain storm to 26l/s for the site or a revised estimate of the QBar rate.
- Provide provision of surface water attenuation storage as stated within the submitted plans (drawing number: 21605/10) or in accordance with 'Science Report SC030219 Rainfall Management for Developments'.
- Surface water is to be provided via a minimum of two trains of treatment using the proposed above ground drainage features within the drainage design.
- Undertaken infiltration testing in accordance with the BRE 365 guidance to clarify whether or not an infiltration type drainage strategy is an appropriate means of managing the surface water runoff from the site.
- Demonstrate that the surface water drainage system(s) are designed in accordance with 'The SuDS Manual', CIRIA Report C753.
- Demonstrate detailed design (plans, network details and calculations) in support of any surface water drainage scheme, including details of any attenuation system, and outfall arrangements. Calculations should demonstrate the performance of the designed system for a range of return periods and storm durations inclusive of the 1 in 1 year, 1 in 2 year, 1 in 30 year, 1 in 100 year and 1 in 100 year plus climate change return periods.
- Provide a maintenance plan to the LPA giving details on how the entire surface water systems shall be maintained and managed after completion for the life time of the development. The name of the party responsible, including contact name and details shall be provided to the LPA.

REASON

To prevent the increased risk of flooding; to improve and protect water quality; to improve habitat and amenity; and to ensure the future maintenance of the sustainable drainage structures.

CONDITION: 30

No part of the development shall be occupied unless and until full details of the proposed public art feature within the main plaza area in front of the amenity building has been submitted to and approved in writing by the LPA. The art work will be inspired by aspects of Rugby's history. Themes could include the following, but not exclusively:

- Characters from Alice's Adventures in Wonderland, recognising the author Lewis Carroll's connection to Rugby.
- The Soldier (poem); Rupert Brooke, war time poet born in Rugby
- The Game of Rugby; William Webb Ellis
- Rugby's engineering history (e.g. Frank Whittle, jet engine design at the
- Former British Thomson-Houston works)
- Other art work as agreed with the Council

Such details shall include scale plans of the public art, the artist selected to deliver the art work, and the selection proceeds undertaken to select the chosen artist. A plaque recognising the artist and subject matter will be positioned adjacent to the work.

REASON

In the interest of visual amenity

CONDITION: 31

Prior to commencement of development a detailed lighting scheme shall be submitted to and approved by the LPA. Such details shall address all of the outstanding issues outlined within the documents entitled Planning Application Lighting Assessment prepared by IT DOES LIGHTING LTD for Rugby Borough Council dated 22nd September 2017. Such details shall also include permanent and temporary external lighting of the site and new junction with the M6. The approved lighting shall be implemented, prior to occupation, and retained in accordance with the details approved.

REASON

In the interest of amenity

STATEMENT OF POSITIVE ENGAGEMENT:

In dealing with this application Rugby Borough Council has actively sought to work with the applicant in a positive and proactive manner, in accordance with paragraphs 186 and 187 of the NPPF.

ENVIRONMENTAL STATEMENT:

The information contained within the Environmental Statement submitted under the Town & Country Planning (Environmental Impact Assessment) Regulations 2011 (as amended) as part of this planning application has been taken into account in the assessment, consideration and determination of the application by the Local Planning Authority.

INFORMATIVE: 1

Severn Trent Water advise that although our statutory sewer records do not show any public sewers within the area you have specified, there may be sewers that have been recently adopted under The Transfer Of Sewer Regulations 2011. Public sewers have statutory protection and may not be built close to, directly over or be diverted without consent and you are advised to contact Severn Trent Water to discuss your proposals. Severn Trent will seek to assist you obtaining a solution which protects both the public sewer and the building. Should you require any further information please contact us on the telephone number or email below.

INFORMATIVE: 2

Public bridleway RB30 must remain open and available for public use at all times unless closed by legal order, so must not be obstructed by parked vehicles or by materials during construction.

The applicant must make good any damage to the surface of public bridleway caused during construction.

No construction may begin on the existing recorded alignment of public bridleway RB30 unless and until it has been diverted by a confirmed legal order that has come fully into effect.

If it is necessary to temporarily close public bridleway RB30 during construction then a Traffic Regulation Order will be required. Warwickshire County Council's Rights of Way team should be contacted well in advance to arrange this.

Any disturbance or alteration to the surface of public bridleway RB30 requires the prior authorisation of Warwickshire County Council's Rights of Way team, as does the installation of any new gate or other structure on the public bridleway.

INFORMATIVE: 3

The applicant is advised that advertisement consent is required for the proposed advertisements.

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton
House (Policies DS3 and DS6)

Appendix G- Coton House Historic England Listing Details.

COTON HOUSE

List Entry Summary

This building is listed under the Planning (Listed Buildings and Conservation Areas) Act 1990 as amended for its special architectural or historic interest.

Name: COTON HOUSE

List entry Number: 1276617

List entry Description

Details:

CHURCHOVER SP57NW 8/22 Coton House 04/12/51 (Formerly listed as Coton House and Stable buildings) - II* House. Late C18. Possibly by Samuel Wyatt. Sandstone ashlar with slate and lead roof. 2 storeys; 5-window range, the central 3-window range bowed from ground to roof. Part-glazed French doors within surround with stone pilasters and entablature. To either side of door a 15-pane sash. To left and right a tripartite sash with unfluted Ionic columns with responds to architraves. Blank segmental arches above. 5 twelve-pane sashes to first floor. The house rests upon a stone plinth, and has blank recessed panels above the door and its 2 flanking sashes. First floor sill band, and moulded stone eaves cornice. Interior: large central staircase, beginning in one flight and returning in 2, with iron balustrade. Alabaster fireplaces, decorated with garlands and friezes. (Buildings of England: Warwickshire p. 232)

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Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

Appendix H- Design and Access Statement for Land at Coton House Estate

DESIGN & ACCESS STATEMENT

LAND AT COTON HOUSE ESTATE, RUGBY



FOR CALA HOMES (MIDLANDS) LTD

MAY 2017 | 189B108 | D&A REV D

OBJECTIVE

To extend the estate village that is being created around Coton House and to provide a wide range of family homes integrated into the parkland and harmonising with the heritage assets and natural environment.

DESIGN & ACCESS STATEMENT

To explain the development principles and concepts that have been applied to the proposed development;

To demonstrate the steps taken to appraise the context of the proposed development and how the design takes that context into account;

To explain the approach to access and how relevant Local Plan policies have been taken into account in the design of the scheme.

Malcolm Payne Group Limited

Architecture | Design | Conservation

174 Holliday Street

Birmingham B1 1TJ

Telephone: 0121 643 3159

www.malcolmpaynegroup.co.uk

RIBA 

Chartered Practice

DESIGN & ACCESS STATEMENT

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INTRODUCTION

SECTION 1

INTRODUCTION

TERMS OF REFERENCE

AUTHORSHIP & PURPOSE

1.01 This Design and Access Statement has been prepared by Malcolm Payne Group Ltd to accompany a planning application submitted by Framptons Town Planning Consultants on behalf of CALA Homes (Midlands) Ltd.

CALA Homes is the housebuilder currently developing new homes in the environs of Coton House. Coton House Rugby Ltd is the landowner of the application site. The residential development proposed in this application is seen as an integral part of an evolving master-plan for the Coton Park Estate and the objective is to achieve the same quality of dwellings and landscape as in the previously consented development.

APPLICATION DETAILS

1.02 The application is for the development of land situated within the Coton Park Estate, Lutterworth Road, Rugby. The site lies between the A426 and the development under construction at Coton Park.

The application is for full planning permission for erection of 100 new homes, provision of a roundabout on the A426, and associated development.

DRAWINGS & DOCUMENTS

1.03 The application drawings, including scheme layout, house types and street-scenes prepared by Malcolm Payne Group Ltd, the land survey prepared by Geoffrey Perry Associates Ltd and the illustrative landscape masterplan prepared by Pegasus Design are set out in the schedule appended to the application.

This Design and Access Statement is arranged in five main sections:

CONTEXT: setting out the contextual issues that provide a framework for development and the resultant opportunities and constraints;

DESIGN PRINCIPLES: describing the evolution of the scheme and setting out the approach to density, mix, scale and architectural treatment.

ACCESS & PARKING: summarising the approach to highway improvement and design, and the strategy for pedestrian and cycle movement, and car parking.

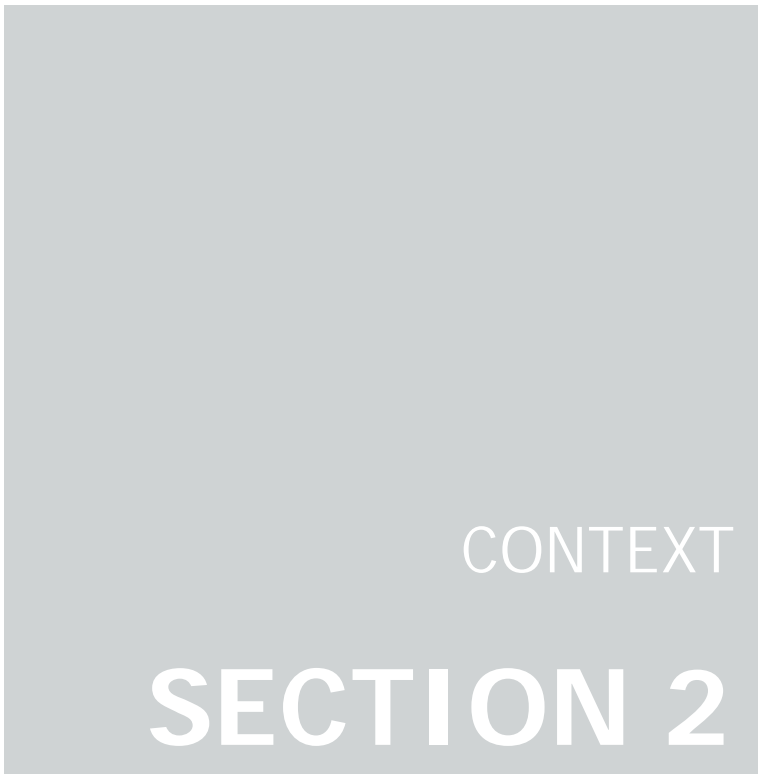
EVALUATION: assessing the effectiveness of the scheme and the impact the proposed development would have on Coton Park and its surroundings.

CONCLUSION: providing an overview of the application proposals.

REFERENCES

1.04 The application proposals have been informed by the following specialist reports to which reference should be made where noted in this document:

- **PLANNING:** Planning Statement prepared by Framptons Town Planning Consultants;
- **LANDSCAPE:** Landscape & Visual Impact Assessment prepared by Pegasus Planning Group;
- **HERITAGE:** Heritage Assessment prepared by Heritage Collective;
- **ARCHAEOLOGY:** Archaeological Assessment prepared by Cotswold Archaeology;
- **TREES:** Tree Survey & Arboriculture Report prepared by Ruskins;
- **TRAFFIC & HIGHWAYS:** Transport Assessment & Highway Design prepared by David Tucker Associates;
- **ACOUSTICS:** Noise Report & Recommendations prepared by Cundall;
- **ECOLOGY:** Ecological Appraisal prepared by FPCR;
- **DRAINAGE:** Flood Risk & Drainage Report prepared by Woods Hardwick;
- **GROUNDWORKS:** Site Investigation Report prepared by Brownfield Consultancy.



CONTEXT

LOCATION & AMENITIES

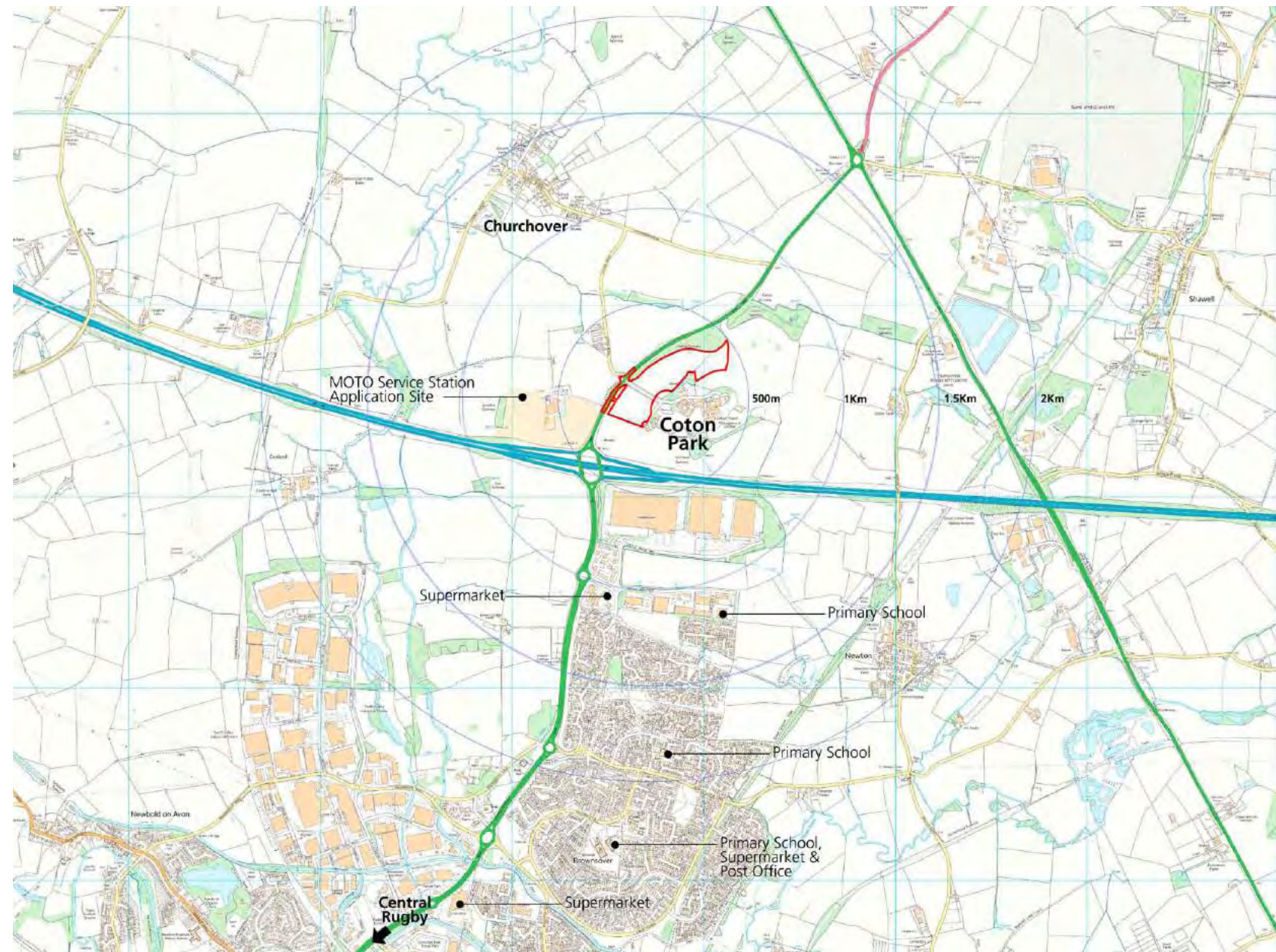
ACCESS & LOCAL SETTLEMENTS

2.01 The Coton Park Estate sits in a triangle of open countryside bounded by the A5 and A426 roads and the M6 Motorway. The estate is situated to the east of the village of Churchover and approximately 5.6 kilometers (3.5 miles) to the north of Rugby town centre, which lies beyond the intervening M6 Motorway.

The estate has two vehicular entrances from the A426. The southern entrance, close to motorway Junction 1, is the primary access to the new residential development under construction at Coton Park. The northern entrance has been restored as the access to Coton House and the retained farmland and woodland. Section 5 of this statement explains how the access would be improved and an additional access provided for pedestrians and cyclists.

The proximity to Churchover, Rugby and the motorway has advantages in terms of access and amenities. Churchover has a village hall and a public house. Rugby town centre has the full range of retail, community and leisure facilities. In addition, substantial edge of town shopping lies between Rugby and the motorway, including a major supermarket some 3.37 km (2.1 miles) from the site.

The scale of development proposed in the application would not support the on-site provision of a school or shops but the proximity to existing facilities ensures that it would be well served by the existing amenities.



CONTEXT

SURROUNDING AREA

THE VILLAGE OF CHURCHOVER

2.02 The village with which the estate is associated is Churchover, an historic settlement lying to the east of the A426 and separated from Coton House and Park by open farmland. The village was essentially a small farming community that retains its character despite the proximity of the motorway to the south and the A5 and A426 roads to the west.

The settlement comprises two roads, School Street and Church Street with a small green at the junction. The houses are mainly related to the streets with little backland development and few subsidiary roads. A loose arrangement of houses and farm buildings is interspersed with well planted gardens and small fields which bring the surrounding landscape into the village. This random spread of houses and barns, particularly towards the edge of the settlement, is interspersed with terraces of cottages situated at back of pavement along both streets that provide a sense of enclosure that contrasts with the openness elsewhere. Holy Trinity Church in the northeast is the only building of major scale and sits in a large churchyard and burial ground.

The grain and appearance of the village is defined by the small scale of terraced, semi-detached and detached houses and cottages in a variety of periods and styles. The local materials are predominantly red brick, tile, and slate. Some buildings are rendered and stand out in the street scene. The style of buildings is generally simple but decorative gables, dormers, chimneys and porches introduce variety in some of the larger houses and terraces. Boundaries are marked by walls, fences and hedgerows.



Church Street, Churchover



Church Street, Churchover



School Street, Churchover



Church Street, Churchover



School Street, Churchover



Lutterworth Road, Churchover

CONTEXT

SURROUNDING AREA

GATEWAY RUGBY

2.03 The most recent development to the south of the M6 Motorway is at Gateway Rugby, where the first two phases of a larger master plan are under construction. The scheme contains a mixture of three-storey town houses and two-storey detached, semi-detached, and terraced houses, some with an attic storey, providing focal points in the street-scene.

The layout includes a small green square defined by town houses of more contemporary design. Frontages to dwellings are mainly open with small front lawns and grassed verges to footpaths along main roads.

The architectural form is consistent in the use of pitched roof and gables but the treatment of elevations is varied in using traditional and modern elements. The predominant materials are red and red/brown brickwork with particular facades picked out in rendered panels. The dwellings are contained within the existing landscape with retained groups of trees and areas of woodland.

Gateway Rugby represents a different typology to the small villages that surround rugby and to the farm and rural estate buildings that are typical of the countryside.



Lower Lodge Avenue, Gateway Rugby



Cypress Road, Gateway Rugby



Swift Avenue, Gateway Rugby



Site Plan, Gateway Rugby

CONTEXT

HISTORICAL BACKGROUND

COTON HOUSE ESTATE

Heritage Collective have prepared a full Heritage Statement which is submitted with the application. The following paragraphs briefly summarise the evolution of the estate and how they have informed the development at Coton Park.

2.04 The origins of the estate lie in a monastic institution which, after the dissolution of the monasteries, fell into private hands with one family holding and occupying the property from circa 1551 to 1757. The property passed to a new owner who built the present neo-classical style house circa 1784 and whose descendants lived in the house until 1936. The design is attributed to architect Samuel Wyatt on stylistic grounds. The stable block dates from the late C19.

Post WWII the house was occupied as a residential training centre with the introduction of a significant amount of large scale development that overwhelmed the original buildings.

The implementation of planning and listed building consents for demolition of modern buildings, restoration of the house and new residential development in its environs has resulted in the relationship between the house and its surroundings being greatly improved.

Around 1870, the Coton House Estate amounted to around 798 hectares (1,974 acres). The present estate extends to approximately 48.2 hectares (119 acres).



Eastern elevation from north-east. Date unknown.



Stable Block. Date unknown.



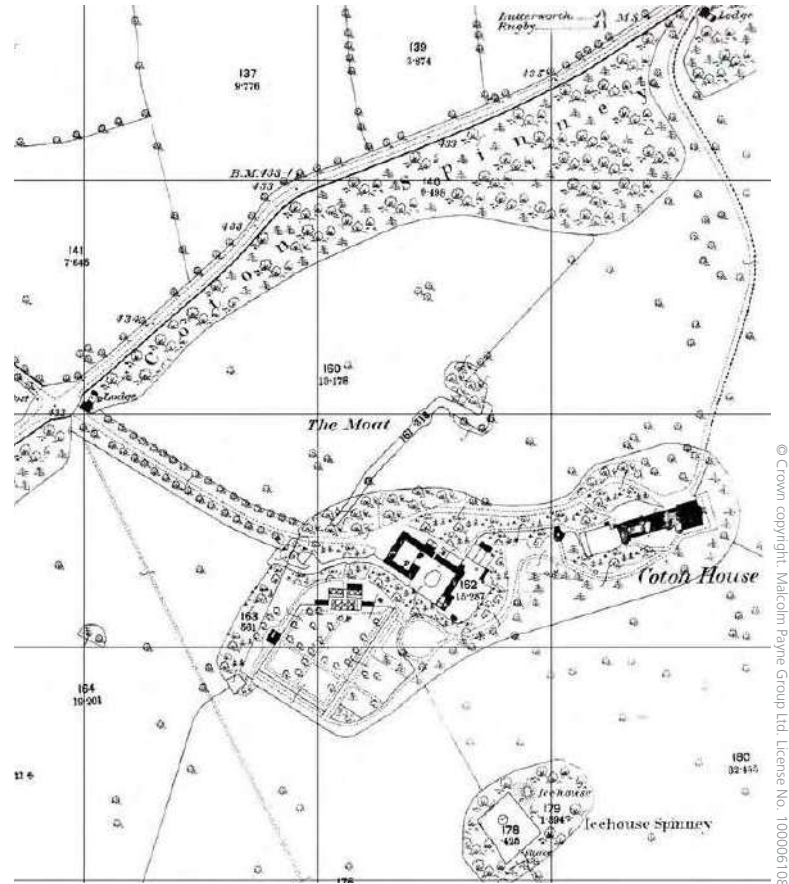
North elevation from north-east c. 1971



View to Southam Court from the north

CONTEXT

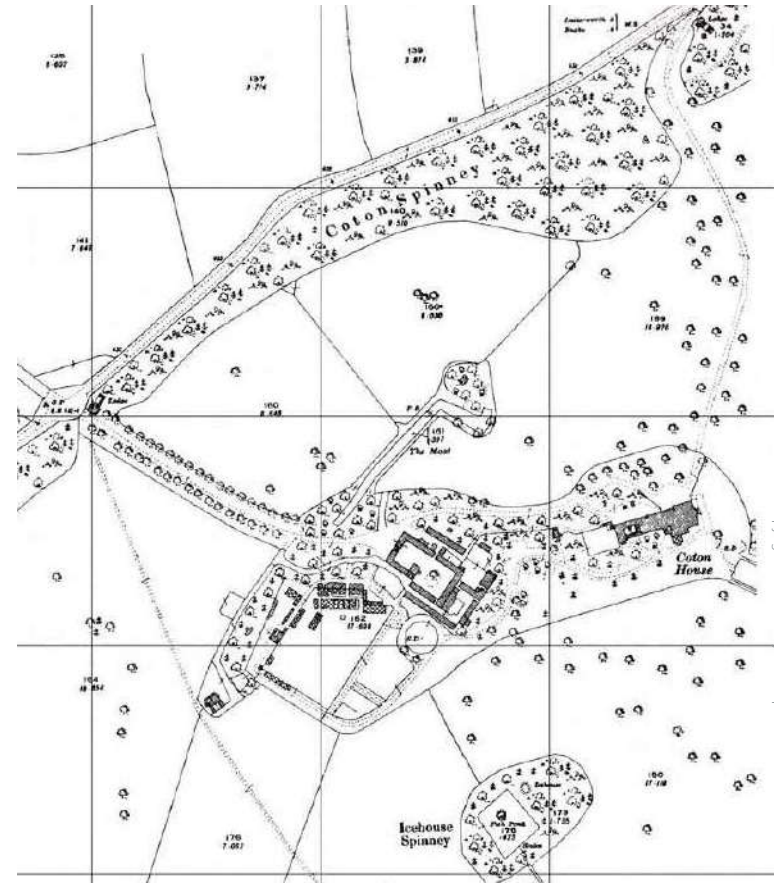
EVOLUTION OF THE SITE



1887 Ordnance Survey Extract

RESIDENTIAL ERA

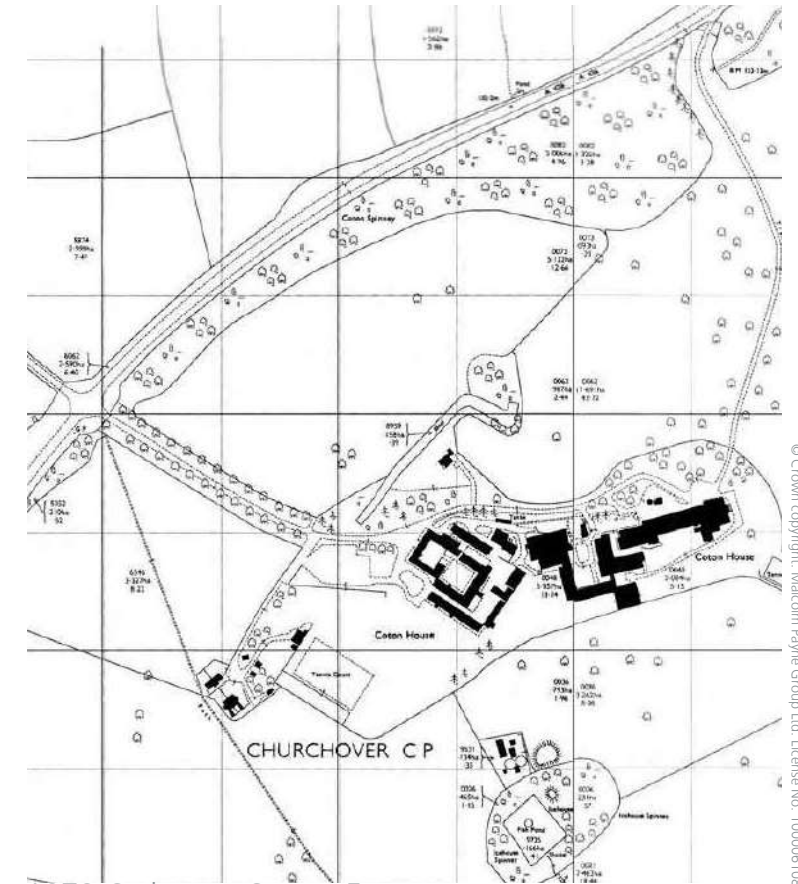
2.05 Historic maps show the evolution of the estate from the 1887 map with the rough layout of the gardens, through to the period as a training centre. Despite the degradation of the estate, the layout had not been completely obliterated, thus providing the opportunity to inform the design of the new estate village.



1925 Ordnance Survey Extract

CHANGING TIMES

2.06 Between the 1887 and 1925 maps an increase in estate buildings around the stable block can be discerned. The house also had a substantial wing on the western side that was demolished sometime after 1948 when the House and Estate were sold for use as a hostel for apprentices.



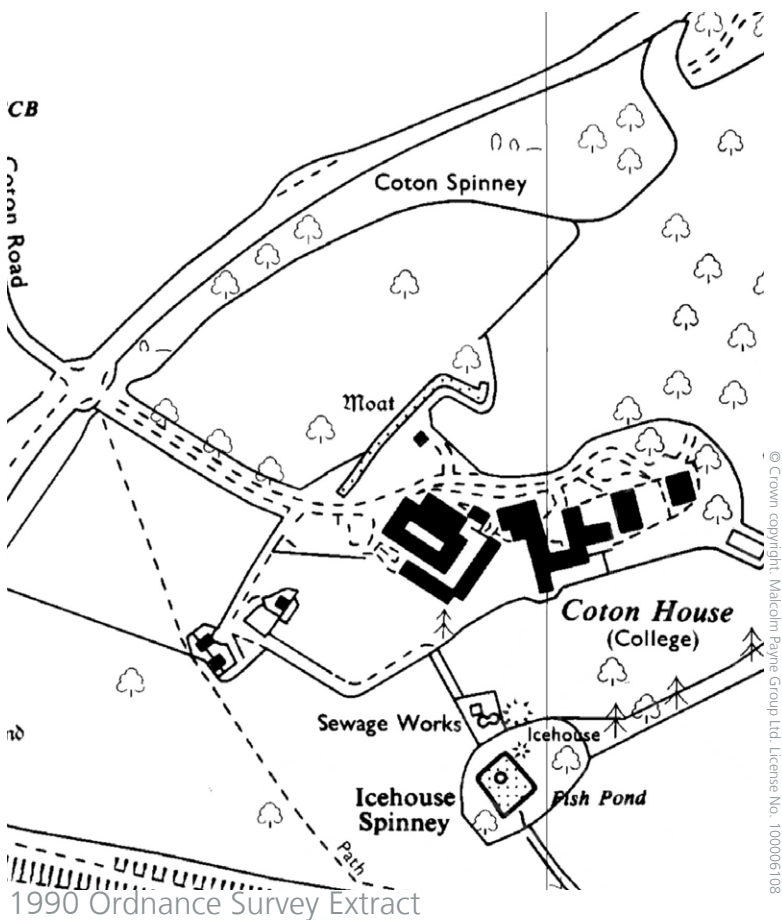
1972 Ordnance Survey Extract

TRAINING CENTRE ERA

2.07 The estate was acquired in 1970 by the Post Office/Royal Mail as a management training college. New buildings were erected and the House was converted and extensively altered circa 1977. In mid-2010 a major fire broke out in the House, resulting in massive interior damage and the loss of the roof.

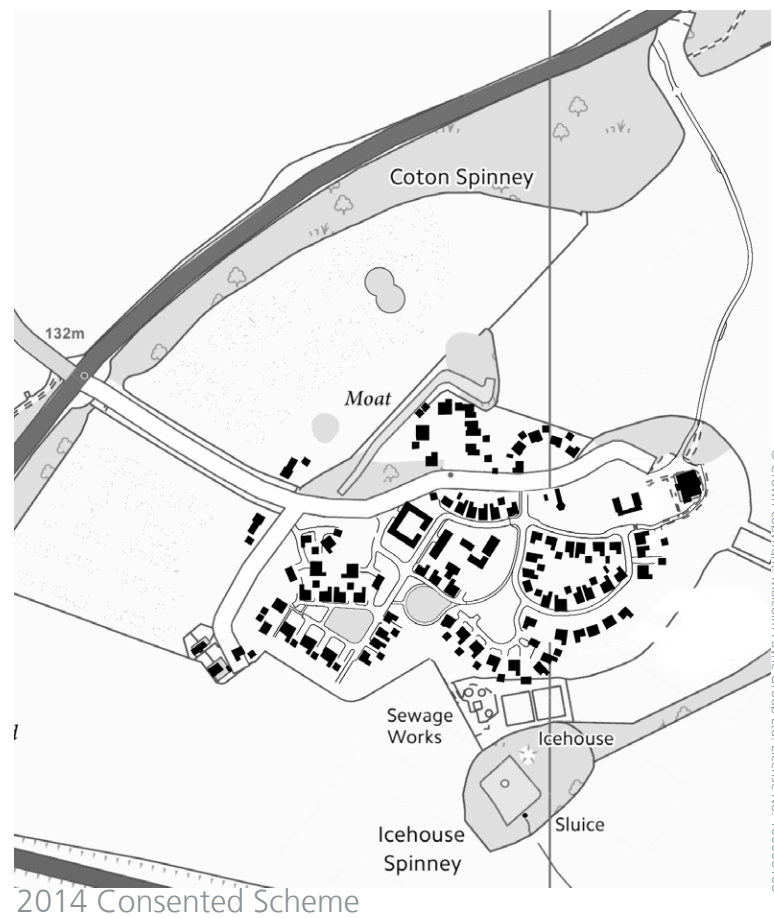
CONTEXT

EVOLUTION OF THE SITE



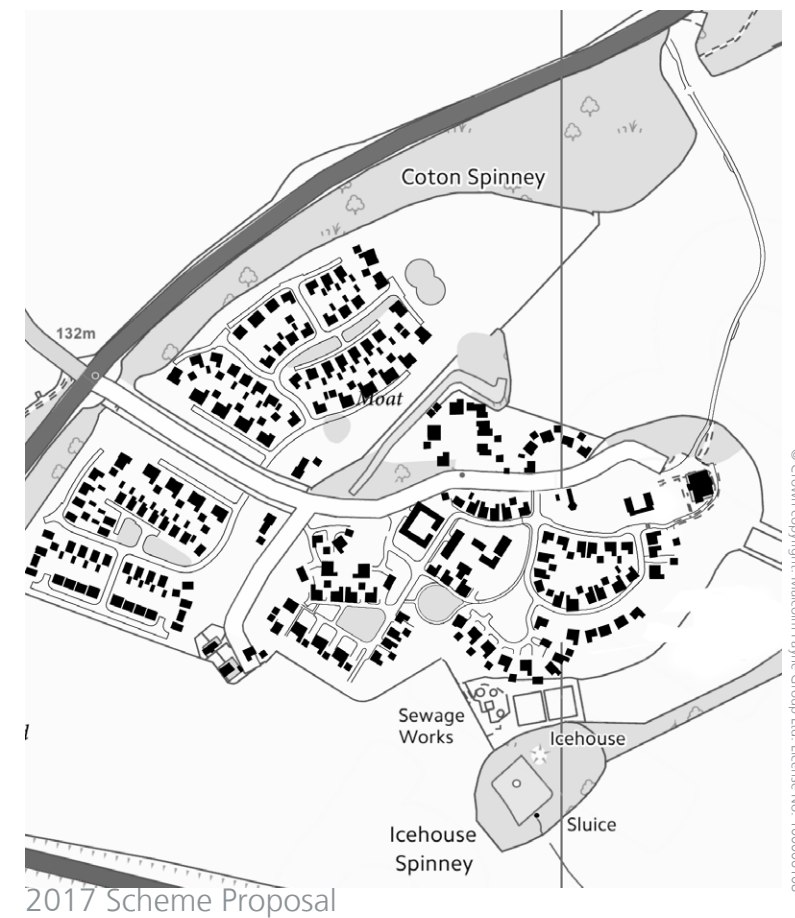
PRE-REDEVELOPMENT

2.08 The listed buildings, although much altered and damaged, still had the potential to be brought back into beneficial use. Demolition of the modern buildings was a prerequisite for the introduction of more sensitive development around the restored House, former stables, dairy and game larder.



CONSENTED SCHEME

2.09 The objective was to create a new residential development in the form of an estate village that would follow the pattern of the original paths, walled garden and landscape features that surrounded the house at the end of C19. A suite of planning and listed building applications gained consent in 2014.



CURRENT PROPOSAL

2.10 The layout proposed in this application would extend the estate village concept on land either side of the south drive. A similar approach to layout is proposed to that established around Coton House in order to harmonise with the existing buildings and landscape, with a similar high quality design of dwellings.

CONTEXT

ESTATE VILLAGE

COTON PARK

2.11 The development nearing completion at Coton Park has been designed to integrate with its parkland setting and retain Coton House, the former Stables and other heritage assets as focal points in the overall layout.

The development was designed in the form of an 'Estate Village' grouped around the country house, estate buildings and gardens. The mature landscape provides a backdrop to the new dwellings and the former garden layout, traced from historic maps, has informed the pattern of roads and the grouping of new houses.

The southern drive from the A426, with its avenue of trees, provides the access and two new lodges have been designed at the entrance to the new housing grouped around Coton House and Stables. The north drive has been reinstated as the approach to the House and specimen trees have been planted as a screen between the listed buildings and the new dwellings

The elements of the gardens that still remain, the topiary garden, remnants of the walled garden and groups of specimen trees form a landscaped backdrop to the built form. As the new buildings weather and the new planting matures, the development will soften and harmonise further with the parkland.



Coton Park Site Plan

CONTEXT

ESTATE VILLAGE

COTON PARK

2.12 The listed buildings, Coton House, the former Stables and Dairy, have been restored and converted to residential use. A parterre garden has been created on the approach to the House, with a garage court to screen cars from view.

The arrangement of houses defines the line of roads and paths; the proportions and style of new dwellings are designed to harmonise with the existing buildings, without being a direct pastiche of earlier styles. The development contains a mix of dwelling types within an integrated architectural treatment.

The materials used in elevations include the red brick of the former Stables. Stone dressings and slated roofs reflect the materials in Coton House. Boundary treatments, using estate fencing and farm gates, are appropriate to the rural setting. Roads and drives are finished in a variety of materials to define the hierarchy of routes and frontage planting defines public and private spaces.

The approach to the layout and design of dwellings provides a precedent for any further development within Coton Park Estate and the detail, materials and landscaping set a quality standard for the architectural treatment and integration in the parkland setting.



Coton House



New Coton Park Development



New Coton Park Development



Former stables

CONTEXT

SITE DESCRIPTION

EXTENT

2.13 The site consists of two parcels of land, formerly used as pasture, on either side of the tree-lined drive to the new housing at Coton Park. The land amounts to approximately 11.31 hectares (27.95 acres) including an area of retained parkland to the northeast.

SITE ACCESS

2.14 The existing hard-surfaced drive from the A426 provides the access to the site and the entrance to each parcel abuts the gardens to the new lodges situated at the south east end of the drive. The principal access to Coton House has been redirected to the north drive.

BOUNDARIES

2.15 The long north-west boundary of both parcels is separated from the A426 by a thick woodland belt which widens to the north and effectively screens the site from the highway. The short southern boundary is marked by a hedge and sporadic trees and is more open to the adjoining farmland.

The south-eastern boundary is separated from the new development around Coton House and the former Stables by a belt of hedges and trees beyond a post and rail timber fence, providing a clear area of separation. The boundaries of the parcels of land on either side of the drive are marked by a line of estate fencing beyond the line of trees.



CONTEXT

SITE DESCRIPTION

TOPOGRAPHY

2.16 The land is relatively flat, open parkland with few trees within the southern parcel of land and sporadic mature trees within the northern parcel. The most notable of these trees are two large poplars that, although not of great value in terms of species, are of considerable size and consequent impact on the landscape.

The land survey prepared by Geoffrey Perry Associates Ltd indicates the topographical features, including the contours of the site.

LANDSCAPE

2.17 The application site is not subject to any specific landscape-related designation. The two parcels of land, laid to grass are situated in a wider area of parkland, woodland, pasture and arable land. The site has a high degree of visual enclosure by the belt of trees along the A426 and the groups of trees and hedgerows to the southern boundary and beyond the site to the east.

The Landscape & Visual Impact Assessment prepared by Pegasus Design fully describes the landscape character of the Coton House Estate and the surrounding countryside.

TREES

2.18 Apart from the poplars referred to above, the significant trees are associated with boundaries and provide effective screening to the north-western and south-eastern boundaries. The avenue of lime trees screen both parcels of land from the south drive.

The Tree Survey & Arboriculture Report prepared by Ruskins describes the planting within and around the site.



1. View south west from northern parcel



2. Moat located adjacent to site boundary



3. View south-west along access drive into site



4. View north-east along A426 towards site entrance



5. Northern lodge building



Photo Location Plan

CONTEXT

SITE DESCRIPTION

ECOLOGY

2.19 The site mainly consists of unimproved grassland. Significant wildlife habitats lie along and beyond the site boundaries, particularly the woodland belt between the site and the highway to the west and the moat and its environs to the east rather than within the site.

The Phase 1 Habitat Survey prepared by FPCR describes the ecological significance of the site.

HERITAGE ASSETS

2.20 In summary, the land falls within the historic setting of the Grade II* listed Coton House, the Grade II listed former Stables and associated structures.

The setting was severely compromised by the modern development associated with the former training centre and has been substantially improved by the new residential development around Coton House, which effectively screens the house from the area of site proposed for the application scheme.

The Assessment prepared by Heritage Collective describes the significance of heritage assets.

RIGHTS OF WAY

2.21 In summary a public footpath runs diagonally across the southern parcel of land connecting the entrance to the south drive on the A426 with the footpath network to the south-west and connects the A426 near the entrance to the drive with a footpath network to the south of the site.

The Traffic Impact Assessment & Highways Report prepared by DTA includes identification of public rights of way.

NOISE

2.22 In summary the site is affected by traffic noise from the A426, only partly mitigated by the woodland belt between the site and the road, and from the motorway across the open boundary to the south.

The Acoustic Report prepared by Cundall describes the noise conditions on the site.

DRAINAGE

2.23 In summary a moat on the adjoining land to the southeast of the northern parcel provides a catchment for a proportion of the surface water from the site.

The Drainage and Flood Risk Report prepared by Woods Hardwick describes the existing surface water system and potential for flooding.

ARCHAEOLOGY

2.24 In summary the site is not considered to have a high potential for archaeological remains and the Archaeological Report describes the scoping study undertaken and the recommendations for further investigation.

The Report prepared by Cotswold Archaeology describes the site's archaeological significance.

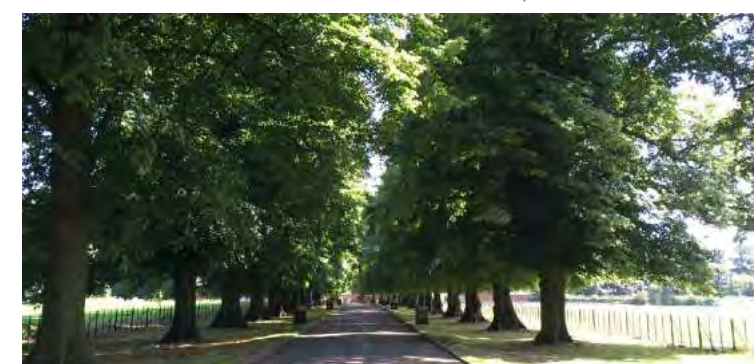
GROUND CONDITIONS

2.25 An existing water main runs diagonally across the southern parcel of land alongside the current public right of way. The inner zone of a high pressure gas main runs south of the site boundary with the outer zone running through the southern parcel of land.

CALA Homes has undertaken a survey of mains services and investigated ground conditions.



6. View north-west across northern parcel of site



7. View south-east along access drive into site



Photo Location Plan

CONTEXT

PLANNING POLICY

LOCAL & NATIONAL POLICIES

The Planning Statement prepared by Framptons Planning Consultants provides a comprehensive commentary on the policies that are applicable to this site and how they have been addressed in the application scheme.

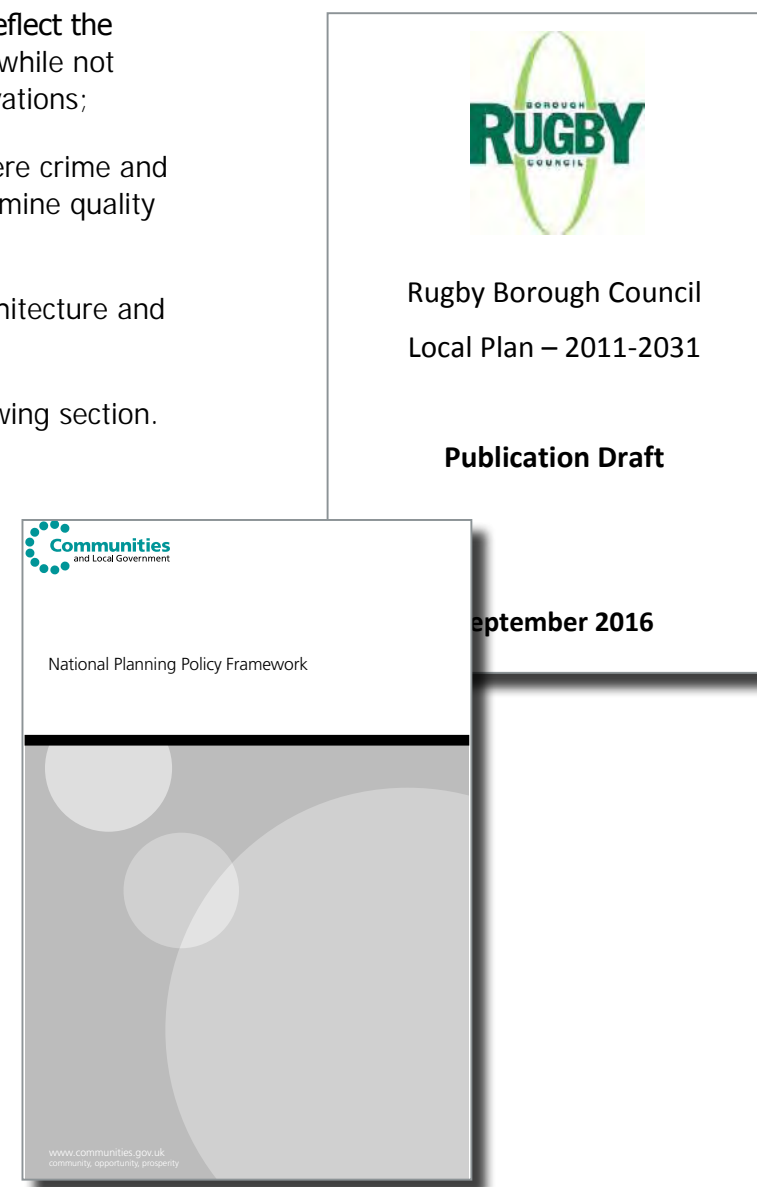
2.26 This Design & Access Statement does not attempt to repeat or summarise the contents of the Planning Statement except to state that the design principles that have informed the development have recognised the importance of the policy set out in Section 7 of the National Planning Policy Framework 2012: 'Requiring good design'.

2.27 The Design Team have been mindful in the evolution of the scheme of the NPPF requirements to ensure that developments:

- will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live, work and visit;
- optimise the potential of the site to accommodate development, create and sustain an appropriate mix of uses (including incorporation of green and other public space as part of developments) and support local facilities and transport networks;

- respond to local character and history, and reflect the identity of local surroundings and materials, while not preventing or discouraging appropriate innovations;
- create safe and accessible environments where crime and disorder, and the fear of crime, do not undermine quality of life or community cohesion; and
- are visually attractive as a result of good architecture and appropriate landscaping.

The design principles are set out in the following section.





DESIGN PRINCIPLES

SECTION 3

DESIGN PRINCIPLES

OPPORTUNITIES & CONSTRAINTS

3.01 Every site has elements that constrain development but such constraints also provide a sound framework for design. The application site is considered to offer the opportunity to provide an attractive extension to the 'estate village' already under construction to the east.

The plan on the following page analyses the constraints on development and the commentary below indicates how the issues that arise can be positively addressed to achieve a scheme that is appropriate to its historic and landscape setting.

DEVELOPABLE AREA

3.02 The extent of site that is considered for built development is defined by views towards the parkland and offsets from boundaries. On the basis of this analysis, a substantial area of parkland in the northeast part of the site has been excluded from development.

Offsets from other boundaries provide a planted buffer to the agricultural land to the south and open space between new housing and the moat. The approach is to contain groups of dwellings within a landscape structure that integrates the development within the countryside.

VEHICLE ACCESS

3.03 The new housing would be served from the existing entrance from the A426, via the south drive with road improvements within the highway boundaries. Entrances on both sides of the avenue of Limes would be required to provide vehicle access to each parcel of land and the chosen positions are at the end of the drive furthest from the highway, where the new lodges in the adjoining development terminate the view.

LANDSCAPE FEATURES

3.04 The woodland belt along the frontage to the A426 provides a visual barrier that would screen new development from the highway and provide privacy to the dwellings within the site. The canopies and root protection zones of the trees and the offset required to protect the 'wildlife corridor' from excessive light spillage have been established, with the result that the proposed houses and private drives have been set back a substantial distance from the belt of trees along the northwest boundary.

The two Poplars trees within the northern parcel are significant elements in the landscape. Their root protection areas are extensive and the arboriculture report recommends work to balance their canopies. Their retention would provide a focal point in the new development and screen new housing from the parkland to the north and east.

HISTORIC FEATURES

3.05 The south drive, with its avenue of Common Lime trees is a particularly important element of the parkland and the approach to the listed buildings, particularly to the former Stable Block now that the north drive has been reinstated as the main access to the House. A margin of grassland on either side of the avenue of trees is proposed to distance new houses and to filter views.

The two new lodges at the head of the drive are now part of the setting of the stables. Positioning the access to the two parcels of land adjacent to the lodges would give them greater prominence as visual markers at the entrance to Coton Park. The site is remote from Coton House and the former Stables and their immediate setting, but views to and from the listed buildings and their environs are matters for consideration.

WILDLIFE PROTECTION

3.06 The scheme presents the opportunity to strengthen boundary planting. Apart from the function of screening new development the landscape scheme is designed to reinforce and extend the wildlife corridors around the site. In addition, there is the opportunity to introduce tree planting within the housing layout to provide further routes and links to the surrounding wooded areas.

NOISE PROTECTION

3.07 Noise levels dictate that the scheme is set back from the A426. Consideration of noise levels within dwellings and in external private gardens is required. Mitigation is necessary in the layout to create 'noise buffers' and screen boundaries, and in the design of houses, in terms of glazing specification and ventilation.

FOOTPATHS & EASEMENTS

3.08 The existing public footpath and water main running across the southern parcel of land would need to be re-routed to achieve an efficient layout and, in the case of the footpath, to link in with new development. The opportunity exists to improve the pedestrian and cycle access and links to public transport.

DRAINAGE & SERVICES

3.09 The landscape buffer along the southeast boundary is seen as an appropriate location for a sustainable surface water drainage system and for a sewage treatment plant. This area is also seen as a suitable position for an electricity sub-station. All mains services are available with connections to the adjacent new housing development.

The southern part of the site falls within the 'outer zone' of a high pressure gas main in which residential development is permitted.

DESIGN PRINCIPLES

CONSTRAINTS PLAN

High Pressure Gas Main (Dithershorpe-Churchover)
Exact location and easement to be confirmed by National Grid Gas

KEY	CONSTRAINT	DESCRIPTION
[Red line]	Site Boundary	From Title Plan
[Red dashed line]	Root Protection Area	From Tree survey completed by Ruskins Group Consultancy in 2015.
[Green circle]	Category C - Low quality and value tree	From Tree survey completed by Ruskins Group Consultancy in 2015.
[Blue circle]	Category B - Moderate quality and value tree	From Tree survey completed by Ruskins Group Consultancy in 2015.
[Green circle]	Category A - High quality and value tree	From Tree survey completed by Ruskins Group Consultancy in 2015.
[Red circle]	Category U	From Tree survey completed by Ruskins Group Consultancy in 2015.
[Red circle]	Tree to be removed for access	
[Red dashed line]	Foul Sewer	From as built records
[Blue dashed line]	Existing Water Main	From as built records
[Red dashed line]	Existing Low Voltage electric cable	From WPD asset plans
[Red dashed line]	Existing High Voltage electric cable	From WPD asset plans
[Green dashed line]	Existing BT Cables	From BT asset plans
[Orange area]	Foul Water Treatment Plant	
[Yellow line]	High Pressure Gas Main	From National Grid Gas (NGG) asset plans
[Dashed line]	Indicative Proposed Surface Water Drainage Pipe	
[Orange area]	High Pressure Gas Main Inner Zone	Based on guidance from HSE (no residential dwellings permitted)
[Light blue area]	High Pressure Gas Main Outer Zone	Based on guidance from HSE (residential dwellings permitted)
[Purple area]	Potential Landscape Buffer	
[Green area]	Park Land	Planning requirement
[Green dashed line]	Public Right of Way	Warwickshire County Council Title 1998 Definitive Map
[Dashed line]	Limit of residential developable area	
[Brown dashed line]	Possible acoustic fencing	Assumed - exact extent and size to be confirmed by noise modelling
[Pink dashed line]	Moat Stand Off	15m stand off to built form
[Blue dashed line]	Proposed Build Line	Line of outward looking development

DESIGN PRINCIPLES

EVOLUTION OF SCHEME



FIRST CONCEPT SCHEME

3.10 The first concept scheme shown above proposed 134 dwellings. The spread of development extended beyond the Poplar trees in the northwest and closely followed the line of the drive. This number was substantially reduced following early discussions with the Council's Officers with a resultant reduction in the developable area shown in blue.

The revisions that were made before the production of the scheme for public consultation included a reduction in the spread of development in the northwest and south to screen views, and the redesign of the layout along the woodland belt to the A426. The revision of the northwest boundary allows the retention of the two Poplar trees that were proposed for removal in the concept scheme.



CONSULTATION SCHEME

3.11 The pre-application and public consultation scheme shown above was reduced to 100 dwellings. This scheme introduced landscaped areas within the developable area to strengthen the interrelationship of dwellings and landscape.

Discussions with the Council's Officers resulted in further revisions shown in blue, including an increase in the offset from the avenue of trees along the drive, a further cut back of the developable area in the northwest to provide a softer edge to the parkland and an increase in open space at the entrance to the site.



APPLICATION SCHEME

3.12 Further revisions have been made to reduce the spread of development in the application scheme illustrated in diagrammatic form above. The updated scheme remains for 100 dwellings. The evolution of the scheme reflects the advice from the Council's Officers and the recommendations set out in the specialist consultants' reports, for instance the development now fronts out to the A426 to offer attractive and active frontages through the trees.

In summary, over the consultation process, the proposed quantity and spread of dwellings has been substantially reduced, prominent views have been taken into account in defining the developable area and a significant amount of landscaping has been introduced within the housing layout.

DESIGN PRINCIPLES

LAYOUT, DENSITY & MIX

LANDSCAPE CONSIDERATIONS

3.13 The development areas and the overall configuration of the layout have been determined through an analysis of noise contours within the site and by careful consideration of the potential impact of new development on the existing environment.

The site is heavily influenced by the established landscape structure of parkland and woodland belts that adjoin and stretch beyond the site boundaries. The development area benefits from a strong degree of containment afforded by the established woodland belt that defines the boundary of the estate along the A426 corridor. Similarly, the margin within and outside the site boundary between the proposed and current development ensures that the impact of the proposals will be highly localised and restricted to the immediate landscape context.

Where open views exist along the southern boundary, additional screening is proposed. Where sensitive views along the drive are to be protected, the proposed development is looser in form than elsewhere and set well back from the avenue of trees. A building line has been established in the north-eastern area of the site based on an analysis of views from Coton House and the north drive, retaining mature trees (including the poplars) and parkland and reinforcing the existing planting.

The overall concept is to integrate the new development within the landscape and to achieve a soft edge to the surrounding parkland, woodland and open fields.

PRINCIPLES OF LAYOUT

3.14 Notable elements introduced into the scheme during the pre-application process include the significant extension of the area of retained parkland to the north east, the widening of margins between the houses and the avenue along the drive, and the introduction of a green route through each parcel of land.

The layouts for the two parcels of land are seen as providing two different character areas, based on their size and configuration. The northern parcel, an elongated strip of land following the gentle curve of the tree belt along the A426 leads to the parkland to the northeast and the developable area is terminated at the pair of Poplar trees. The proposed groups of houses follow the curve and a central spine road is enhanced by a grassed area and line of trees along one side to provide a link with the parkland beyond.

The southern parcel is more or less square and, in contrast with the linear northern layout, the configuration of boundaries is reflected in the central green space with houses set back on either side. In both areas, the pattern of roads and drives has been designed to maximise the use of shared surfaces.

RESIDENTIAL DENSITY

3.15 The overriding principle is to achieve a low density scheme that would be well integrated into the parkland. The site area comprises 11.31 hectares, excluding the entrance drive, of which approximately 4.33 hectares or 38% of the total site is the development area. The overall density proposed is around 9 dwellings per hectare and the density on the net development area of housing is around 23 DPH.

ORIENTATION OF DWELLINGS

3.16 The houses on either side of the Drive are set back from the Limes behind a margin of grass. They follow a formal building line to delineate the route into the site and are arranged symmetrically. Car parking is set behind the front building line and garages at the rear of the gardens.

The houses along the tree belt to the main road are set back with a wide margin between their private drives and the edge of the woodland. Houses along the southern and south western boundaries have their main elevations facing the areas of open grassland and/or planting that provide a landscape buffer to the adjoining land.

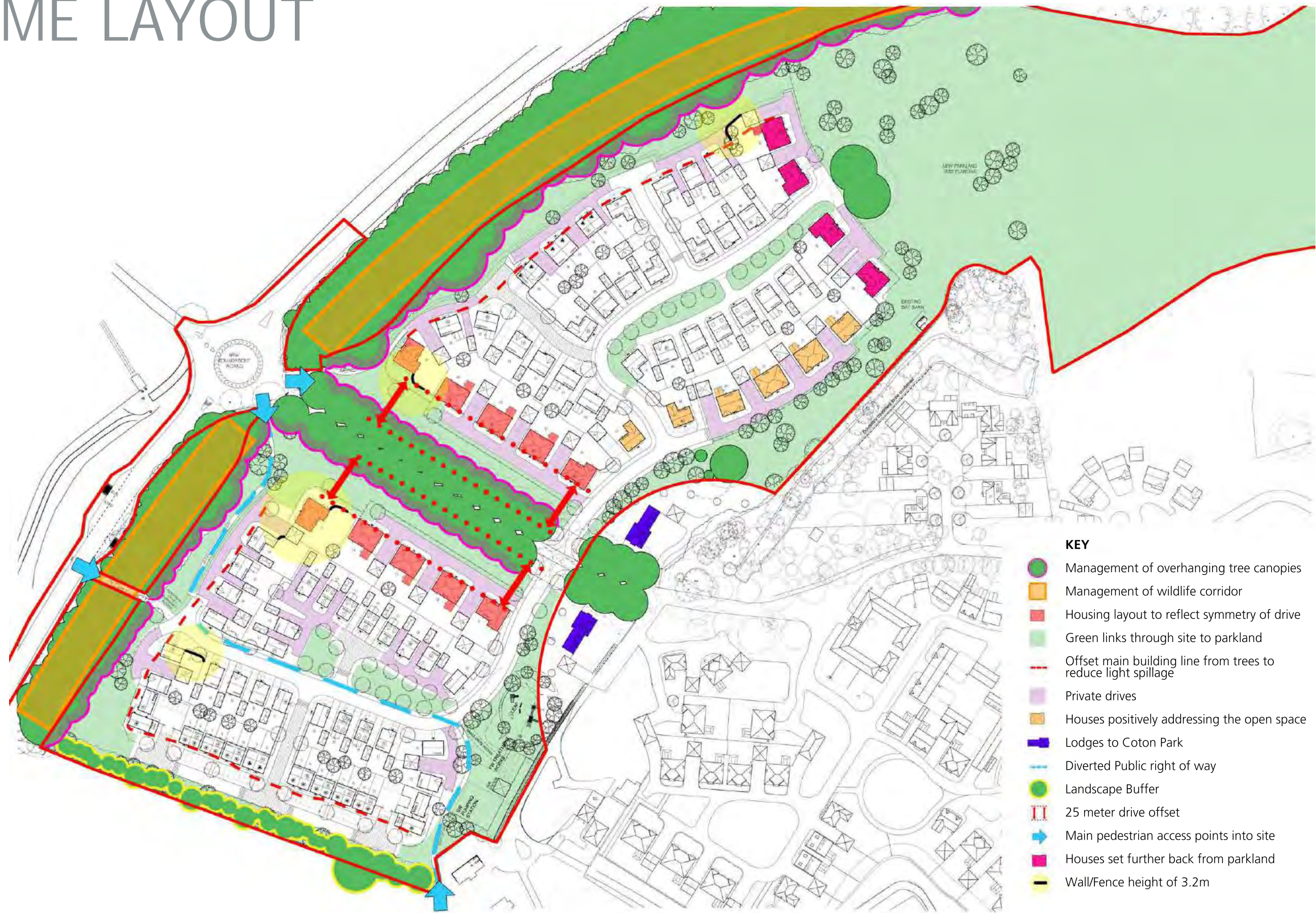
MIX & SCALE

3.17 The objective is to achieve a wide mix of dwelling types, sizes and tenures to extend the range and choice of dwellings within the locality. The layout plan is based on a mixture of dwellings ranging between one bedroom apartments and five bedrooms detached houses, as shown on the diagram.

3.18 The scale of the houses would be similar to those permitted in the development under construction. The dwellings would be up to two storeys in height, some with rooms in the roof-space. Their overall height would vary with the 'classical' style houses being higher and the 'cottage' style dwellings being lower, to achieve in each case the appropriate proportions of windows and doors.

DESIGN PRINCIPLES

SCHEME LAYOUT



DESIGN PRINCIPLES

DENSITY, MIX & SCALE

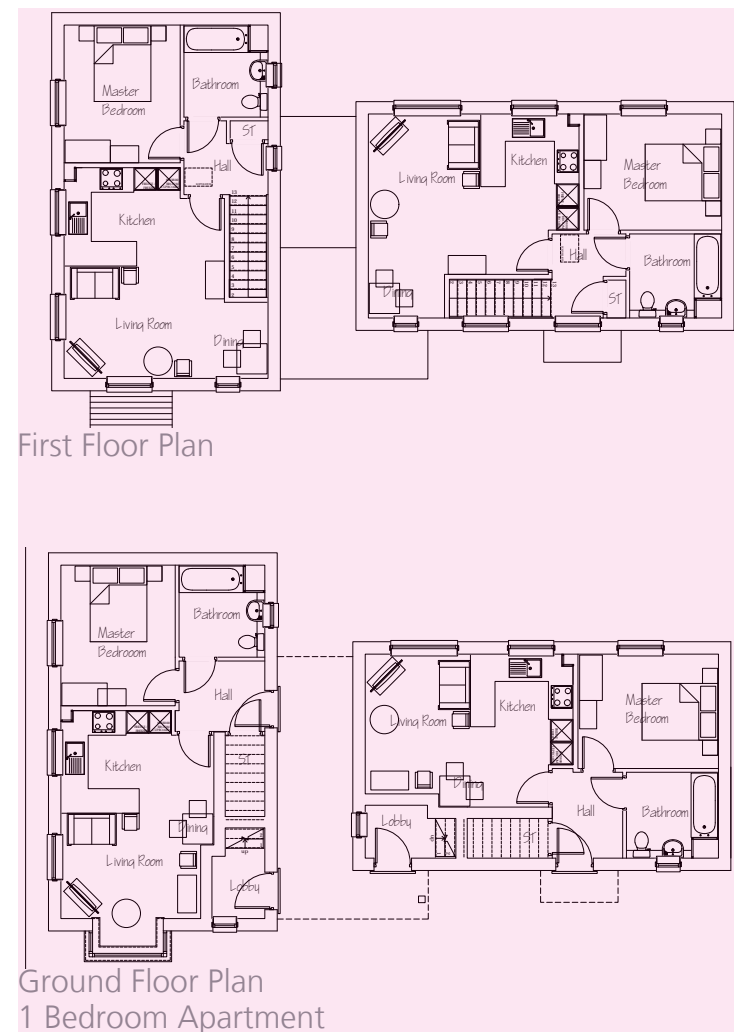


DESIGN PRINCIPLES

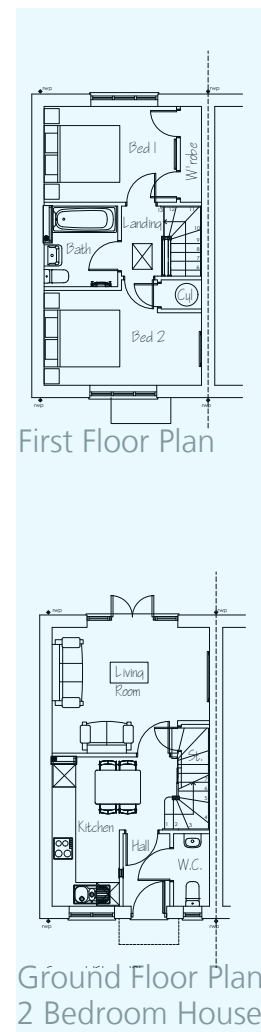
DWELLING TYPES

FLOOR PLANS

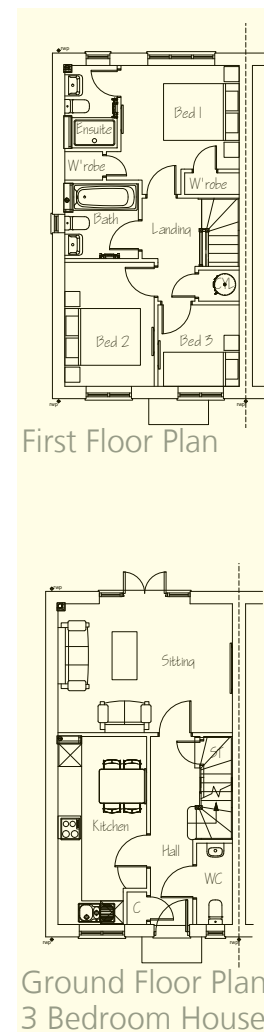
3.19 The diagrams illustrate the range of dwellings proposed. The affordable range of dwellings is wider than the range proposed for private sale with more dwellings at the smaller end of the scale but both ranges achieve the same space standards as equivalent houses with the same number of bed spaces.



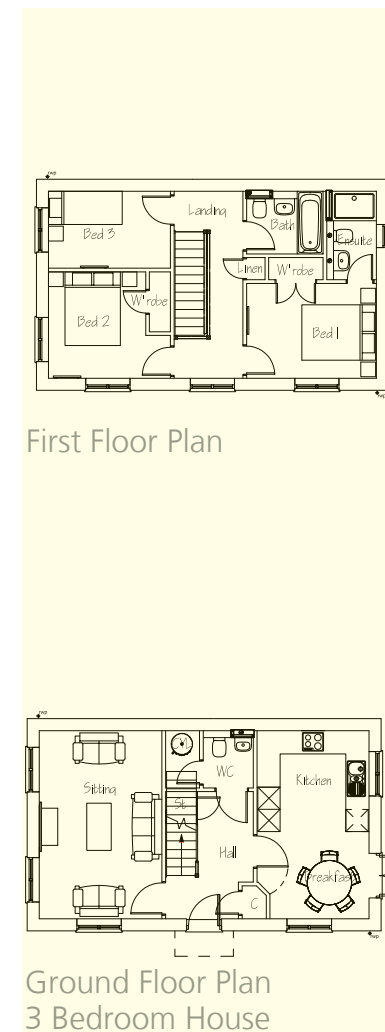
MAISONETTES



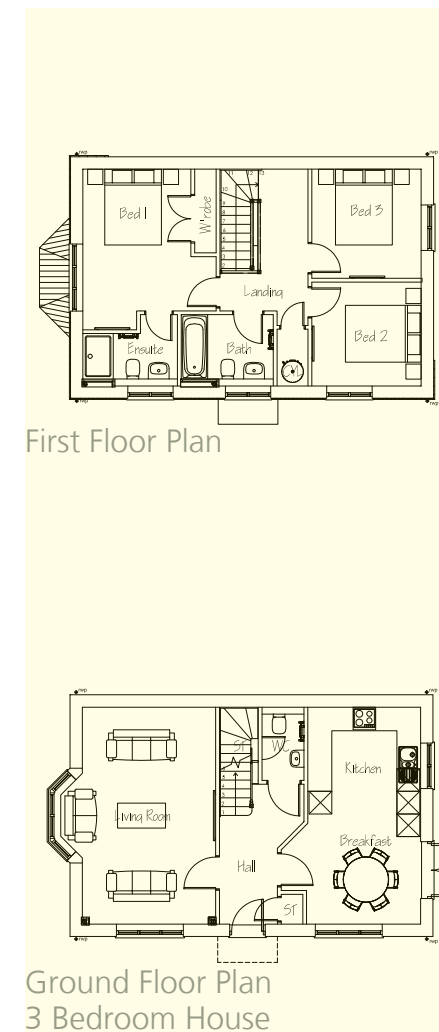
BELFORD



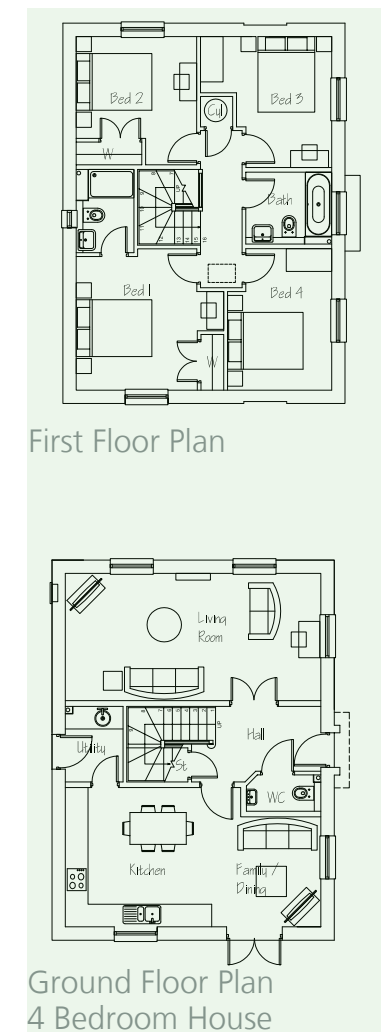
GOSFIELD



HISWICK



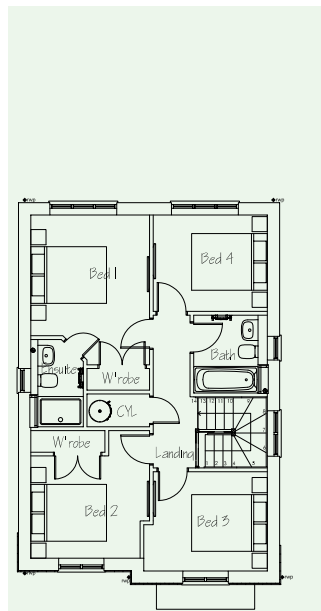
HURWICK



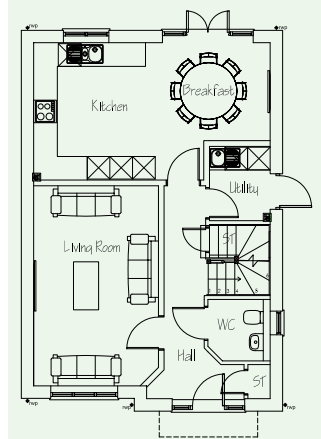
GLOUCESTER

DESIGN PRINCIPLES

DWELLING TYPES

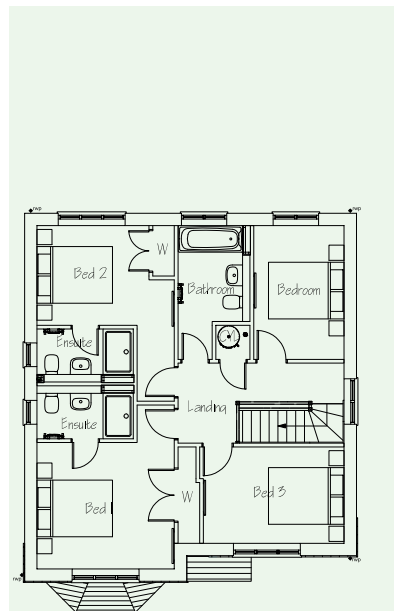


First Floor Plan

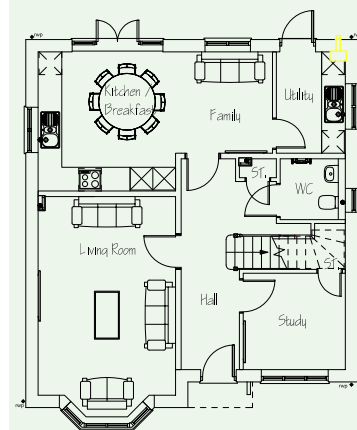


Ground Floor Plan
4 Bedroom House

KINFIELD

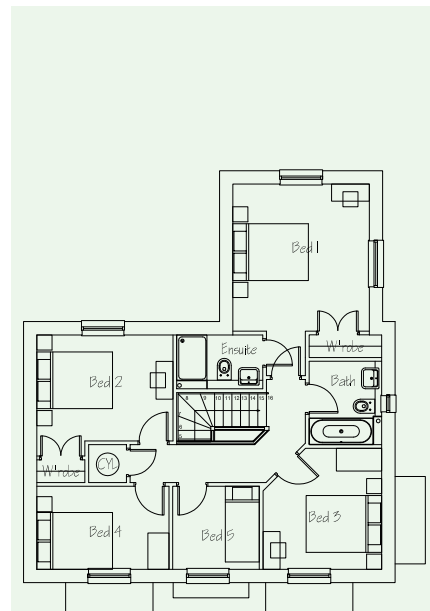


First Floor Plan

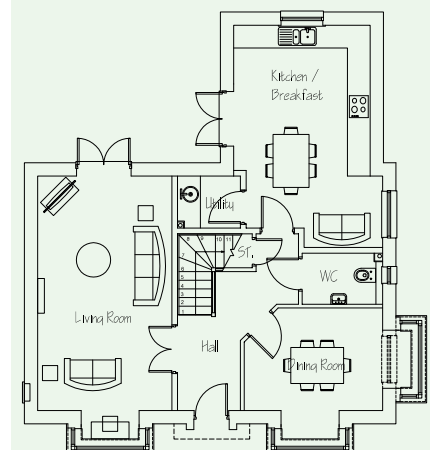


Ground Floor Plan
4 Bedroom House

NENHURST

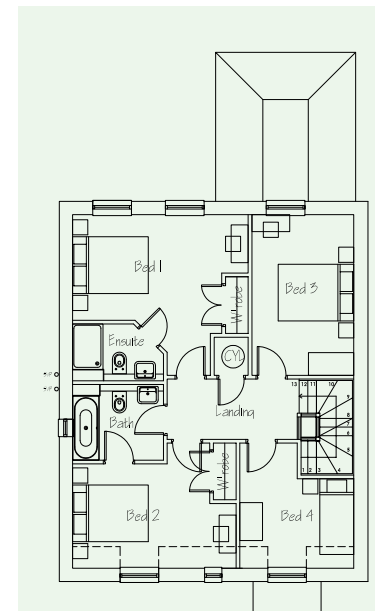


First Floor Plan

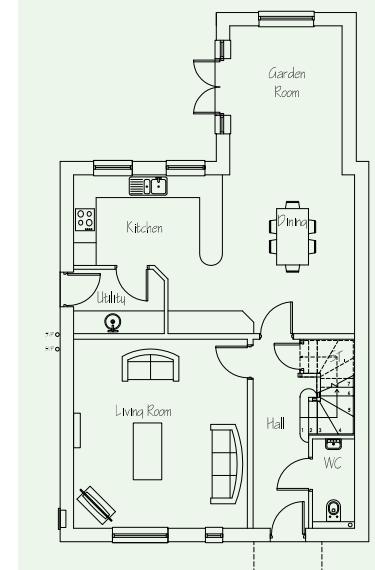


Ground Floor Plan
4 Bedroom House

HELMSLEY

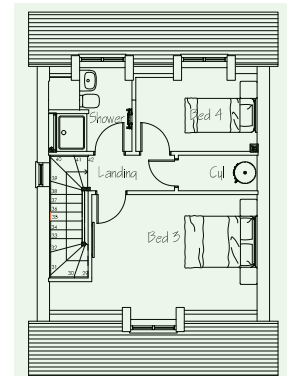


First Floor Plan

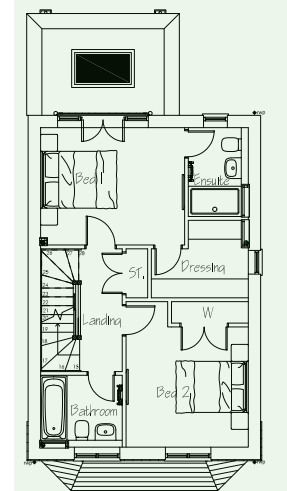


Ground Floor Plan
4 Bedroom House

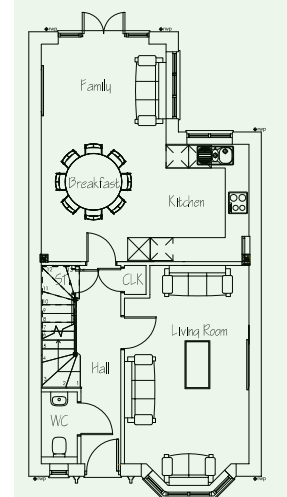
HAZELMERE



Second Floor Plan



First Floor Plan

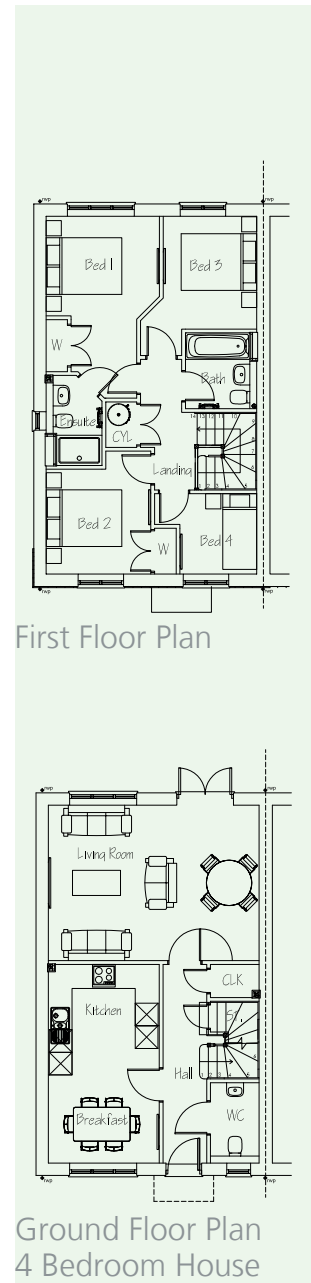


Ground Floor Plan
4 Bedroom House

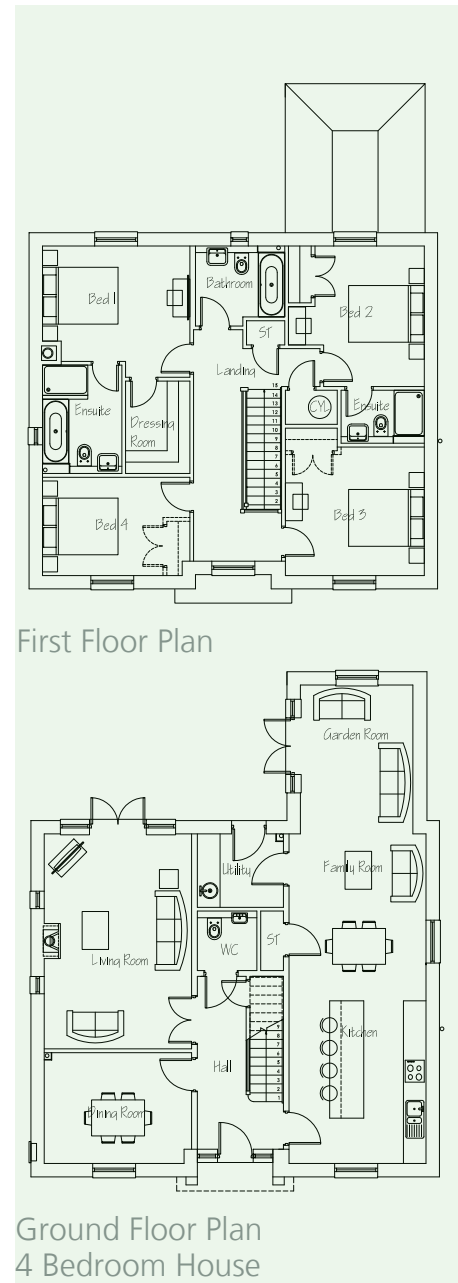
MADELEY

DESIGN PRINCIPLES

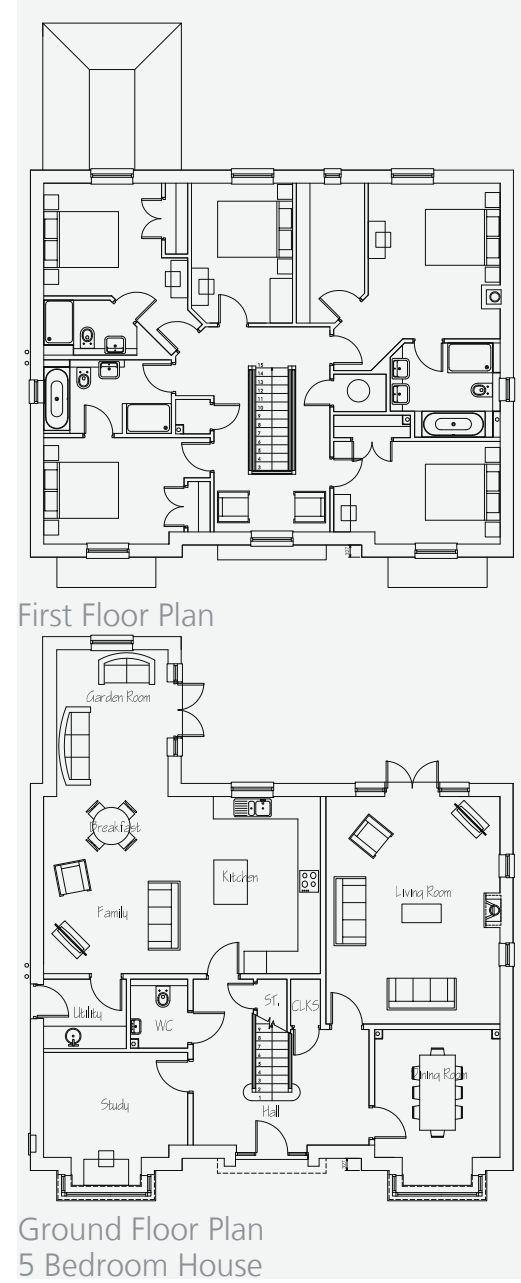
DWELLING TYPES



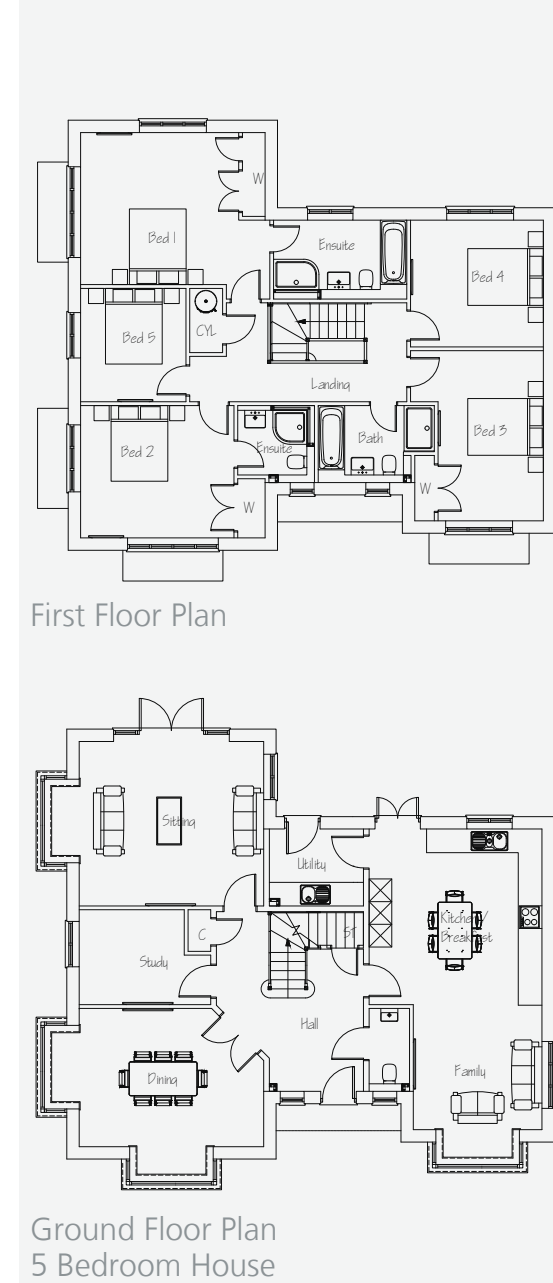
JAYFIELD



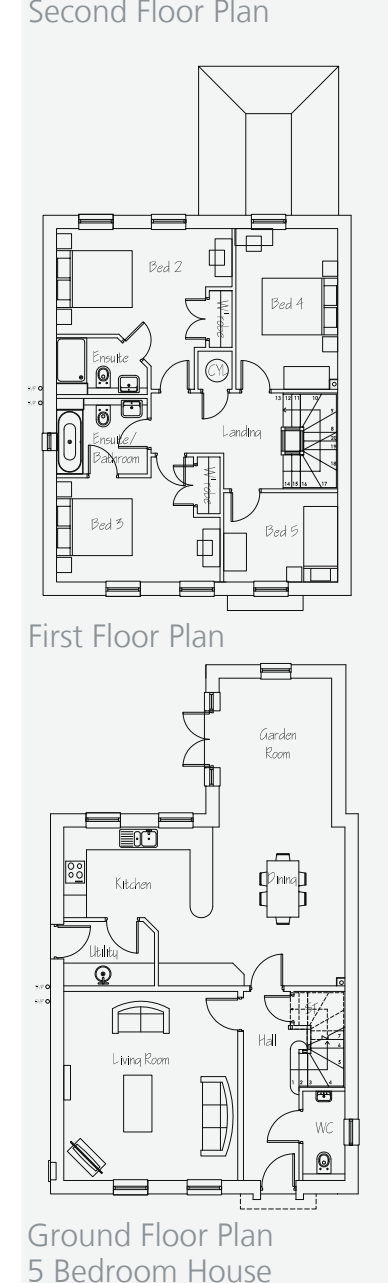
KEMPTON



RUISLIP



TILHURST



HAZELMERE +

DESIGN PRINCIPLES

ARCHITECTURAL TREATMENT

ARCHITECTURAL TREATMENT

3.20 The architectural treatment follows the bespoke range of dwellings designed for the **consented scheme for Coton Park**. The objective is to follow the architectural vernacular and landscape principles that have created a harmonious development around Coton House and the former stables, and to provide a sense of continuity in built form and establish an integrated community.

LARGER HOUSES

3.21 The 'classical' style for larger dwellings are so described because of their more symmetrical elevations and the proportions of window and door openings. They are used in areas that demand a degree of formality in layout and appearance, such as on either side of the avenue along the drive. They are also used as focal points in the layout.

SMALLER HOUSES

3.22 The 'cottage' style is more suited where the backdrop of woodland and boundary planting requires a more random grain of development and a more varied skyline. The vertical emphasis of gables and dormers sits well against the backdrop of the woodland trees.

The same quality of design and materials and the same attention to the detail of hard surfaces, planting and boundary treatments is proposed for the larger and smaller dwellings and their surroundings to reinforce the continuity between the groups of houses.



DESIGN PRINCIPLES

DETAIL & MATERIALS

STYLE

3.23 The vocabulary of architectural detail is illustrated on the houses currently under construction at Coton Park. The use of gables and projecting bays provides depth and texture on the main and prominent return elevations.

The contrast between the 'classic' style dwellings and the 'cottages' avoids conformity of layout and produces groups of dwellings with a particular character, and individual houses that provide 'markers' in the street scene and in particular views.

MATERIALS

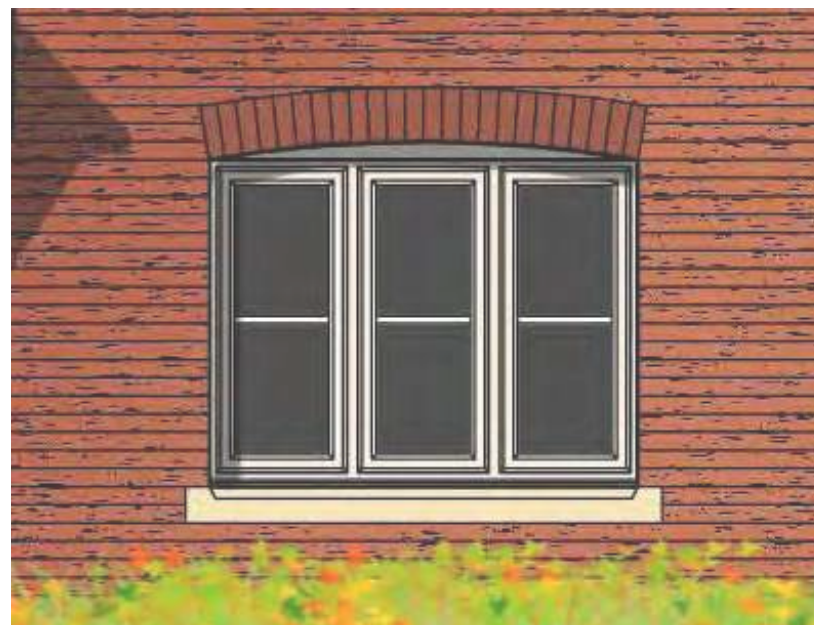
3.24 A restricted palette of materials is used to achieve harmony between the proposed dwellings and those that are under construction. The choice of materials in the earlier scheme picked references from the former Stables and Coton House and took account of the colours and textures of materials against the backdrop of trees and hedges.

BOUNDARIES & FLOORSCAPE

3.25 Similarly, boundary treatments are chosen to provide the subtle enclosure of open grassland or the more robust fencing of woodland and pasture.

The detail landscape proposals will be developed to offer a variety of 'soft' and 'hard' boundaries using hedgerows and shrubs, or timber and estate fencing, depending on the need for openness or privacy.

Materials for paving, roads, drives and paths will be chosen for their wearabilities and permeability qualities, according to location, and their texture and colour.



DESIGN PRINCIPLES

STREET SCENES

THE AVENUE

3.26 Development along the drive is restricted to just five symmetrically designed houses, set back from the avenue of trees and separated by parking bays with recessed garages

The two houses nearest to the road take the form of two-storey 'lodges' marking the entrance to the site and lie behind the building lines for the remaining four houses on each side of the avenue.

The gardens to the 'lodges' are screened by curved brick boundary walls and the houses are set behind a wide expanse of grass with low estate boundary fencing.



THE CENTRAL SPINE

3.27 The central spine road is flanked on one side by a grass border and line of trees that provide a link to the Poplars and parkland beyond.

A hierarchy of house, with a limited number of sizes and types on either side provides a degree of formality behind the gentle curve of the road and trees.



DESIGN PRINCIPLES

STREET SCENES

THE GREEN

3.28 The houses on the south side of the 'Green' define the line of the street whilst the dwellings on the north side have a more intimate relationship with the open space and are served from private drives.

The planted area provides a physical and visual link to the land to the east, which provides open space and contains a children's play area.



SOUTHERN BOUNDARY

3.29 The principle of symmetry is followed along the southern boundary with the apartment blocks as 'bookends' to complete the composition.

The parking courts are enclosed by boundary walls to screen cars from view and differentiate private from public parking.

The houses provide an acoustic buffer to mitigate noise levels in the private gardens at the rear.





ACCESS & PARKING

SECTION 4

ACCESS & PARKING TRANSPORT ASSESSMENT

The Transport Assessment, Travel Plan and Highway Design have been prepared by David Tucker Associates and reference should be made to their report which provides a comprehensive commentary on the issues to be addressed, the solutions that are proposed and the design of off-site highway improvements and internal roads.

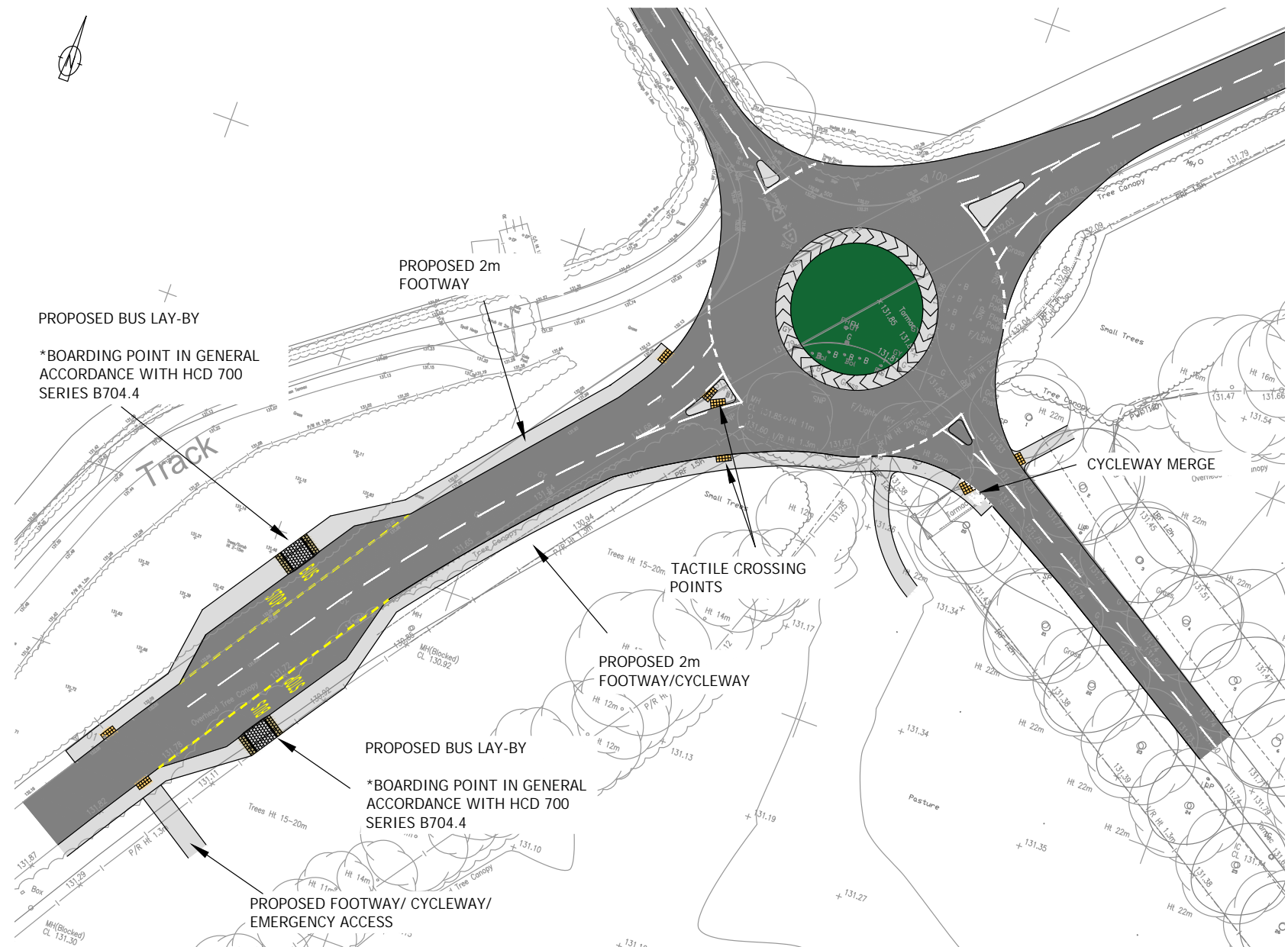
4.01 In summary, the Transport Assessment demonstrates that the proposed development would not have an adverse impact on the safety or operation of the adjacent highway network and that the proposed development accords with the relevant transport policy tests.

HIGHWAY IMPROVEMENTS

4.02 The scheme proposes that the existing site access crossroads is converted into a four arm roundabout and the residential layout accompanying the application has been designed to incorporate David Tucker Associates' design (as illustrated).

4.03 In addition to the proposed new roundabout, David Tucker Associates have set out a series of improvements that have been incorporated into the scheme, including:

- New bus lay-bys in both directions on the A426 to the southwest of the new roundabout and accessible from the site;
- A new pedestrian and cycle link from the southern parcel of land to the A426, that can also serve as an emergency-only link for vehicles;
- Improvements to the footway to provide a new 2m wide pedestrian and cycle shared surface route along the A426 between the proposed new roundabout and the existing roundabout at M6 Junction 1, continuing south to link with the route at Central Park Drive.



ACCESS & PARKING

INTERNAL ROADS & PARKING

INTERNAL ROUTES

4.04 The layout is designed to minimise the extent of formal roads, subject to highway safety, including visibility, turning and the maneuvering requirements of refuse and delivery vehicles. David Tucker Associates have undertaken an analysis of vehicle turning in the internal road system and this is included in their transport assessment.

4.05 The layout has been designed to minimise the extent of formal roads whilst providing an efficient pattern of circulation. Wherever possible, private drives give access to dwellings and roads are designed as low speed, shared surfaces.

4.06 It is proposed to re-route a public footpath through the site and improve pedestrian links from the development currently under construction to public transport routes.

PARKING

4.07 The majority of the houses are provided with private car parking to the side of the dwelling and the larger houses are provided with single or double garages, as appropriate. The smaller houses at the southern end of the site are provided with frontage parking and the apartments with small private parking courts. Garages would be designed to accommodate cycle storage and houses without garages would be provided with sheds in the rear garden for this purpose.





EVALUATION

SECTION 5

EVALUATION SCHEME DESIGN



5.01 The scheme has been designed to follow the grain of development in the adjoining housing layout around Coton House. The pattern of buildings and open landscaped spaces would link the two developments and the surrounding parkland to provide a cohesive settlement.

The landscaped spaces within the development areas determine the distinctive character of each group of dwellings. Sketches on the following pages show the relationship of buildings to landscape in the following areas:

A - THE AVENUE: where the symmetrical approach to the new houses and former stables at Coton Park informs the alignment of a small number of houses;

B - THE CENTRAL SPINE: where a linear tongue of landscaping provides a link to the parkland to the north west and provides an attractive setting to a group of larger houses;

C - THE GREEN: where a landscaped amenity space provides an attractive footpath link between the bus route on the A426 and the footpath links to Coton Park and beyond.

EVALUATION SCHEME DESIGN



A - VIEW ALONG THE DRIVE

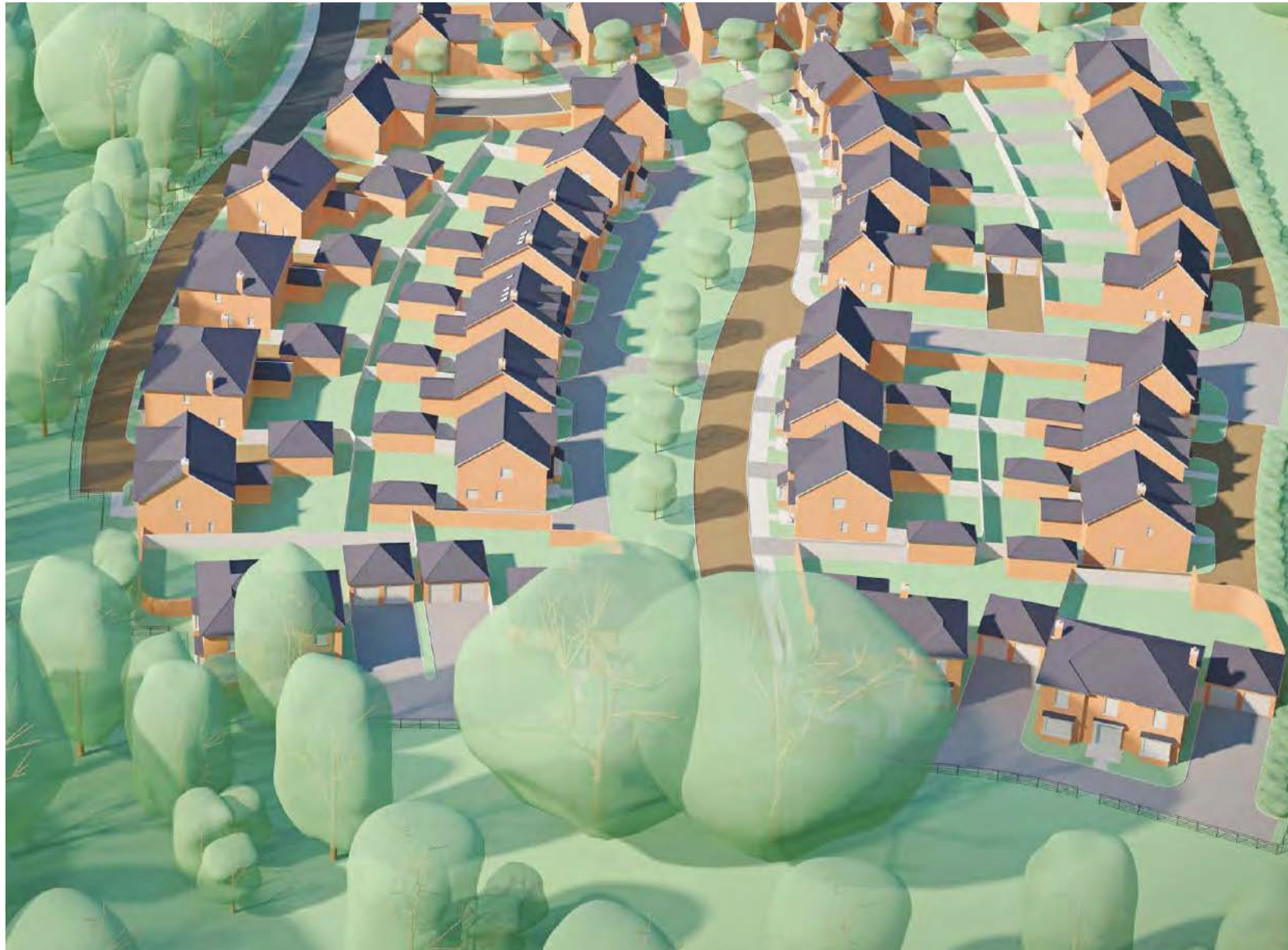
5.02 Sketch view taken from the entrance to the South Drive, looking towards the southeast. The avenue of Limes remains the dominant feature along the drive, with the houses and private drives set back behind the grassland and boundary fence.

The five houses on each side of the avenue are clearly spaced to read as individual dwellings. The two houses nearest the entrance are set behind the building line in the style of 'lodges' enclosed by boundary walls. The formality of their arrangement reflects the structure of roads and landscape.



The symmetrical design of the houses reflects the formality of the group of buildings at the head of the avenue, comprising the two new lodges, the converted former stable block, and the new houses around the former walled garden.

EVALUATION SCHEME DESIGN



B - VIEW ALONG THE CENTRAL SPINE

5.03 Sketch view from the northeast end of the central spine with the retained Poplars in the foreground. Four detached houses, partially screened by the trees, face onto the open parkland.

The gently curving grass verge and line of trees along the spine road provides a visual link with the parkland and a 'green corridor' parallel with the trees along the A426.



EVALUATION SCHEME DESIGN



C - VIEW OF THE GREEN

5.04 Sketch view looking through the 'Green' seen from the open land to the west. The green space provides a visual focus for the group of surrounding houses and adjoins the redirected route of the right of way which is extended to provide access to the bus route on the A426.

The Green provides a small, intimate open space at the heart of the development enclosed by a small number of houses, which contrasts with the open views from the frontage houses over surrounding land.

EVALUATION LANDSCAPE

LANDSCAPE PLAN

The Landscape & Visual Impact Assessment prepared by Pegasus Design arrives at the following conclusions:

5.05 On the surrounding landscape character: 'Both the site and its context are heavily influenced by urban fringe activity, including the presence of large scale commercial built form in many views; major highways infrastructure, and associated noise.'

On the site itself '...the parkland structure, whilst evident, is reduced in comparison with the areas of parkland directly north and west, by virtue of reduced tree cover. It does however include a mature Lime avenue. Directly east of the site, modern residential development is now a characteristic of the built core of the Estate.' In terms of the impact on the character of the surrounding area: 'It is considered that the effects on the wider landscape character are likely to be neutral; and the effects on the local landscape are likely to be minor adverse'.

5.06 Pegasus Design observe that the site is very well visually contained and they consider that the design proposals '... will ensure that the overall legibility of the parkland landscape that encloses the site will be retained'. They note the important role that landscape considerations have had in determining the development areas and in the layout of the dwellings. It is therefore their judgement that the landscape will remain dominant to the scale and character of the built form, which was one of the primary considerations in the collaborative approach adopted by the design team.

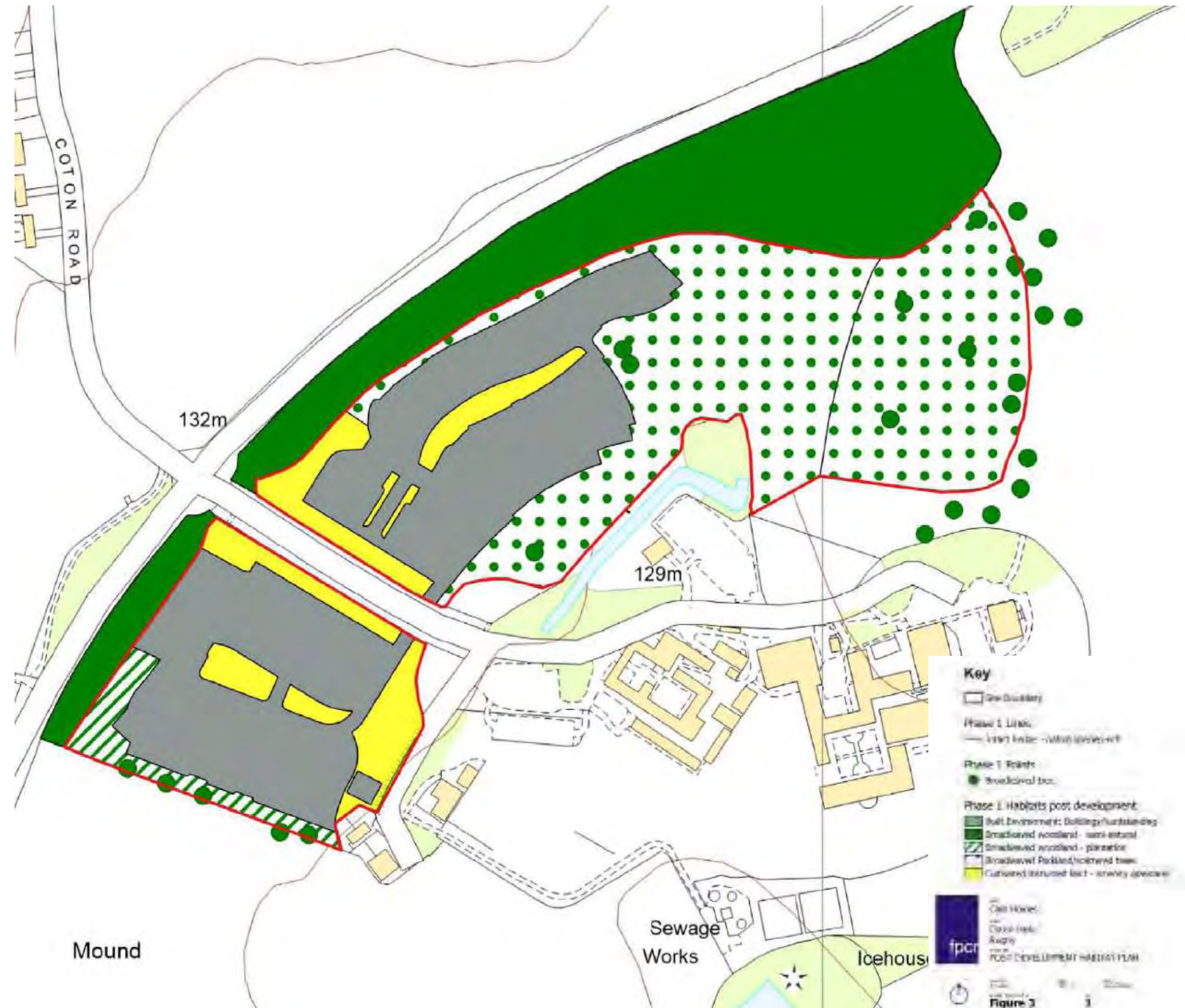


EVALUATION ECOLOGY

5.07 The ecological survey completed across the site over the relevant survey period of 2015 – 2017 confirmed that the habitats affected by the proposals were dominated by species poor semi-improved grassland identified as low ecological value. Habitats of ecological value including parkland to the east of the site and a belt of broadleaved woodland to the north of the site were present. These habitats are retained and will be enhanced through the application of sensitive native species planting and long term management. The Biodiversity Impact Matrix confirms the proposals will result in a net gain to biodiversity.

The protected species survey did not identify any statutory ecological constraint from protected species. Bat activity surveys confirmed the assemblage using the site comprised species which were common / widespread. The main foraging area / commuting route was adjacent to the broadleaved woodland to the north of the site. This woodland has been retained and appropriate measures have been implemented to avoid light spill onto the woodland thus ensuring a dark corridor commuting route / foraging area for bats is maintained.

From the completed assessment, it has been concluded that development of the site is likely to provide positive benefits for biodiversity locally.



EVALUATION HERITAGE

The Heritage Statement undertaken by Heritage Collective contains the following conclusions:

5.08 Coton House, statutorily listed Grade II* and the Stableblock, Grade II, are designated heritage assets.

'The heritage significance of Coton House and the Stableblock can be summarised as being derived from the architectural form and appearance of the buildings, in particular the neoclassical exterior and surviving historic elements of Coton House, and the historic interest of the estate as a whole as an illustrative example of a surviving, modestly sized country house with association with the distinguished English architect Samuel Wyatt.'

5.09 The application site is located within Coton Park which is a non-designated heritage asset. The application site is in an area of lower sensitivity which possesses a greater capacity for accepting change than other areas of the park and garden ;' '...while forming the setting of Coton House and the Stableblock, the area of parkland forming the application site makes only a very limited contribution to the significance of these two designated heritage assets.'

5.10 The conclusion of the Heritage Statement discusses the potential impact of the proposed development on the designated and undesignated heritage assets. In terms of the impact on Coton House and the Stableblock the conclusion is that the proposal has the potential to result in a slight effect on their significance. 'Any harm incurred would be demonstrably low and at the lowest end of less than substantial in NPPF terms. In terms of the impact on Coton Park, '...the provision of residential development onto two currently open fields, will have an effect on the significance of this asset. In this case the harm incurred will be minimal and the significance of the non-designated parkland is low.'



EVALUATION NOISE & SUSTAINABILITY

SUSTAINABLE DESIGN

5.11 Sustainability will be the key factor in the design construction and operation of the application scheme.

5.12 The new dwellings would be technically designed to meet the requirements of current Building Regulations with high levels of air tightness and insulation to avoid heat losses. This 'fabric first' approach will be coupled with the use of low energy appliances and lighting systems to further reduce energy demand. The proposals will incorporate renewable energy technologies to reduce the overall predicted carbon dioxide emissions by a minimum of 10% through the use of (either solely or a combination) of solar thermal panels, photovoltaic panels, air source heat pumps.

5.13 The proposed water usage would be in line with Code Level 4 of the Code for Sustainable Homes with water consumption limited to 110 litres per person per day. This would be achieved through a combination of water saving features including: reduced flush toilets, low flow showers, flow restrictors to taps, low water volume baths.

Further sustainability measures may also be provided subject to a viability assessment.

SUSTAINABLE CONSTRUCTION

The construction of new dwellings would meet the statutory requirements and the management of the construction process would follow best practice guidance.

5.14 Management techniques would be employed to comply with the Considerate Constructors Scheme's Code of Practice. A safe, clean, considerate and environmentally conscious site would be operated where noise, vibration and air quality are actively controlled. The Waste Hierarchy of reducing, reusing and then recycling waste would ensure that minimum volumes of materials are used.

INCLUSIVE ACCESS

5.15 The scheme has been carefully considered to enable inclusive access and the detailed design would be in accordance with Building Regulations Part M.

Externally, measures would include the provision of drop kerbs, level approach and level thresholds to access into buildings and appropriate door widths at entrances.

Internally, measures would include the provisions of toilet accommodation to the appropriate standard to the ground floors of houses and appropriate door widths to all rooms.

DRAINAGE DESIGN

Woods Hardwick have provided a flood risk assessment and recommendations for surface and foul water drainage

5.16 A sustainable surface water drainage system will be provided, utilising where appropriate existing water courses and the drainage systems being installed on the adjoining land at the Coton Park Development.

ACOUSTIC DESIGN

Cundall Acoustic Engineering have provided advice on noise levels and on acoustic design.

5.17 Environmental noise impact on the proposed residential development has been considered and a preliminary assessment indicates that some additional mitigation measures will be required to limit noise levels to the target design criteria.

Provision of enhanced perimeter treatments to private gardens at a small number of locations have been included in the design to reduce noise levels.

Glazing and ventilation strategies have been proposed, based on predicted facade noise levels. It is concluded that standard thermal double glazing, with the appropriate performance will be suitable for the majority of the dwellings. A small number of facades will require enhanced glazing performance, these predominantly being properties that have a direct relationship with the adjacent A426 or on the site boundary where noise from the M6 is prominent. Glazing to these areas will require a performance uplift and a ventilation strategy.

An assessment of the noise impact from the proposed Rugby Motorway Service Area has been made, based on the environmental statement produced as part of the submitted application. Published results indicate that the operational noise should not have a significant impact upon the Coton Park site, and no mitigation measures are therefore proposed.

Cundall's assessment concludes that the proposed residential development can meet the target acoustic criteria, both in terms of internal ambient noise levels within habitable rooms and within external amenity spaces. It is therefore considered that noise impact should not be a determining factor in this application.



CONCLUSION

SECTION 6

OVERVIEW

SUMMARY & CONCLUSION

6.01 The design is an evolution of the current successful scheme at Coton Park with high quality, bespoke housing in a rich landscaped setting. The application scheme has been the subject of considerable refinement after pre-application consultations and careful attention to heritage, landscape, ecological and environmental issues. Notable areas where the design has been developed include:

- **Reducing the number of dwellings and containing new development within the area to the southwest of the retained group of Poplar trees to considerably reduce the impact on the area of parkland in the north-west area of the site;**
- **Setting back development a greater distance from the woodland belt along the A426 with the introduction of hedgerows as a visual boundary and ecological enhancement;**
- **Orientating dwellings to face the woodland boundary towards the A426 to provide an attractive street-scene where glimpses of houses are possible through the tree screen and as a noise buffer to private gardens;**
- **Widening the grass margins between the houses and avenue of trees along the west drive and setting back the nearest houses at the entrance.**

These improvements to the scheme represent a robust response to the issues raised in consultations with the Council's Officers.

6.02 The site consists of two parcels of land within a wider application boundary, including highway land - where improvements are proposed - and the drive and avenue of trees leading to the permitted development at

Coton Park. The application is for full planning permission for the erection of 100 new homes, provision of a new roundabout on the A426, and associated works.

6.03 **In terms of heritage impact:** the two parcels of land between the Coton Park development and the A426 are considered to be in an area of lower sensitivity than the land in the previously consented development and to make only a very limited contribution to the setting of Coton House and the Stables. The conclusion is that the proposed development would result in only a slight effect on the significance of these designated heritage assets. The heritage significance of the undesignated parkland in the area of development is considered to be low and any harm incurred would be minimal.

6.04 **In terms of landscape and visual impact:** the assessment finds that the overall visibility of the site from the local and wider landscape is very limited due to the visual containment provided by belts of woodland and smaller groups of trees. There is little intervisibility between the site and the setting of Coton House and the former Stables. The avenue of Lime trees is maintained as a significant landscape element. The conclusion is that the effects of the development on the wider landscape would be neutral and the effects on the local landscape would be minor adverse.

6.05 **In terms of biodiversity:** the two parcels of land consist of fields with a low biodiversity value. A combination of mitigation and enhancement is proposed to limit the impact of development on protected species and, where possible to improve wildlife habitats.

6.06 **In terms of environmental considerations:** The layout, design of houses and enclosing walls have been based on the conclusions of the environmental noise assessment and the development would meet the target acoustic criteria in terms of internal ambient noise levels in habitable rooms and external private amenity spaces.

6.07 These heritage, landscape, ecological and environmental considerations have informed the extent and layout of the housing. The development areas are restricted to leave a significant undeveloped area of parkland in the north-west part the site, wide margins of grass between new houses and the avenue of Limes, and substantial landscape buffer areas to all boundaries. Existing landscaped elements are proposed to be enhanced to reinforce the enclosure of the site, provide visual amenity, and improve wildlife habitats.

6.08 The arrangement of houses around a landscaped central spine road in the northern parcel and a 'green' to the south links amenity space within the development to the surrounding landscape and creates a pattern of dwellings and open spaces that reflect the grain of the development around Coton House and Stables.

6.09 The wide mix of dwelling types and tenures are designed to provide additional choice in the range of housing within the Borough. The architectural treatment of dwellings is designed to achieve continuity with the earlier Coton Park development and harmony with the heritage assets and the surrounding landscape.

6.10 The application scheme proposes improvements to highways, bus set downs, pedestrian and cycle routes. The residents would enjoy a high degree of visual amenity and access to landscaped amenity areas within and around the residential layout, including the swathe of parkland retained in the north of the site.

6.11 In conclusion, the application proposes an enclave of houses on either side of the retained avenue of Limes that has been carefully designed to complement the adjoining development at Coton Park and to integrate into the existing mature landscape.

DESIGN & ACCESS STATEMENT

LAND AT COTON PARK CHURCHOVER RUGBY



FOR CALA HOMES (MIDLANDS) LTD

MAY 2017 | 189B108 | D&A REV A

Matter 4- Non- strategic Housing Allocations at Main Rural Settlements and Coton House (Policies DS3 and DS6)

Appendix I- Letter from Warwickshire County Council to Mr Back concerning a Highways response to a proposal at Land to the west of Coton House Estate.

Your ref: R15/1195

My ref: 151195



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Mr Rob Back
Head of Planning & Recreation
RUGBY BOROUGH COUNCIL
Town Hall
Rugby
CV21 2RR

FAO : Richard Holt

23rd Febraury 2018

Dear Mr Back

PROPOSAL: Full planning application for the construction of 100 dwellings, roundabout on the A426, public open space and associated infrastructure.

LOCATION: LAND TO THE WEST OF COTON HOUSE ESTATE LEICESTER ROAD CHURCHOVER, RUGBY

APPLICANT: Mr A Russell

Warwickshire County Council, hereby known as the 'Highway Authority', has undertaken a full assessment, of the planning application, and the additional information provided in response to the Highway Authority response dated the 17th October 2017.

Based on the appraisal of the development proposals and the additional information provided the Highway Authority revises its response to one of **no objection subject to the following conditions and planning obligations**. The justification for this decision is provided below.

ANALYSIS:

The submitted planning application submitted aims to secure full planning permission for the construction of 100 dwellings at on the Coton House Estate. .

In addition to the 100 dwellings the proposals include landscaping, open space, a roundabout access onto the A426 Leicester Road and pedestrian and cycle link alongside the A426 Corridor across M6 Junction 1.

The development proposals also include detailed drawings for the layout of the development and street hierarchy.

*Working for
Warwickshire*

The development proposals have been assessed in accordance with the following guidance and policy documents.

- National Planning Policy Framework published by Department for Communities and Local Government in March 2012;
- National Planning Policy Guidance: Travel plans, transport assessments and statements in decision making published by the Department for Communities and Local Government in March 2014;
- DfT Circular 02/2013; Strategic Road Network and the Delivery of Sustainable Transport;
- Warwickshire Local Transport Plan 2011 – 2026, published by Warwickshire County Council in 2011;
- Saved Local Plan Policies (Post Core Strategy Adoption), published by Rugby Borough Council in June 2011; and,
- Submitted Rugby Borough Local Plan 2011 – 2031, published by Rugby Borough Council in September 2016.

This section provides commentary on the analysis undertaken by the Highway Authority and the justification for the objection to the development proposals based on this assessment.

Transport Assessment:

The applicants have submitted a Transport Assessment (TA) which was prepared on their behalf by David Tucker Associates, hereby known as DTA.

The TA provides an assessment of the potential impacts that the development proposals could have upon the safe and efficient operation of the highway network. Through this process the identification of issues will enable discussion and potential mitigation schemes to be identified and agreed with the Highway Authority.

The Highway Authority has considered this document which has been prepared in accordance National Planning and Transport Policy and Guidance. Based on the Highway Authority's analysis the assessment demonstrates that the level of development will not have a detrimental impact upon the effective operation of the highway network based on the mitigation identified as part of the development proposals.

An improvement scheme of M6 Junction 1 has been committed as part of the permission for the approved Motorway Service Area. This scheme will provide additional operational capacity at this junction and will full signalise all approaches. The assessments included the expansion of the Coton House Development, as identified within this application. The outputs of the model demonstrated the junction operated with significant reserved capacity and accommodates the development on the network.

Based on this assessment the Highway Authority is now satisfied with the arrangement and removes its objection to this element of the application.

Access Arrangements:

As mentioned above the proposed access will be obtained from the A426 Leicester Road with the creation of a roundabout junction. This proposal does provide merits as it will realign the existing crossroad junction which is in place, and has a poor safety record.

The proposals have been supported by the provision of a Road Safety Audit Stage One and tracking drawings for vehicles accessing and exiting the proposed site, as well as vehicles travelling through the revised junction layout.

The Highway Authority in consultation with the Road Safety and Transport Planning Teams, have reviewed the proposals. Based on our assessment there are a number of matters which can be addressed through the detailed design of the junction for technical approval and suitable conditions.

Since our initial response of objection, there has been substantial correspondence stating that the proposed access arrangements will be unsafe and will lead to an increase of accidents onto the A426 Corridor. The Highway Authority has undertaken an in depth review of the access arrangements in partnership with the Road Safety Team we are satisfied that the arrangements conform to required standards as set out in Design Manual for Roads and Bridges. In addition it should be noted that if planning approval is given, then the access arrangements will be subject to further assessment of technical approval through the detailed design and implementation under Section 278 of the Highways Act 1980.

More recent correspondence has been received has been related to the fatal accident on the A426 on Saturday 17th February. It is understood that the accident took place in the evening and was located north of the proposed access arrangement. In terms of other information this is still being investigated and considered by Warwickshire Police. Therefore at this time until the investigation is completed the Highway Authority cannot fully consider it as part of the determination of this application.

Based on the analysis and consideration of the residents correspondence the Highway Authority maintains its no objection to the proposed access arrangements.

Sustainable Transport:

This section reviews the information submitted in terms of accessibility of the development by non-car based modes of transport. The Highway Authority has had on-going discussions with the applicants, their consultants and technical teams within the County Council.

The provision of a signalised pedestrian crossing across the A426 will not be supported by the Traffic Signals Team. The justification being that there will be insufficient pedestrian traffic from the development to the bus stops to justify its requirement. Consideration has been given to the fact the Motorway Service Area is now committed and will provide services and retail elements which will be to the benefit of residents of the proposed development. However it was still considered that it would not be sufficient. The Highway Authority therefore has considered that a contribution towards a signalised crossing could be achieved through a Section 106 Agreement under the Town and Country Planning Act. The Highway Authority has considered this option and considers it to be CIL Compliant with the Regulations 122 and the relevant tests.

The Highway Authority will therefore be seeking the total sum of £100,000.00 towards the provision of a signalised crossing to enable the safe crossing of residents across the A426 from the development to utilise the bus infrastructure and access services at the approved Motorway Service Area.

Discussions with the County Cycling Officer have identified options to provide a dedicated pedestrian / cycle route within the vicinity to provide access to Rugby Town Centre, and do not required pedestrians or cyclists to cross M6 Junction 1. Based on these discussions it has been considered that the development can aid the delivery of this route through the provision of a financial obligation through a Section 106 Agreement under the Town and Country Planning Act. The Highway Authority has considered this option and considers it to be CIL Compliant with the Regulations 122 and the relevant tests.

The Highway Authority will therefore be seeking the total sum of £200,000.00 towards pedestrian and cycle route infrastructure and enhancements towards Rugby Town Centre.

Based on this assessment the Highway Authority is now satisfied with the arrangement and removes its objection to this element of the application.

SITE LAYOUT ASSESSMENT:

The Highway Authority has undertaken an assessment of the proposed site layout with the amendments. Based on this assessment the Highway Authority is now satisfied with the arrangement and removes its objection to this element of the application.

SUMMARY & CONCLUSION:

The Highway Authority has undertaken a thorough and robust assessment in accordance with national and local land use and transport planning policy. Based on this assessment the Highway Authority revises its response to one of no objection subject to the following conditions and financial obligations.

Conditions:

The Highway Authority requests the following conditions to be put in place if the Planning Authority is minded to approve the planning application.

1. No construction will be undertaken until a Construction Management Plan, which must contain a Construction Phasing Plan, details to prevent mud, debris and obstacles on the Highway and HGV Routing Plan, is submitted and approved by both the Planning and Highway Authorities.
2. No HGV movements during the construction phase will take place Monday to Friday during the time periods 07:30 – 09:15 and 16:30 – 18:00, to ensure that HGV movements are limited during the peak travel periods.
3. No part of the development hereby permitted shall be occupied until the detailed design of the site access arrangements (as indicatively shown on Drawing 17147-09 Rev E, provided in Appendix E of the submitted Transport Assessment), and have first been submitted to and approved in writing by the District Planning Authority. Thereafter the approved highway access works shall be implemented in accordance with the approved plans and permanently retained thereafter.

Section 106 Request:

The Highway Authority requests the following S.106 monies to be put in place if the Planning Authority is minded to approve the planning application.

1. Prior to first occupation of the development site the sum of £100,000.00 for the provision of a signalised pedestrian crossing on the A426.
2. Prior to occupation of the 50th dwelling of the development site the sum of £200,000.00 towards pedestrian and cycle link enhancements towards Rugby.

Yours Sincerely

Ben Simm

Ben Simm
Development Group

FOR INFORMATION ONLY
Councillor Warwick – Fosse